

PORT OF SILVERDALE



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

JANUARY 2020

MISSION STATEMENT

“Providing boating facilities, recreational opportunities, and economic benefits for the greater Silverdale community in an environmentally friendly and financially responsible manner.”



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EXECUTIVE SUMMARY

A Comprehensive Plan is meant to act as a guide, defining goals and identifying potential ways to achieve them. This 2019 Comprehensive Plan explores many variables affecting Port of Silverdale decisions as they stand today. Through the research process and gathering of public stakeholder input, **including the Port's** Citizen Advisory Committee, three preliminary focus areas were identified: Overwater, Byron Street & Waterfront Uplands, Lowell Street, and Strawberry Creek. Focus areas were then analyzed to discover what opportunities might exist for complementing the future of the area known as Old Town Silverdale in response to public and stakeholder comments, **and fulfilling the Port's Mission Statement: "Providing boating facilities, recreational opportunities, and economic benefits for the greater Silverdale community in an environmentally friendly and financially responsible manner."**

The identified opportunities show a community desire to update and enhance the connection between the waterfront and the rest of Silverdale. Opportunity: Overwater explores a way to update the current dock and float. It would promote motorized and non-motorized boating activity, potentially increasing the number of out of town visitors attracted by waterfront events and enhancing functionality for local volunteer groups and business.

The possibility of a joint project involving Port of Silverdale and Kitsap County Public Works Department (KCPWD) to incorporate the redesign of the existing Pump Station #3 into the layout of a new boating center and public meeting space is also explored in this 2019 Comprehensive Plan as Opportunity: Joint Project With KCPWD & Human-Powered Boating Center. A community hub like this would blend activity with enterprise to create a more unified Old Town for local activities and visitors alike.

Opportunity: Sailboat Storage & Retail Project recognizes the need for a better watercraft storage facility supporting local volunteer boating groups and locals renting space. By blending a boat repository with commercial or retail space, the project would complement activity already taking place along Byron Street and promote economy in downtown Silverdale.

Considering the proximity of **Lowell Street's waterfront terminus to that of Strawberry Creek**, an opportunity to enhance existing pedestrian walking trails and better connect them throughout the Old Town area is apparent. This possibility is further explored in Opportunity: Trail Connection Enhancement where it is shown that Port properties in this sector lend themselves to future cultural, educational or hospitality use. Opportunity: Restoration of Salmon-Bearing Stream explores creek restoration and partnership opportunities with Central Kitsap School District for property redevelopment.

Under Washington State law (RCW Title 53), a Port is required to develop a Comprehensive Plan for the use of its existing and future properties. The concepts included in this plan apply to the Port and Port properties only and will not affect how private property owners can develop their land; only County zoning and land use regulations apply to private property. The plan elements identified here are based on current available information and conditions, and development scenarios would only occur when approved by the Port Commission and funding is available and dedicated. Additional opportunities for community input and environmental review will occur when any significant capital project moves forward.

STRAWBERRY CREEK BENEFITS

- STRENGTHEN ECOLOGICAL HEALTH OF STRAWBERRY CREEK
- PROMOTE OLD TOWN CONNECTION AND PARTNERSHIP WITH SCHOOL DISTRICT

POTENTIAL PROPERTY ACQUISITIONS

LOWELL STREET BENEFITS

- INCREASE PEDESTRIAN TRAIL ACCESS
- PROMOTE FUTURE CULTURAL, EDUCATIONAL AND HOSPITALITY OPPORTUNITIES

POTENTIAL PROPERTY ACQUISITION

TRAIL CONNECTION ENHANCEMENT

FOCUS AREA: LOWELL STREET

FOCUS AREA: BYRON STREET & WATERFRONT UPLANDS

BYRON STREET & UPLANDS BENEFITS

- PROMOTE ECONOMIC AND COMMUNITY ACTIVITY
- MAINTAIN OLD TOWN CHARACTER
- ADD 1-3K SF ROWING SHELL STORAGE AND TRAINING TO SUPPORT ROWING PROGRAMS
- ADD 1-3KSF FLEXIBLE COMMUNITY SPACE
- ADD 400-700 SF RETAIL/MANAGEMENT SPACE
- PARTNER WITH KITSAP COUNTY TO PROVIDE FOR INTEGRATED PUMP STATION #3 UPGRADES
- PROVIDE A 'FRONT PORCH' FOR THE HUMAN-POWERED BOATING CENTER WATERFRONT ACCESS
- ADD 8-12K SF SAILBOAT STORAGE TO SUPPORT SAILING PROGRAMS
- ADD 3-6K SF NEW RETAIL SPACE FOR SMALL BUSINESSES
- PROVIDE OPPORTUNITIES FOR FURTHER LOCAL RETAIL DEVELOPMENT ON BYRON STREET
- MAINTAIN WATERFRONT VIEW CORRIDORS FOR ALL

NEW SAIL BOAT STORAGE AND RETAIL BUILDING

POTENTIAL PROPERTY ACQUISITION

EXISTING OLD TOWN PUB

PUBLIC RESTROOM

HUMAN-POWERED BOATING CENTER

SHELL HOUSE

RETAIL SPACE

PLAZA

PUMP STATION #3

BEACH MITIGATION

NEW BOAT WASH RAIN GARDEN

DREDGING EXTENTS

NEW NON-MOTORIZED FLOAT

BEACH MITIGATION

MEAN HIGHER HIGH WATER

ORDINARY HIGH WATER MARK

BEACH MITIGATION

NEW ACCESSIBLE GANGWAY

NEW FINGER FLOAT FOR MOTORIZED CRAFT

OVERWATER BENEFITS

- ENHANCE MOTORIZED AND NON-MOTORIZED BOATING
- DREDGING TO MAINTAIN AND ENHANCE USE OF EXISTING BOAT LAUNCH AND MOTORIZED BOAT MOORAGE
- PROVIDE NEW FLOAT TO SUPPORT THE EXISTING AND EXPANDING NON-MOTORIZED BOATING COMMUNITY SUCH AS SAILING AND ROWING ACTIVITIES, INCLUDING LOCAL EVENTS AND REGIONAL EVENTS THAT SUPPORT LOCAL BUSINESSES
- NEW FLOAT GANGWAYS TO ENHANCE ACCESSIBILITY FOR ALL

FOCUS AREA: OVERWATER



NOT TO SCALE

EXECUTIVE SUMMARY PLAN



INTRODUCTION

The shoreline of Silverdale has long been utilized for community and commercial use. In 1854, loggers came to the area, mooring their floating bunkhouses near the shore. Thirty years later, immigrants from Scandinavia arrived to begin farming and by 1887 **had formed the Farmer’s Cooperative to sell extra crops** and animal products to Seattle and Bremerton via the Silverdale wharf and dock.

Silverdale’s waterfront area, now known as “Old Town”, remained a commercial and transportation hub until the mid-1960’s when the development of the Bremerton to Poulsbo highway shifted the commercial center off the waterfront inland to the highway and commercial boating service waned. Saved from the large scale retail development further inland, Old Town remains a modestly scaled, walkable commercial and waterfront recreational center.

The Silverdale Port District is now a local government in Kitsap County and was established in 1920. The Port District includes approximately 16 square miles with over 21,500 residents, a regional shopping center, the unincorporated Silverdale Urban Growth Area under the current Kitsap County Comprehensive Plan, approximately two miles along Hood Canal south of Bangor and three miles along the north end of Dyes Inlet.

Late in 2017, the Port of Silverdale advertised for a planning consultant to assist the Port in updating their Comprehensive Plan, which was originally published in 2007. Early in 2018, the Rice Fergus Miller and Art Anderson Associates team was selected and contracted to assist in preparing the 2019 Comprehensive Plan.

PREPARED FOR

PORT OF SILVERDALE (POS) - SILVERDALE, WA
Henry Aus, Current Commissioner
Ed Scholfield, Current Commissioner
Caleb Reese, Current Commissioner
Lawrence Greaves, Past Commissioner

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WHAT CAN A PORT DO?

A Port is a governmental agency created to develop and support local economic activities and is guided by specific State laws. A summary of the primary authorized functions of The Port of Silverdale is included below. **More detailed information on the Port's authority is included in Appendix A:**

- Acquire property
- Lease property
- Engage in economic development programs
- Improve land for industrial and commercial purposes
- Establish local improvement districts

Expend Port funds on:

- Park and recreation facilities
- Streets and roads that serve port facilities
- Acquisition, maintenance and operation of passenger carrying vessels
- Promotion of tourism

WHAT HAS THE PORT OF SILVERDALE DONE?

Over the years, the Port has developed and maintained several in-water facilities, upland support, and recreational and commercial properties. Landmark developments include:

- 1976 interlocal agreement between the Port and Kitsap County to develop, construct and maintain a public park and recreational moorage facility
- 1984 land exchange with the County to enable development of the County's Waterfront Park and construction of the Port's transient moorage facilities
- 1993 development of the Port's boat ramp and associated upland parking area
- 2001 property acquisition for expansion
- 2003-2005 redevelopment of restroom facilities, development of shower facilities and enhancement of boat trailer parking area
- 2014 acquisition of commercial property at 3423 NW Byron Street, parcel 4458-009- 002-0008
- 2016 acquisition of Old Town Pub building at 3473 NW Byron Street, parcel 4458-009-001-0009





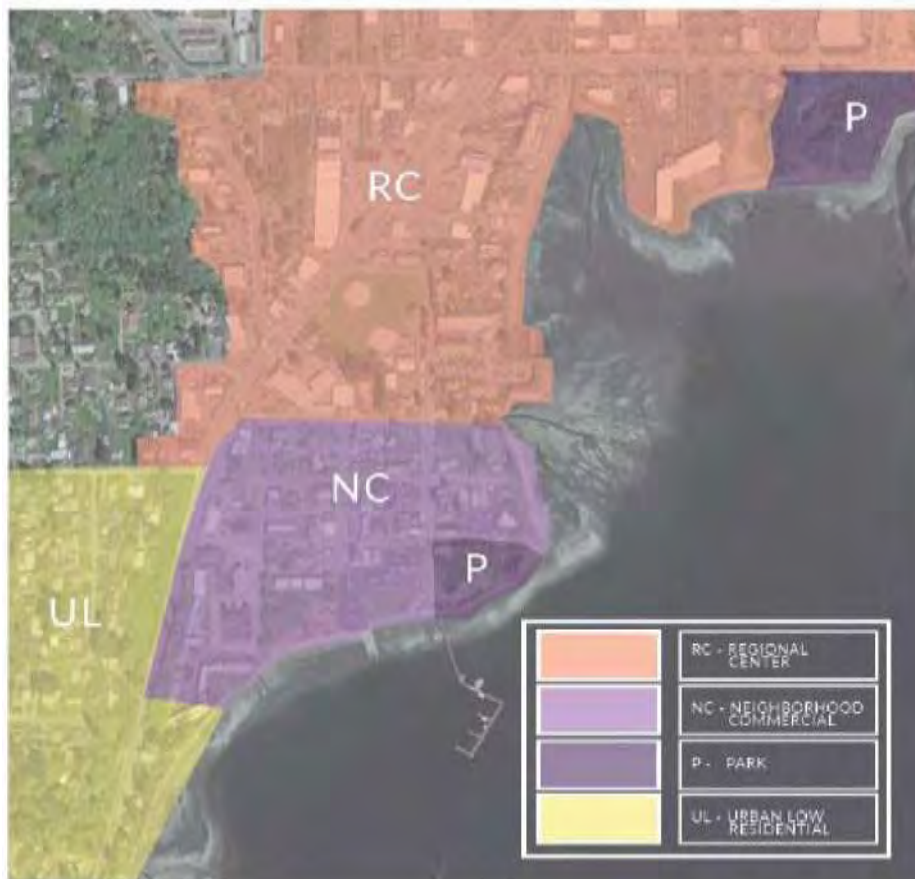
STUDY AREA

Current Kitsap County zoning of Old Town Silverdale is shown on below.

Most of the area is designated Neighborhood Commercial (NC) with the area north of NW Carlton Street designated as Regional Center (RC).

Both zones are subject to Silverdale Design Standards for the designated Old Town (OT) Design District in accordance with the Kitsap County Zoning code Land Use tables 17.410.042, 17.410.044, and 17.410.050 Footnote 19. The applicable pages of the Design Standards for OT District are included in Appendix B.

CURRENT ZONING





PROCESS

The 2019 Comprehensive Plan process began in the spring of 2018 by collecting data from the Port Commissioners and stakeholders, **including input from the Port's Citizens' Advisory Committee meeting on March 26, 2018**, followed by developing **User-Orientated Concept Plans which focused on the Port's properties south of NW Byron Street and their overwater facilities**. The User-Orientated Concept Plans were refined and presented in August 2018 for Commissioner and public input, followed by updates in September **2018 focusing on the Port's waterfront properties and developing concepts for overwater facilities** based on user input from meetings listed in Meetings and Outreach. An overwater stakeholder meeting was conducted in December 2018 and updated the previous concepts for overwater facility development presented on January 18, 2018 for comment – **see Appendix C**. The concept plans and draft 2019 Comprehensive Plan was presented to the Port in February 2019 and finalized in January 2020.

Kitsap County Department of Public Works was contacted **regarding the County's plans to replace Pump Station # 3**, which is located west of the southern terminus of Washington Street NW with Port waterfront property immediately adjacent on the west and north sides. This process entailed attending a series of meetings with KCPWD that are in this section under Meetings and Outreach, attending a Public Works public information meeting, and facilitating discussions with the County to develop a potential joint-use facility for **the benefit of Public Works' infrastructure needs, the Port, and the public**. These discussions are documented in Port meeting minutes found in Appendix C.

As part of discussion regarding the pump station plans, a joint public survey was conducted by KCDPW concerning the Pump Station #3 project and Port properties. The portion of the survey involving Port properties focused on current use and what future uses might be desired. The survey was conducted in June of 2018 and the results presented to the Commissioners at the July 19, 2018 meeting. The results are included in **Appendix E**.

CENTRAL KITSAP SCHOOL DISTRICT COORDINATION MEETINGS

The Central Kitsap School District owns nearly six acres on the north side of Old Town Silverdale, which is easily the largest parcel of contiguous land in Old Town and includes a former school building now used for CKSD administration and ball fields. The District intends to fully vacate the former school within a few years and is exploring options for thoughtful surplus or future use. Restoring Strawberry Creek as far as it touches CKSD properties is also of great interest to the District. Three meetings were conducted over the summer of 2018 between the Port and representatives of CKSD to share goals, future planning steps, and realize joint opportunities. The third and final meeting cumulated with representatives of Kitsap and Central Kitsap Community Councils present.

TRIBAL INPUT

A joint meeting took place on **October 2, 2018 with POS, Suquamish Tribe biologist Alison O'Sullivan, RFM, and Steve Segoe of Waterman Mitigation Partners**, a shoreline mitigation consultant, in order to gain input from the **Tribe regarding the Port's plans and discuss possible waterfront mitigation that could take place as part of the Port's future**. These discussions are documented in Port meeting minutes found in Appendix C.



OLD TOWN PUB STRUCTURAL ANALYSIS

The Old Town Pub, located at 3473 NW Byron Street, was acquired by the Port in 2016. On October 19, 2017 Kitsap County issued a Notice of Restricted Use, effective November 2, 2017, due to structural and life safety concerns. This restricts entry to the structure for the purpose of repairs only.

At the Commissioner Meeting on March 9, 2018, a proposal for an independent structural analysis of the building was presented. This proposal was approved at the May 17, 2018 meeting.

Findings of the draft report prepared by Structural Engineering firm Reid Middleton, Inc. were presented by RFM to the Port at the August 16, 2018 meeting, and the report finalized on September 10, 2018. Notes from the August meeting can be found in Appendix C and the final Structural report in Appendix F.

MEETINGS AND OUTREACH

The following indicates meetings and outreach conducted as part of the 2019 Comprehensive Plan. Published notes or presentations for these meetings are included in Appendix C.

January 12, 2018	Port Facilities walkabout with Phil Best and RFM
January 18, 2018	Port of Silverdale Regular Meeting with RFM update
January 19, 2018	Meeting with Angie Silva of Kitsap County Public Works, RFM and AAA to engage Kitsap County input
February 2, 2018	Meeting with Port of Silverdale, Kitsap County Public Works, and RFM to begin dialog regarding KC Public Works timeline and work to date regarding Pump Station #3
February 6, 2018	Port of Silverdale Special Meeting with Kitsap County to review proposed easements on Port Property for sidewalk improvements
February 27, 2018	Kitsap County Public Works Silverdale Pump Station 3 Stakeholder Management Meeting attended by RFM and Port of Silverdale
March 9, 2018	Port Commissioners Planning Kickoff Workshop with RFM and AAA to establish design goals and review draft public input survey questions
March 15, 2018	Port of Silverdale Regular Meeting
March 26, 2018	Port of Silverdale Citizens' Advisory Committee Meeting attended by RFM and AAA to gather stakeholder input
April 19, 2018	Port of Silverdale regular Meeting with RFM update
May 2, 2018	Central Kitsap Community Council Meeting attended by RFM with



presentation by RFM to update the community regarding the Port's planning process and present first draft of Concept Use Plan.

May 10, 2018	Kitsap County Public Works with POS and RFM to review draft public input survey questions prepared by Kitsap County and RFM on behalf of POS
May 17, 2018	Port of Silverdale regular meeting with RFM update
May 29, 2018	First of three coordination meetings between POS, RFM, AAA, and CKSD
June 26, 2018	Port of Silverdale Regular Meeting
July 16, 2018	Kitsap County Public Works meeting with POS and RFM to review public survey results
July 19, 2018	Port of Silverdale Regular Meeting with RFM update
July 19, 2018	Second of three coordination meetings between POS, RFM, AAA, CKSD
July 26, 2018	Kitsap County Public Works Pump Station 3 Upgrades Public Meeting attended by RFM
August 16, 2018	Port of Silverdale Regular Meeting with RFM Concept Use Plan Revisit presentation included in Appendix C.
August 23, 2018	Third of three coordination meetings to further discuss POS, CKSD, and Kitsap County involvement in Old Town Silverdale's future. Hosted at CKSD Jenne Wright Building and facilitated by RFM and AAA.
September 12, 2018	Port of Silverdale meeting with presentation by RFM and AAA
September 20, 2018	Port of Silverdale Regular Meeting
September 25, 2018	Kitsap County Public Works Pump Station #3 meeting with RFM
October 2, 2018	Joint meeting with POS, Suquamish Tribe biologist Alison O'Sullivan, Washington Department of Fish and Wildlife, RFM, and Steve Segoe of Waterman Mitigation Partners.
October 16, 2018	Port of Silverdale Regular Meeting with RFM update
November 11, 2018	Port of Silverdale Regular meeting with RFM update. RFM facilitated a visit and update from Kitsap County Parks and Recreation planner Steve Stanley



December 4, 2018	Meeting with POS, non-motorized boating organizations, and RFM to discuss overwater options at POS. Hosted and facilitated by RFM with AAA
December 17, 2018	Port of Silverdale Regular Meeting
January 17, 2019	Port of Silverdale Regular Meeting with overwater options presentation by RFM and AAA
February 21, 2019	Port of Silverdale Regular Meeting with RFM presenting DRAFT 2019 Comprehensive Plan for POS review and comment
March 21, 2019	Port of Silverdale Regular Meeting to receive comments on 2019 Comprehensive Plan
July 18, 2019	Port of Silverdale Regular Meeting for status report of comments received in June 2019
August 15, 2019	Port of Silverdale Regular Meeting to discuss current status of ACOE permits
September 19, 2019	Port of Silverdale Regular Meeting to discuss Grant Funding & Comprehensive Plan, partnerships with other agencies, and RCO review of comp plan
October 17, 2019	Port of Silverdale Regular Meeting to discuss updates to comp plan including a capital improvement program, grant funding available, and RCO review
November 18, 2019	Port of Silverdale Special meeting to discuss the draft updated comp plan
November 21, 2019	Port of Silverdale Regular Meeting to discuss comp plan presentations scheduled for December 4 th at the Silverdale Beach Hotel
December 19, 2019	Port of Silverdale Regular Meeting to discuss RCO's review of comp plan prior to final adoption before March 1st



STAKEHOLDER PARTICIPANTS

2019 Comprehensive Plan work was conducted and presented in open, public, Port of Silverdale meetings. Other stakeholders, in addition to members of the public who attended Port Meetings, were included via outreach and are listed below:

- **Port of Silverdale Citizens' Advisory Committee**
- **Central Kitsap Community Council and their public audience at CKCC's open meeting on May 2, 2018**
- Kitsap County Public Works Department
- Kitsap County Commissioners Office
- Central Kitsap School District Administration
- Kitsap Economic Development Alliance
- Visit Kitsap Peninsula
- Kitsap Public Facilities District
- Non-motorized boating groups with programs associated with the Port of Silverdale
- Kitsap County Department of Parks and Recreation
- The Suquamish Tribe
- Washington Department of Fish and Wildlife



PLAN GOALS

2020 Comprehensive Plan goals include:

- Coordination with Kitsap Public Works and their effort to update Pump Station #3
- Coordination with Silverdale Sub Area plan
- **Coordination with Central Kitsap School District's early planning efforts** of significant property holdings north of Carlton Street
- Coordination with Kitsap County Parks and their facilities across Washington Avenue
- Coordination with the Suquamish Tribe and other stakeholders on proposed RCO grants



CURRENT UPLAND PROPERTIES

8886 McConnell Ave NW, Silverdale, WA 98383	.27 acres
8889 McConnell Ave NW, Silverdale, WA 98383	.24 acres
3403 NW Byron Street, Silverdale, WA 98383	.25 acres
3489 NW Byron Street, Silverdale, WA 98383	.11 acres
3497 NW Byron Street, Silverdale, WA 98383	.11 acres
3423 NW Byron Street, Silverdale, WA 98383	.23 acres
3473 NW Byron Street, Silverdale, WA 98383	.11 acres
9020 Washington Ave NW, Silverdale, WA 98383	.23 acres
3295 NW Lowell Street, Silverdale, WA 98383	.11 acres
3255 NW Lowell Street, Silverdale, WA 98383	.16 acres
3215 NW Lowell Street, Silverdale, WA 98383	.28 acres
3332 NW Lowell Street, Silverdale, WA 98383	.34 acres
Parcel 4458-009-009-0001 (No SITUS address)	1.44 acres
Parcel 4458-018-001-0000 (No SITUS address)	.06 acres
Parcel 4458-009-006-0004 (No SITUS address)	.12 acres



ECONOMIC ANALYSIS

BST Associates prepared a socio-economic report issued in 2007 to provide background information on the economy and demographics of the Silverdale area and to help identify potential development opportunities that meet the goals of the Port and community. The report is included in the 2019 Comprehensive Plan to provide economic context and will not be used to determine future decisions without prior review and updates if necessary. This section summarizes the findings of their report, which is included as Appendix G. Overall, the report revealed:

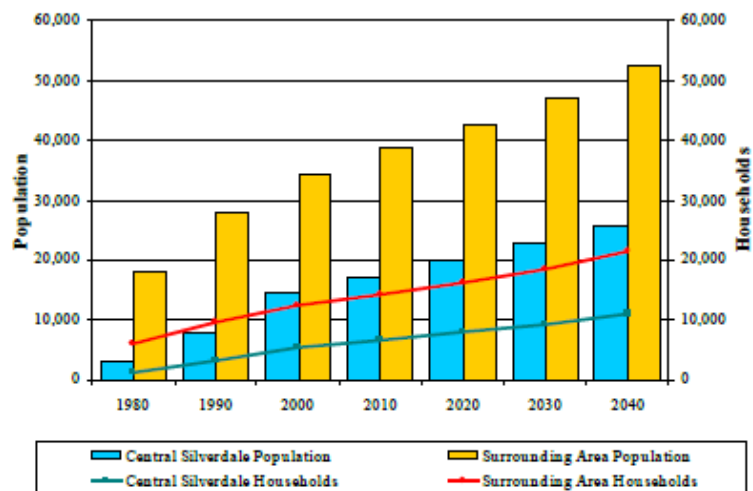
- Population growth in the Silverdale area is expected to continue to rise
- Employment in the service sector (finance, insurance, real estate and others) is expected to experience the most significant growth, adding approximately 100 jobs per year through 2020
- Demand for commercial space, particularly office space, is strong; there will likely be demand for mixed-use development in the Old Town area from the retail and service sectors
- There does not appear to be significant industrial/manufacturing opportunities for the Port currently

Neither permanent moorage nor fueling facilities are considered feasible projects for the Port at this time; however, the Port should track usage of the transient dock to determine if the demand for temporary moorage exceeds the amount of available space. When that occurs, the Port should explore expanding existing transient moorage facilities

DEMOGRAPHICS

The Puget Sound Regional Council (PSRC) reports that between 1980 and 2000 the population of Kitsap County grew by an average of more than 4,100 residents per year. Though this growth trend slowed after the year 2000, it is expected to accelerate again between 2010 and 2020.

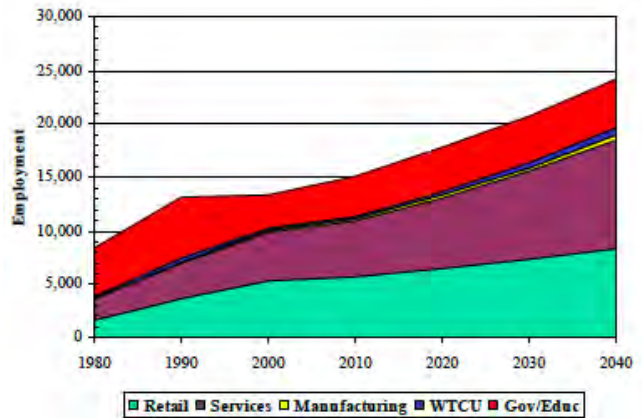
Figure 1. Population and household growth trends in Central Silverdale and surrounding areas.





According to the historical trends and forecasts from PSRC, the population of the Central Silverdale¹ area jumped from fewer than 3,000 to more than 8,100. Between 1990 and 2000 the number of residents grew by an additional 6,400. From 2000 through 2020 the PSRC forecast an average annual growth of 1.6%, with the population growing to approximately 17,000 in 2010 and 20,000 in 2020, or by approximately 300 new residents per year. Growth in the number of households shows a similar pattern.

Figure 2. Silverdale area employment rates by sector.



EMPLOYMENT

Government is currently the biggest source of jobs in Kitsap County, due mainly to the presence of the Navy. However, forecasts indicate that employment in the retail and services sectors is expected to grow much faster through 2022.⁹

The Silverdale area retail sector accounted for 75% of all new area jobs between 1980 and 2000, largely due to the construction of the Kitsap Mall and other nearby stores. Retail and service jobs are expected to continue to increase through 2020, with a projected average of 60 new jobs per year.

The service sector (finance, insurance, real estate and others) is expected to grow at a much faster rate in the Silverdale area, adding approximately 2,000 new jobs between 2000 and 2020 (100 jobs per year).

The economic analysis also explored the potential for development opportunities in the manufacturing sector. However, while manufacturing is an important source of jobs in Kitsap County, significant growth is not expected. In the Silverdale area, manufacturing accounts for very little employment and is expected to grow by just an additional 88 jobs through 2020.

While government jobs in the Silverdale area dropped by approximately 3,000 jobs between 1980 and 2000, government and education jobs are expected to grow slowly through 2020, adding approximately 40 new jobs per year.

UNEMPLOYMENT

Unemployment in Kitsap County has tended to be less severe than in the rest of the state, primarily due to the high percentage of government jobs in the county. In 2006 unemployment rates in Kitsap County were nearly at their lowest point in the last 17 years at 4.7%.



INCOME

Personal income comes primarily from three sources: wage and salary income, transfer payments (such as Navy pensions and Social Security), and dividends/interest/ rent. Since 1970, Kitsap County and the rest of the state have witnessed a decline in the share of income that comes from wages and salaries. Meanwhile, the share of income contributed dividends, interest and rent has increased, reflecting the growth of the retired population in both Kitsap County and Washington State.

RETAIL SALES

In Kitsap County taxable retail sales grew by an annual average rate of 4.7% from 1998 through 2005, which is a very strong growth rate and much faster than the population growth rate. In contrast, taxable retail sales in Washington State grew by an average of 2.0% per year during the same period. The fastest growth in retail sales occurred in unincorporated parts of the county, including Silverdale, where annual growth was 7.2 % **per year**. While “big box” retailers like Wal-Mart and The Home Depot accounted for a significant percentage of annual retail sales, they did not account for the entire annual percentage rate growth. Table 1 illustrates taxable retail sales by category.

Category	1998	Share of Total	2005	Share of Total	AAGR *
Motor Vehicle and Parts Dealers	\$34.4	5.3%	\$55.7	5.2%	7.1%
Furniture and Home Furnishings Stores	\$21.3	3.2%	\$20.4	1.9%	-0.6%
Electronics and Appliance Stores	\$32.1	4.9%	\$65.3	6.1%	10.7%
Building Material and Garden Equipment and Supplies Dealers	\$57.7	8.8%	\$164.4	15.4%	16.1%
Food and Beverage Stores	\$31.9	4.9%	\$40.8	3.8%	3.6%
Health and Personal Care Stores	\$12.1	1.8%	\$15.4	1.4%	3.6%
Gasoline Stations	\$10.9	1.7%	\$14.5	1.4%	4.1%
Clothing and Clothing Accessories Stores	\$36.8	5.6%	\$55.4	5.2%	6.0%
Sporting Goods, Hobby, Book, and Music Stores	\$35.1	5.3%	\$55.7	5.2%	6.8%
General Merchandise Stores	\$273.4	41.7%	\$383.3	35.8%	4.9%
Miscellaneous Store Retailers	\$34.3	5.2%	\$64.2	6.0%	9.4%
Non-store Retailers	\$6.1	0.9%	\$18.7	1.7%	17.3%
Food Services and Drinking Places	\$69.7	10.6%	\$116.7	10.9%	7.6%
Total	\$655.7	100.0%	\$1,070.5	100.0%	7.3%

Taxable Retail Sales in Unincorporated Kitsap County by Business Category* Average Annual Growth Rate (AAGR)

Source: BST Associates, WA State Department of Revenue data

INDUSTRIAL DEVELOPMENT

While Port districts in Washington State commonly develop industrial parks in order to attract tenants who will create good-paying jobs in a community, there does not appear to be a strong need for the Port of Silverdale to enter the industrial land business. Approximately 181 acres of industrial-zoned land and land



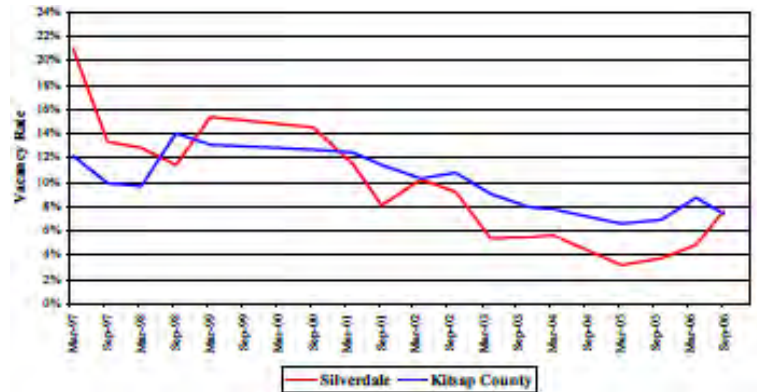
with industrial space in the Silverdale UGA (Urban Growth Area) is vacant, though vacancy rates for industrial buildings in North Kitsap have generally been low since 2001 (5% - 7.6%). This low vacancy rate is likely a result of reluctance by developers to build speculative industrial space, resulting in less industrial space available on short notice.

The large amount of vacant industrial land around Silverdale suggests that there has not been much market incentive for private developers to build industrial space in the area. It is possible that the Port of Silverdale could develop speculative space in order to try to attract industrial jobs, something that the private sector is reluctant to do. However, the low number of jobs expected over the long run makes this a less compelling prospect for the Port.

OFFICE SPACE

The vacancy rate for office space in Kitsap County has been declining for most of the past decade, and until very recently the rate in Silverdale was even lower. Figure 3 illustrates office vacancy trends in both Kitsap County and Silverdale.

According to commercial real estate agents, the asking base rate for office space in Silverdale is close to \$17.00/sq. ft/year, while the countywide asking rate is closer to \$15.00. Triple net2 costs add an additional \$3.50 - \$8.00 per year to this price. With services employment projected to grow by approximately 1,100 to 1,370 jobs between 2007 and 2020, an additional 440,000 to 575,000 square feet of office space will be needed by 2020 within the Port of Silverdale district.



Source: Bradley Scott, Inc.

RETAIL SPACE

Fluctuations in the rate of vacancy for retail space in Silverdale has followed the same general pattern as those for the county, with Silverdale seeing higher highs and lower lows than the county.

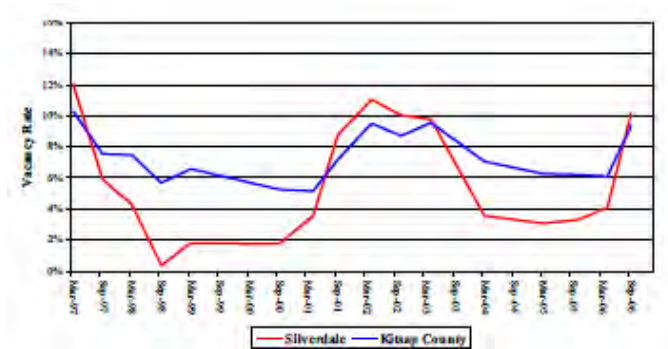


Figure 4 illustrates retail vacancy trends in both Kitsap County and Silverdale.

Source: Bradley Scott, Inc.

According to interviews with commercial realtors, the average asking rate in Kitsap County can be as high as \$24.00/sq. ft./year for retail space, but currently listed properties range from \$11.00 to \$20.00. The relatively low asking price for currently listed properties is likely due to the relatively high vacancy rates. However, with retail employment projected to grow by approximately 475 to 725 jobs between 2007 and 2010, an additional 265,000 to 600,000 square feet of retail space will be needed by 2020 within the Port of Silverdale district.

MIXED-USE

Development of mixed-use buildings in waterfront areas has become a very popular and financially successful business in the past decade. There are numerous examples of these facilities in the Pacific Northwest including the Riverplace project in Portland, East Bay Landing in Olympia and Marina Village in Everett, among others. The waterfront in downtown Bremerton is currently undergoing a massive transformation, with redevelopment adding a new regional waterfront conference center, hotel, parking, restaurants, retail space condominiums, and expanded marina facilities. Mixed-use developments include the following types of uses:

- Offices for professional services such as law offices, financial services, real estate, etc.
- Retail space generally oriented toward upscale retail goods such as art, jewelry, leather goods, etc.
- Personal services such as hair salons and barbers
- Restaurant, delicatessen and/or related food products, wine shops, etc.
- Condominiums or apartments (usually second and/or or third level)

The Port of Silverdale is in a good position to take the lead in promoting this kind of development. Overall, the demand for commercial space in Silverdale is strong, especially for office space to serve the fast-growing services industry. Employment growth projections for the Silverdale area indicate that as much as 600,000 square feet of office space may be needed between 2007 and 2020. The office market presents an attractive



opportunity for the Port of Silverdale's Old Town properties. Retail space is also seeing increasing demand, although the rate of growth in the retail sector is not expected to be as strong as growth in services. Additionally, mixed-use redevelopment of Port property in the Old Town area is likely to see demand from the retail sector as well as from the services sector.

The growth in the population of Kitsap County also means a need for additional housing. The Port of Silverdale property in Old Town Silverdale may present an attractive location for development of mixed-use buildings that contain both housing and commercial space.

PERMANENT MOORAGE AND MARINE FUEL FACILITY

There are currently an estimated 39,000+ slips in marinas in Puget Sound, with Kitsap County accounting for approximately 7% of the total permanent slips. However, a large portion of these slips are less than 40 feet and cannot accommodate the growing number of larger vessels on the market.

Growth in the number of boats that require moorage will generate the need for around 300 to 400 additional slips per year for the next ten years in Puget Sound. Kitsap County is expected to account for 12% to 15% of this demand, or 40 to 50 slips per year.

Two projects are likely to satisfy the demand for additional moorage/larger slips in Kitsap County. The first is the completed downtown Bremerton marina with 221 permanent slips and room for 80-100 visiting boaters, while a potential reconstructed Seabeck marina is expected to provide 200 slips. In total these two facilities will provide a maximum of 521 slips, while anticipated demand for the next ten year is for 509 slips.

A marine fueling facility is also likely to prove financially risky. The short boating season of Memorial Day to Labor Day means that a marine fuel facility must generate enough sales over a four-month period to cover 12 months' worth of operation & maintenance costs as well as financing costs. In addition, much of the boating activity in the area involves trailered boats, which can fuel at gas stations that can charge a lower price than a marine fuel facility.

MARINA FINANCE CONSIDERATIONS

Construction costs for marinas are escalating rapidly, which makes it difficult to build marinas profitably or to find financing. Compounding the problem are low moorage rates in the Kitsap County market. Estimated construction costs at some recent marina projects in the Puget Sound have run \$10.00 per linear foot or more, but moorage rates at marinas near Silverdale are low:

Port of Poulsbo - \$4.23

Port of Brownsville – \$4.75-\$5.75

Port of Kingston - \$4.57

The new Bremerton marina was an approximately \$32 million project, and a similarly sized project for Silverdale would likely face similar costs. This level of commitment would likely use all the Ports financing capacity and preclude the Port of Silverdale from engaging in other types of projects.



TRANSIENT MOORAGE EXPANSION

Demand for transient moorage is seasonal, with nearly all the demand occurring between Memorial Day and Labor Day. The demand for additional transient moorage space in Silverdale will depend on boaters being attracted to Old Town. Redevelopment of Old Town may increase the number of shore-side activities available to boaters and increase the demand for temporary moorage. The Port should track usage of the existing transient dock to determine when the demand for temporary moorage exceeds the amount of available space. When that occurs that Port should examine the possibility of expanding the existing facility.



PRELIMINARY FOCUS AREAS

Given the newest property acquisitions listed in 'What Has the Port of Silverdale Done?' on page 9, Commissioners were interested in the 2019 Comprehensive Plan taking a fresh look at potential uses for their combined property holdings in Old Town Silverdale. Additionally, the condemnation by Kitsap County **of the Port's recently acquired property known as the Old Town Pub, formerly the Emel Building**, suggested a need to evaluate it for the possibility of stabilization, renovation, or both.

The preparation of a Concept Use Plan was proposed to create a focus for the POS 2019 Comprehensive Plan. That exercise led to a meeting with user groups and, upon invitation, a presentation to the Central Kitsap Community Council. The Concept Use Plan can be found in Appendix C.

PORT OF SILVERDALE & CENTRAL KITSAP SCHOOL DISTRICT COLLABORATION

As a result of the presentation to the Central Kitsap Community Council, leadership at Central Kitsap School District proposed a series of three meetings with POS to discuss the future of Old Town. Minutes from those meetings are contained in Appendix C. CKSD has significant land holdings north of Carlton Avenue and proposed the joint meetings to share their future goals and objectives with the Port, and vice versa. One outcome of those meetings was a graphic aerial of Old Town showing CKSD and Port holdings and suggesting three focus areas, shown on the following page. This exercise provided the two agencies and design team with a clearer, shared understanding of how their respective properties might, in a complementary way, be best utilized for the future of Old Town.

For the Port of Silverdale, the exercise made it easy to see that the Port's waterfront holdings do not need to support larger landmark uses that would create high traffic, demand for parking, and loss of openness near the waterfront. Instead, it confirmed that the Port's mission to provide for recreational opportunities could be expanded there, at an appropriate scale, and would enhance the historical 'front porch' of Old Town and greater Silverdale. It also suggested even more reason to strengthen pedestrian trails and walking routes in and around Old Town.



CKSD
POS

FOCUS AREA: STRAWBERRY CREEK

STRAWBERRY CREEK

FOCUS AREA: LOWELL STREET

SILVERDALE SUB AREA PLAN
OLD TOWN DESIGN DISTRICT

PACIFIC

LOWELL

CONNELL

WASHINGTON

FOCUS AREA: BYRON STREET
& WATERFRONT UPLANDS

BYRON

FOCUS AREA: OVERWATER

N
NOT TO SCALE



FOCUS AREA: OVERWATER

OPPORTUNITY: OVERWATER UPGRADES

Given the number of established non-motorized boating programs, both volunteer and commercial, in Silverdale, an improvement in facilities to support them makes sense and fits the Port's recreation and access mission.

The overwater project has three pieces:

- Replace the current float with a larger, more accessible float to better serve non-motorized craft:
 - Sailboats connected with the volunteer youth and adult boating programs associated with the Port.
 - Kayaks, stand-up paddleboards, and other craft associated with seasonal vendors occupying a small facility on the Old Town dock during summer months.
 - Volunteer youth rowing program associated with the Port

The float would be open to all non-motorized craft and would feature a low freeboard for easy **access in and out of the water. Its placement northeast of the current marina floats will provide** better protection from prevailing winds. The float will be situated so as not to prevent motorized craft from tying up to the existing marinas dock.

- A new gangway from the existing Old Town Pier to the existing marina dock to optimize accessibility.
- A new finger dock extension onto the southernmost part of the existing marina dock, to provide better mooring area protection and maintain or increase moorage space.

For placement of this project see Focus Area: Overwater Plan on page 27.
Art Anderson Concept Mooring Plan is shown on page 28.



ACTION ITEMS

NON-MOTORIZED

- Upgrade the current float situation in support of increased and more diverse non-motorized boating activities with the following goals:
 - A new float with low freeboard in a protected location to support rowing, paddling and sailing activities, including seasonal storage of boats for a commercial kayak vendor. The new float will essentially replace the existing small float now used for summer sail boating.
 - A new gangway from pier to float to improve overwater accessibility.

MOTORIZED

- Dredging Goal: Maintain adequate depth for continued use of the existing boat launch and mooring facilities
- No loss of mooring spaces for motorized boating with 1:1 replacement at minimum
- Add a new finger float to existing float configuration that will provide better wind protection and allow larger boats to tie up.
- Install a new gangway from pier to floats to improve accessibility.

FINDINGS

- **The solutions above match the Port's ability to fund and construct upgrades without a general bond and maintain flexibility to entertain upland projects.**
- Developing a 250-300 slip marina with permanent moorage, and the parking to support it, is not currently a focus for the Port.



EXISTING BOAT RAMP

DREDGING EXTENTS

NEW NON-MOTORIZED FLOAT

NEW ACCESSIBLE GANGWAY

NEW FINGER FLOAT FOR MOTORIZED CRAFT

EXPLORED NEW FLOAT LOCATIONS



NOT TO SCALE

FOCUS AREA: OVERWATER PLAN



FOCUS AREA: BYRON STREET & WATERFRONT UPLANDS

OPPORTUNITY: JOINT PROJECT WITH KITSAP COUNTY PUBLIC WORKS & HUMAN-POWERED BOATING CENTER

The Port of Silverdale explored and validated the need for a near-water “Human-Powered Boating Center” as an appropriate landmark project for its waterfront properties on Dyes Inlet. This would provide a home for growing volunteer kayak and rowing programs in the Central Kitsap area and beyond, which are currently operating with limited storage and active use features. At the present time there are no adequate storage facilities for any of the area’s boating programs associated with the Port. Such a facility would be a major boost to the Port’s mission of increasing opportunities for access to recreational activities and would provide the possibility of bringing water-centered events to Silverdale.

Additionally, the Port has long been fielding requests at POS Public Meetings for an appropriately scaled, flexible meeting space in the heart of Old Town. This use can be accommodated as part of the boating center, providing more options for use and a revenue stream to the Port. It should be noted that a) Silverdale’s former go-to meeting space at the Billie Eder Center at the Central Kitsap Community Campus was demolished in 2016, and b) discussions with Central Kitsap School District have brought a mutual understanding that large gatherings creating heavier traffic and parking requirements would be better served farther north in Old Town.

Lastly, an opportunity was found to bring a more comprehensive, integrated solution to the described needs of the Port and KCPWD, who went public with early ideas to replace their sewer system’s Pump Station #3, front and center on Silverdale’s waterfront and immediately adjacent to Port property. Incorporating a public plaza into the solution’s design would complement plans to improve the ‘front porch’ aspect of the waterfront, increase walkability, and enhance Old Town trail connections. Pump Station #3 improvements are part of the County’s efforts to upgrade their county-wide sewer conveyance system.

The “Human-Powered Boating Center” would be a joint project to house all three needs. The project was inspired by a similar facility in Gig Harbor, which has been widely embraced. The concept was met with acceptance from Kitsap County Public Works and the Port of Silverdale. Achieving this project will require the Port to enter a Memorandum of Understanding with Kitsap County to address funding, development and planning, ownership, operations, and maintenance.

This proposed structure would create a harmonious, integrated, and useful project for Silverdale and Central Kitsap County.

See Focus Area: Byron Street & Waterfront Uplands Plan on page 34 for placement of this project.



PRELIMINARY PROJECT PROGRAM

Main Level:

- 1,000-3,000 square foot human-powered boat storage, fitness, and flexible use space
- 1,000-2,000 square foot pump station space with secured access and proximity to wet well site
- 400-700 square foot event-related/quasi-public restrooms
- Potential 500-800 square foot small retail or management office area
- Storage

Upper Level:

- 1,000-3,000 square foot flexible community meeting and event space with catering area
- Outside combination open and covered deck space
- Storage
- Stair and elevator accessibility
- Restroom

Site Improvements:

- Approximately .70-acre site development including new building footprint
- Integration with Kitsap County Washington Street road improvement project
- Waterfront Plaza area
- Pedestrian path connections, including a section of waterfront pedestrian path
- Motorized boat haul-out and rinse-off rain garden

OPPORTUNITY: SAILBOAT STORAGE & RETAIL PROJECT ON BYRON STREET

The Port of Silverdale explored and validated the need for a solution to the current storage condition of sailboats and other watercraft such as motorized follow-boats and those that belong to citizens paying for space. These craft are currently stored in an open, vacant, Port-owned property south of Byron Street and west of McConnell. While boat storage in this neighborhood is in demand - especially in proximity to the waterfront - its location entails potentially hazardous crossings of the main entry street to Old Town for groups hand-carrying shells and sailboats to the water. The property in question directly fronts Byron Street, **considered Old Town's main artery, and is underutilized while presenting an unsightly appearance.**

This site suggests an infill project for boat storage because of its proximity to the boat launch, and the fact that it backs up against existing three-story apartments which already block any potential for views to the water. However, because the property is in the heart of Old Town and Byron Street is lined with businesses, it makes sense to include a pedestrian and street-friendly retail or small business component.

To achieve a project like this and make it a worthwhile scale, acquisition of the property at the southwest corner of Byron and McConnell is recommended. This infill project would benefit the Port and create increased economic activity on Byron Street.

See Focus Area: Byron Street & Waterfront Uplands Plan page 34 for placement of this project.



PRELIMINARY PROJECT PROGRAM

Main Level:

- 6,000-8,000 square foot covered boat storage
- 3,000-6,000 square foot small business and retail space along Byron Street. This is currently shown conceptually as three spaces of equal square footage but can be divided in any manner.

Upper Level:

- 2,000-4,000 square foot usable loft area to support covered boat storage

Site Improvements:

- Minimal

ACTION ITEMS

- Maintain a sense of openness in the upland waterfront area, with view access from Byron Street to Dyes Inlet, and develop activities on the waterfront.
- Define and better identify pedestrian areas and linkages in the waterfront and near-shore uplands. Currently, parking dominates the area with very little obvious and safe pedestrian choice.
- Determine if the existing Old Town Pub building can be renovated at a reasonable cost and what **value it might provide to this part of Old Town given the Port's priorities**, or if replacement or redevelopment is more feasible.
- Find improved solutions for the storage and transport of human-, sail-, and motor-powered watercraft currently stored on Port property. Many of these watercrafts are used in programs now officially supported by the POS.
- Look at opportunities to strengthen beneficial working partnerships with agencies near the Port, including Kitsap County Public Works and Kitsap County Parks and Recreation.
- Identify restoration and mitigation opportunities along shoreline parcels owned by the Port.
- Determine opportunities to provide interpretive trails and/or built structures, for passive enjoyment or watercraft storage, in the identified wetland between the foot of Pacific Avenue and the current boat launch.
- Help find a more universally beneficial solution to the planned Kitsap County Public Works replacement of Pump Station #3. Be aware of limitations, restrictions, and compliance requirements tied to the granting of open space and recreational use accrued when the Federal Government deeded waterfront parcels to the Port.
- Identify attractive target properties not currently owned by the POS for acquisition.



FINDINGS

- **Defining better pedestrian areas and linkages does meet the Port’s recreational mission and meets the Port’s Comprehensive Plan, Kitsap County’s Comprehensive Plan, Silverdale Downtown guidelines, and the Old Town Association’s vision statement.**
- A structural and life safety assessment of the Old Town Pub was undertaken by Reid Middleton Inc., a consultant to RFM. In summary, the Old Town Pub building is in poor condition and does not meet current structural, seismic, or life safety codes. A very substantial investment would be required to make the building habitable.
 - At the time of this 2019 Comprehensive Plan, Commissioners have not elected to pursue a full cost estimate for this work.
 - Using public funds to restore the OTP for its most recent use as a drinking establishment does not provide broad value to the public, as other drinking establishments exist in Old Town.
- **Scaling projects that fit both the Port’s mission and the goals for the neighborhood make it possible** for the Port to fund and construct upgrades.
- Raised interpretive trail and structure development in identified wetland parcels will not be supported.
- Solutions were proposed to address storage and transport of human-, sail- and motor-powered watercraft in a way that fits the objectives and various plan guidelines for the Byron Street and Waterfront Uplands area.
- As a result of this 2019 Comprehensive Plan effort, an approach to and an association with Kitsap County Public Works was made. This resulted in the possibility of working together to develop a joint facility that would serve Port and community needs including watercraft storage and workout space, meeting area, and space for replacement of the County’s Pump Station #3. A similar project in the City of Gig Harbor was referenced. At the time of this 2019 Comprehensive Plan, a Memorandum of Understanding has been executed for the Port and Kitsap County Public Works to consider. Approvals for compliance with deed restrictions must be obtained.



PROPERTY ACQUISITIONS

In support of a Sailboat Storage & Retail Project on Byron Street, the purchase of real estate at the southwest corner of Byron Street and McConnell Avenue is desirable:

Parcel 4458-004-006-0005
3481 Byron Street
Approx. 4,356 square feet (0.10 acres)

Parcel 4458-004-006-0104
2475 Byron Street
Approx. 436 square feet (0.01 acres)

Note: this property appears to be two separate parcels divided by a public alley, but Kitsap County GIS information shows only one parcel number.

EXPLORED BYRON STREET INFILL OPTIONS

SAIL BOAT STORAGE AND RETAIL BUILDING

RETAIL COMPLEX WITH PUBLIC PASSAGE

POTENTIAL PROPERTY ACQUISITION

EXISTING OLD TOWN PUB

SHELL HOUSE

PUBLIC RESTROOM

PUMP STATION #3

RETAIL SPACE

PLAZA

EXPLORED HUMAN POWERED BOATING CENTER AND JOINT KITSAP COUNTY PROJECT

EXPLORED INTERPRETIVE TRAIL

BEACH MITIGATIONS



BOAT STORAGE RETAIL SPACE WITH SAIL LOFT ABOVE

SAIL BOAT STORAGE AND RETAIL
SECTION AND NORTH ELEVATION



RETAIL SPACE SHELL HOUSE

PUMP HOUSE

MEETING / CLASSROOM

SHELL HOUSE BUILDING
SOUTH ELEVATION



NOT TO SCALE



FOCUS AREA: LOWELL STREET

OPPORTUNITY: TRAIL CONNECTION ENHANCEMENT

- According to the Executive Summary Plan on page 5, the port holdings around Lowell Street would make an excellent location for future cultural, educational or hospitality use.
- Due to its proximity to Strawberry Creek and the existing pedestrian trail, it makes sense to expand this connection further by enhancing walkability.
- Partnership with CKSD to restore Strawberry Creek would further strengthen this part of Old Town.

ACTION ITEMS

- Use this location to enhance and connect pedestrian trail links in and through Old Town.
- Identify attractive target properties not currently owned by the Port for acquisition.
- Discuss options for creek mitigation.

FINDINGS

- Developing active overwater access at Lowell Street's eastern terminus is not attractive due to the very shallow ocean depths and the proximity to the mouth of Strawberry Creek.

PROPERTY ACQUISITIONS

One key piece of property on Lowell Street, a waterfront parcel on north side of Lowell Street, is targeted for acquisition:

Parcel 4458-012-007-0007
3250 NW Lowell Street
Approx. 10,890 square feet (0.25 acres)



STRAWBERRY CREEK

POTENTIAL
PROPERTY ACQUISITION

EXPLORED TRAIL
CONNECTION ENHANCEMENT





FOCUS AREA: STRAWBERRY CREEK

OPPORTUNITY: RESTORATION OF SALMON-BEARING STREAM

In 2018, the Port held a series of high-level planning discussions with Central Kitsap School District (CKSD) to discuss partnership opportunities. CKSD is a major property owner in north Old Town. Strawberry Creek, a salmon-bearing stream, bisects CKSD's acreage and flows through culverts into Dyes Inlet in a compromised fashion. The Port and CKSD share a desire to see Strawberry Creek restored.

ACTION ITEMS

- Continue open discussions with CKSD. Together, the Port and CKSD can set a direction for appropriate redevelopment of CKSD acreage and other north Old Town parcels in a way that best complements other Old Town Focus Areas.

PROPERTY ACQUISITIONS

Where it may be advantageous for stream restoration efforts, the Port should seek to acquire key properties between CKSD's acreage and Dyes Inlet:

Parcel 202501-1-183-2009 (No SITUS address)

Parcel 202501-1-184-2008
9191 Washington Ave NW, Silverdale, WA 98383

Parcel 4458-006-004-0200
9161 Washington Ave NW, Silverdale, WA 98383

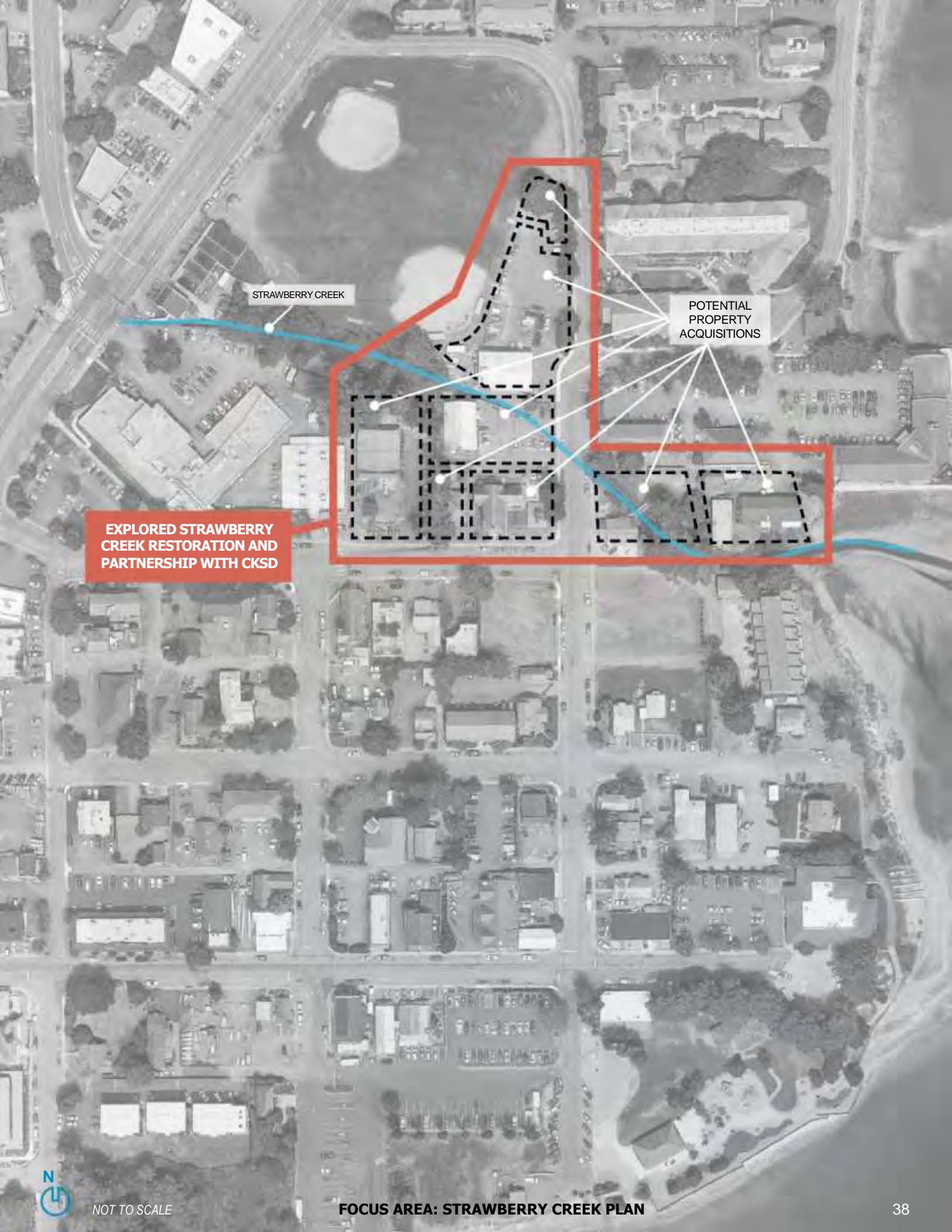
Parcel 4458-006-001-0005
3450 NW Carlton Street, Silverdale, WA 98383

Parcel 4458-006-010-0103
3392 NW Carlton Street, Silverdale, WA 98383

Parcel 4458-006-010-0004
3382 NW Carlton Street, Silverdale, WA 98383

Parcel 4458-011-003-0003
9130 Washington Ave, Silverdale, WA 98383

Parcel 4458-011-010-0004
9220 Bayshore Drive NW, Silverdale WA 98383



STRAWBERRY CREEK

POTENTIAL
PROPERTY
ACQUISITIONS

EXPLORED STRAWBERRY
CREEK RESTORATION AND
PARTNERSHIP WITH CKSD



NOT TO SCALE

FOCUS AREA: STRAWBERRY CREEK PLAN



BEACH MITIGATIONS

A meeting was held between the Port, the Suquamish Tribe Fisheries Department, the State of Washington's Department of Fish and Wildlife, the design team and special consultant Waterman Mitigation Partners on October 2, 2018 to discuss beach restoration and mitigation opportunities. The meeting produced five conclusions for the Port to consider:

- Beach mitigation between the current boat launch and the Old Town dock:
 - Soft armoring and having a new pedestrian trail pulled back from current eroding lawn edge is strongly encouraged.
- Beach mitigation between boat launch and the foot of Pacific Avenue at two wetland parcels:
 - Removal of creosoted logs and opportunities for interpretive viewing into wetland parcels is strongly encouraged.
- Beach mitigation at Port property at the foot of Pacific Avenue:
 - Removal of concrete rubble and soft armoring strongly encouraged. There is a good possibility of a safe pedestrian connection from paved street to beach.
- Existing natural swale and de-facto wetland adjacent boat launch:
 - Should be developed as a functioning rain garden that would accept runoff from boat rinsing after haul-out. This would actively manage contaminants. The Port can get credit for this during shorelines permitting.

Waterman Mitigation Partners anticipates the process would begin by performing a formal site assessment. The assessment would establish the baseline condition of today's environmental function and condition and would project the future restored function based on a restoration and enhancement plan. The difference between these two conditions would point to the "ecological lift" that could be achieved by performing the restoration work. In turn, that ecological lift can be quantified as a mitigation credit value, and that work can be monetized -assigned a value or benefit of ecological lift and cost to perform- allowing the Port to attract grant funding or private capital to accomplish the mitigations.

Waterman Mitigation Partners is currently consulting with the US Navy on a regional stream mitigation project that includes Old Town's Strawberry Creek. Given the timing, it may be possible to wrap the Port's mitigation opportunities into the larger work.

See Executive Summary Plan on page 4 for location of possible mitigations.



DECISION CRITERIA

Below are three questions with must be asked of any proposed changes or updates to Old Town and the Port of Silverdale to ensure the community will be promoted, the future will be supported, and the history and natural flow of the area will be respected.

DOES IT PROMOTE THE COMMUNITY'S VISION?

- In 10 years, both volunteer and commercial community boating organizations have flourished and gained a vital role in prompting the Port to rethink the way it sees the marina in terms of human-powered boating and overwater events.
- Port of Silverdale Commissioners wish not to pack the Old Town area with uses that generate constant and heavy traffic and large parking requirements. This begs the question: What can we design that maintains the scale and space necessary for organic smaller-scaled community activities?

ARE THERE ANY PARTNERSHIP OPPORTUNITIES?

- During **the Port's 2019 Comprehensive Plan** process, both Kitsap County and Central Kitsap School District were identified as having potential for partnership.
- Central Kitsap School District has been proactive in reaching out to POS, realizing their complementary mission and goals, to provide coordinated efforts in Old Town.
- An agreement in principal between Port of Silverdale and Kitsap County is now being explored to coordinate efforts on a joint facility for Old Town.
- The Suquamish Tribe may be interested in supporting beach enhancement and mitigation activities.

DOES IT RESPECT THE FOCUS AREAS SHOWN IN THE EXECUTIVE SUMMARY GRAPHIC ON PAGE 4?

- The consensus from joint meetings is that large-scale events which demand additional parking areas would be better developed in the northern end of Old Town, while smaller-scale activities and overwater recreation would be better developed near Byron Street and the waterfront.



RECOMMENDATIONS

PARTNERSHIP OPPORTUNITIES

There are outstanding opportunities close at hand to work in a coordinated fashion with other agencies for the overall benefit of Old Town. Failure to do so will create missed opportunities.

PARTNERSHIP ACTION ITEMS

- Pursue a joint project with Kitsap County Public Works that will coordinate the Port's recreational needs near the waterfront with KCPWD's needs for its Pump Station #3 and could possibly add much needed community meeting space in the process.
- Continue conversations, which began during the assembly of the 2019 Comprehensive Plan, with Central Kitsap School District, who owns nearly 6 acres north of Carlton Avenue, including Strawberry Creek. CKSD is contemplating best uses for their property over the next generation. CKSD sees itself as a **steward of this public asset and wishes to see it used for Old Town's** collective good in a way that complements proposed Port activities.
- **Continue public input with the Port's Citizens' Advisory Committee, Silverdale Community Council,** motorized and non-motorized volunteer and commercial boating community, and any other group or person concerned with the future of the Port and Old Town Silverdale.

ENVIRONMENTAL STEWARDSHIP

Mitigation opportunities were identified by Waterman Mitigation Partners, with the Suquamish tribal input, along Port-owned waterfront parcels between Pacific Avenue and the Old Town dock. These beach restoration opportunities should be pursued in concurrence with project development priorities identified in this section.

ADDITIONAL ACTION ITEMS

- Coordinate with strategies, guidelines and standards expressed by applicable Kitsap County Plan documents.
- Be aware of what is going on around the Port involving other public agencies and respond appropriately.
- Propose projects or development that:
 - Respect and promote good use of open space and enhance pedestrian connections.
 - Provide solutions for current unmet needs and allow current programs to grow and flourish.



- Promote Old Town as a haven for small business in Silverdale.
- **Enhance Old Town's role as Silverdale's historical "front porch."**
- Ensure this piece of Silverdale remains distinctive from the rest of town.

Capital Improvement Program

Boating, Recreation and Shoreline Enhancement Projects

This document provides a six-year plan and 15-year vision for the Ports boating, park, and recreation systems, and the steps needed for development, improvements, acquisitions, new park facilities and expanding recreational opportunities based on expressed public needs and desires. The Commissioners of the Port of Silverdale have initiated the prescribed public review and hearing process described in this plan as specified in Chapter 36.70A of the Revised Code of Washington (RCW). The Commissioners have informed the public through news releases, articles, and Web site updates and have consulted residents through public hearings and meetings to determine what recreation and restoration facility improvements are desired by the local citizenry.

The Commissioners determined that the following projects were top recreation, boating, and shoreline enhancement priorities in the short term (Years 1-4).

1. Overwater facilities will be expanded, with new non-motorized boating facility ramp coming easterly off the end of the fixed pier, and possible extension of the southerly end of the mooring floats. Project will require dredging – county and state permits in hand, awaiting final Army Corps of Engineers (ACOE) permits. Includes 5 year permit for dredge in 2020 with 2024 touch up as needed.
2. Beach enhancement with soft beach protection from the boat launch east to the fixed pier, including the area where PS#3 existing pump station will be removed.
3. Non-motorized boating center, with design including joint use of a new building located north of the existing sewer pump station (PS#3) as joint project with Kitsap County and public viewing on top. This includes close work on timing and joint design with Kitsap County Public Works.
4. Restoration and improvement of large motorized moorage floats – design completed for electrical rewiring, lighting and plug-in stations; possible re-plumbing and other features as designs progress.
5. Boating facilities improvements including new boat storage along Byron west of McConnell.
6. Joint effort with Central Kitsap School District (CKSD) to improve Strawberry Creek as a salmon stream, including acquisition of property to allow the stream to meander near Washington Avenue and the Scout Hall.
7. Planning and development of trail systems to enhance pedestrian connections, respect the natural flow of the area for future cultural, educational or hospitality use. Due to its proximity to Strawberry Creek and the existing pedestrian trail, it makes sense to expand this connection further by enhancing walkability.
8. Demolish the Old Town Pub building, possibly relocate the existing buildings to the east of the Pub building, and design/build a facility for waterfront oriented non-profit use and retail.



Long Range (Years 4-7) plans include the following objectives:

1. Identify attractive target properties not currently owned by the POS for acquisition. Acquire available properties to expand facilities. Work with the community to convert properties to beneficial use.
2. Acquire property for sailboats storage and expanding non-motorized boating opportunities. Provide wheel chair-accessible walkways and increased parking to improve park usability.

As per the Washington State Recreation and Conservation Office (RCO) Program Guidelines, we have prepared a list of these priority projects, potential funding, and completed the Self-Certification Form located in **Appendix H**. For the Port to grasp investment from state/federal/non-profit organizations, they will first need to adopt the Short and Long Term projects that are listed in this document. The set projects are priorities that community members and others had made known through public meetings, discussion sessions, emails, and surveys that were completed.

Funding opportunities include the Ports own resources, local volunteer assistance, community organizations, and local funding resources as well as State and Federal agencies such as RCO, ESRP, WSDOT, WDOC, WDNR, NPS, USDA, NFS, EDA, etc., with funding available. The Port needs to plan their project needs and become aggressive in grasping dollars that will assist the Port in implementing this documents goals and objectives set forth.

CONCLUSION

This 2019 Comprehensive Plan is a tool to understand what best to do with the Port's accumulated real estate holdings under their joint mission of economic benefit and recreational access for the community.

This Comprehensive Plan was developed through a cycle of planning, presentations to Commissioners and **the Port's Citizen's Advisory Committee**, and response sessions at Port of Silverdale Public Meetings with invited guests and the general public.

PORT OF SILVERDALE APPENDICES





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**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

APPENDIX A: AUTHORITY OF THE PORT

Selective Abstract

RCW 53.08.010 – Acquisition of property

A port district may acquire all lands, property, property rights, leaseholds, or easements necessary for its purposes and may exercise the right of eminent domain, and may levy and collect assessments upon property for the payment of all damages and compensation in carrying out its purposes . . .

RCW 53.08.040 – Land improvement for industrial and commercial purposes

A district may improve its lands by dredging, filling, bulkheading, providing waterways or otherwise developing such lands for industrial and commercial purposes.

A district may also acquire, construct & operate sewer and water utilities & other facilities to control or eliminate air, water or other pollution – and make available to others.

RCW 53.08.050 – Local improvement districts

A district may establish local improvement districts within the district and levy special assessments in up to 10 year annual installments on all property specially benefited by the local improvement on the basis of special benefit.

RCW 53.08.060 – Improvement of waters and waterways

A district may improve navigable and nonnavigable waters within the district and improve any water, watercourses, bays, lakes or streams, whether navigable or otherwise, flowing through or located within the district.

RCW 53.08.080 - Lease of property

A district may lease all lands, wharves, docks and real and personal property owned and controlled by it, for such purposes and upon such terms as the port commission deems proper.

RCW 53.08.110 – Gifts, improvement

Port commissioners are authorized to accept for the port district gifts of real and personal property and to expend in improvements and betterment such amount as may be necessary.

RCW 53.08.160 Studies, promotion of facilities

Port districts may initiate and carry on necessary studies, investigations and surveys required for proper development, improvement and utilization of all port properties, utilities and facilities, and for industrial development within the district carried out by public agency, institution, or body for a public purpose – and for the proper promotion, advertising, improvement and development of such port properties, utilities and facilities

RCW 53.08.240 Joint exercise of powers and joint acquisition of property

Two or more port districts may jointly exercise all powers granted to each district, and may acquire jointly all lands and property necessary for their purposes. A district may enter into any contract with the United States, or any state, county, or municipal corporation, or any department of those entities, for carrying out any of the powers that each of the contracting parties may by law exercise separately.

RCW 53.08.245 Economic development programs authorized

It shall be the public purpose for all port districts to engage in economic development programs. In addition, port districts may contract with nonprofit corporations in furtherance of this and other acts relating to economic development.

RCW 35.08.255 Tourism promotion

Any port district has power to expend moneys and conduct promotion of resources and facilities in the district or general area by advertising, publicizing, or otherwise distributing information to attract visitors and encourage tourist expansion.

RCW 53.08.260 Park and recreation facilities

A port district may construct, improve, maintain, and operate public park and recreation facilities when such facilities are necessary to more fully utilize boat landings, harbors, wharves and piers, air, land, and water passenger and transfer terminals, waterways, and other port facilities authorized by law pursuant to the port's comprehensive plan of harbor improvements and industrial development.

RCW 53.08.330 Streets, roads

Any port district may expend port funds toward construction, upgrading, improvement, or repair of any street, road, or highway that serves port facilities.

RCW 53.29.020 Trade centers, facilities authorized

Any port district may acquire, construct, develop, operate and maintain all land or other property interests, buildings, structures or other improvements necessary to provide a trade center including but not limited to:

- (1) A facility consisting of one or more structures, improvements and areas for the centralized accommodation of public and private agencies, persons and facilities in order to afford improved service to waterborne and airborne import and export trade & commerce;
- (2) Facilities for promotion of such import and export trade and commerce, inspection, testing, display and appraisal facilities, . . office meeting rooms, auditoriums, libraries. . . marketing and exhibition facilities, facilities for federal, state, county and other municipal and governmental agencies providing services relating to the foregoing . .

RCW 39.33.060 Transfer of property or use for park & recreational purposes

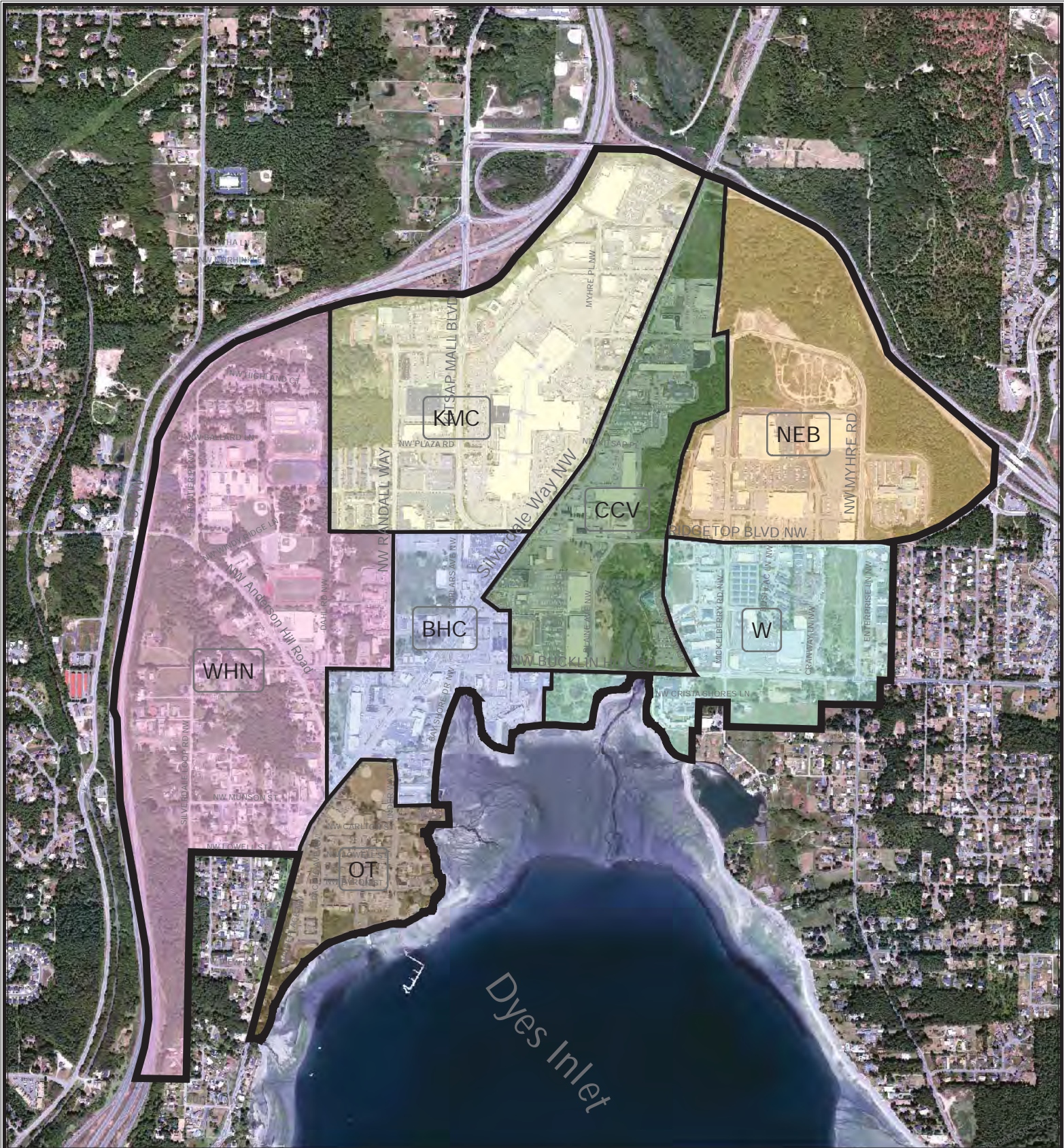
A port district may convey its real or personal property to, or contract for its use by, the county or park and recreation district wherein such property is located, for park or recreational purposes, upon such terms and with such consideration as might be mutually agreed to by such port district the board of county commissioners.

RCW 39.34.030(1) Any power a port can exercise may be exercised jointly with any other public agency having the power.



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

**APPENDIX B: SILVERDALE SUB AREA PLAN
OLD TOWN DESIGN DISTRICT STANDARDS**



Downtown Silverdale

Design Districts

Kitsap County Department of Community Development
 614 Division Street, MS-36, Port Orchard, Washington 98366
 VOICE (360) 337-7181 * FAX (360) 337-4925

This map was created from existing map sources, not from field surveys. While great care was taken in using the most current map sources available, no warranties of any sort, including accuracy, fitness, or merchantability accompany this product. The user of this map assumes responsibility for determining its suitability for its intended use.
 * THIS MAP IS NOT A SUBSTITUTE FOR FIELD SURVEY *



- Bucklin Hill Center (BHC)
- Clear Creek Village (CCV)
- Kitsap Mall Center (KMC)
- Northeast Business (NEB)
- Old Town (OT)
- Waterfront (W)
- West Hill Neighborhood (WHN)



Chapter 3. Old Town District

3.1 Physical Identity Elements & Opportunities

Old Town's physical identity is different from the remaining portions of downtown Silverdale. Old Town's physical identities include:

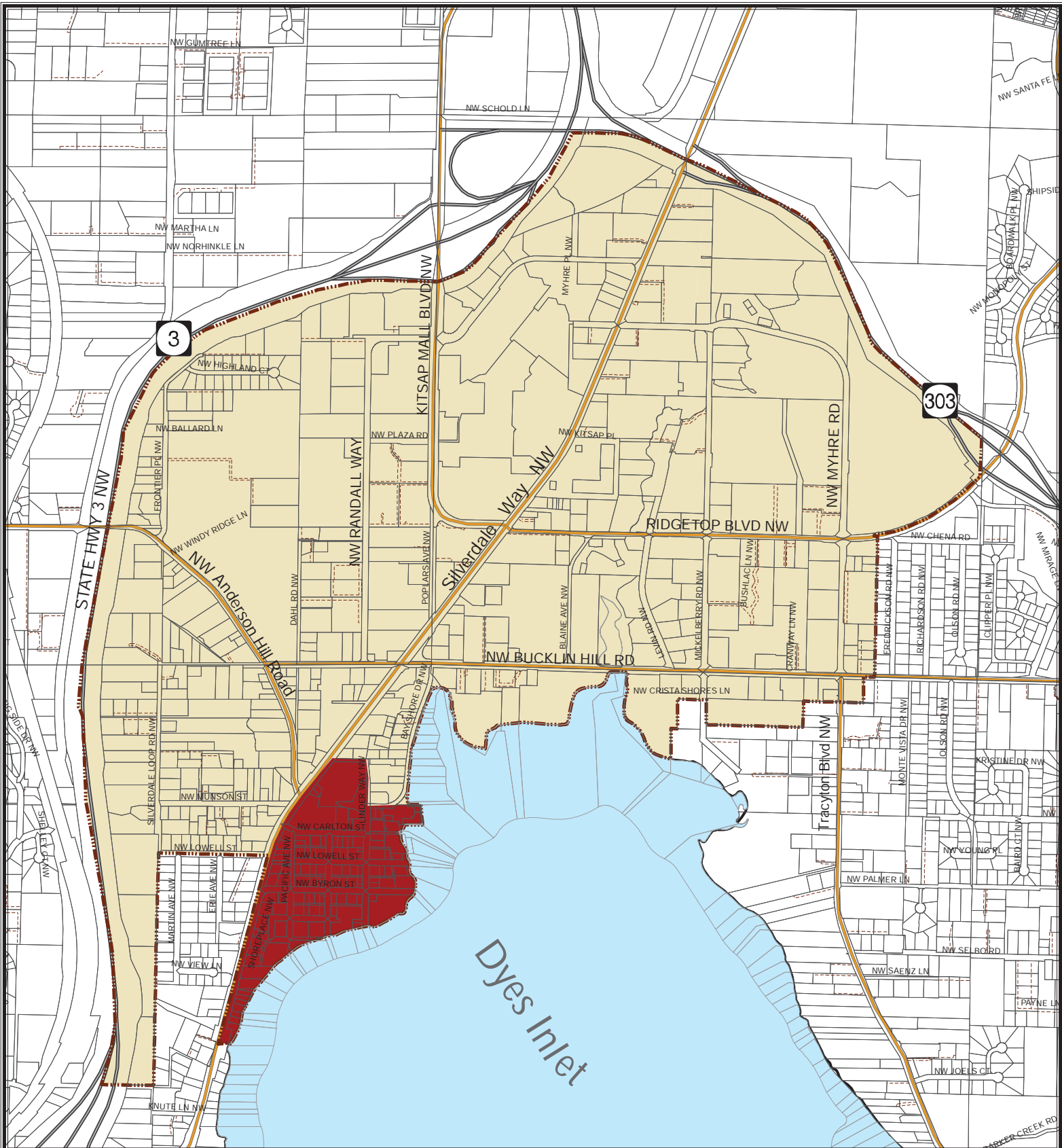
- Block grid street pattern, 200 feet by 300 feet, two way vehicular movement.
- Older one to two story, small-scale commercial and office building types with little or no front yard setbacks.
- Dyes Inlet, shoreline, waterfront parks and views of Inlet.
- Strawberry Creek and riparian area.
- "Silverdale Creek," an underground drainage channel that emerges from a culvert southeast of the Bucklin Hill Road and Bay Shore Drive intersection.
- Linder Field.
- Older, small-scale residential buildings on small blocks with landscaped front yards.
- Older churches and public/semi-public buildings throughout Old Town.

3.2 Design Intent

Modifications to existing buildings and design of new buildings will respect the historic design characteristics of the block grid pattern and adjacent and nearby buildings. The historical relationship of buildings fronting on the street or sidewalk, or the landscaped "front yard" pattern, if applicable, will be maintained in new and reconfigured development. Parking will be located to the rear or side yards or, where appropriate, may be located adjacent to the public roadway, configured as back-in angle parking. The creeks and Dyes Inlet shoreline will be protected and the riparian areas of the creeks will be expanded as major physical features and assets in the development pattern of Old Town.

3.3 Design Principles

- A. The vegetated slopes should be protected and maintained along both sides of Silverdale Way from approximately NW View Lane north to NW Lowell Street right-of-way.
- B. The Silverdale Way right-of-way should be landscaped with materials and colors different from other sections of Silverdale Way north.
- C. The Strawberry Creek channel and riparian edge along both sides of Silverdale Way should be expanded.
- D. Key entries to Old Town and its special features should be highlighted with landscape art in the form of sculpture, signs, fabrics and lights.
- E. The small block grid building pattern in the Old Town commercial center, with contiguous buildings along block fronts oriented toward the sidewalk and street rather than parking lots, should be protected and maintained.
- F. New or redevelopment should be encouraged to respect existing smaller, non-contiguous development, where appropriate, such as that shown in Figure 3-2 *Smaller-Scale Development*.



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 * THIS MAP IS NOT A SUBSTITUTE FOR FIELD SURVEY *

Map Date: January, 2007

Downtown Silverdale

Design Districts

Old Town (OT)

- Old Town (OT)
- Design District





Figure 3-2 Smaller-Scale Development

- G. Public access to and along the Dyes Inlet shoreline, including view points where appropriate, should be maintained and expanded.
- H. Pedestrian activities on public and private open space within and along the commercial/office blocks should be promoted.
- I. Shared off-street parking lots should be provided in key locations for employee and consumer parking.

3.4 Design Actions

3.4.1 LANDSCAPING, NATURAL FEATURES & SIGNAGE

- A. A flowering tree type or hedge should be planted along both sides of Silverdale Way from the Downtown Silverdale southern boundary to NW Lowell Street right-of-way on the west and to NW Byron Street on the east.
- B. Where signage/tree conflicts exist with adjacent uses, the trees should be arranged in clusters rather than a linear edge along Silverdale Way.
- C. The Strawberry Creek riparian area has been constricted by adjacent developments and Silverdale Way culverts, increasing the intensity of water flow and safety issues. The Strawberry Creek channel and riparian area on both sides of Silverdale Way could be expanded to reduce water flow intensity and increase the visual amenity of the creek area. Habitat signage, sculpture, and viewing areas along Silverdale Way at the creek could be added. Engineering studies of appropriate safety barriers, which permit visual and/or physical viewpoint access to the expanded creek area, could be conducted.
- D. If the school administration site is redeveloped, the Silverdale Way frontage can be designed as a landscaped gateway with additional landscape setbacks along Silverdale Way with clusters or rows of ornamental trees.
- E. Old Town logo signage or sculpture could be located at the intersection of NW Byron Street and Silverdale Way, Strawberry Creek, and NW Carlton Street. See Figure 3-3 *Potential Signage* as an example.
- F. If the NW Carlton Street right-of-way is vacated at Silverdale Way, a portion of right-of-way at Silverdale Way could be used for gateway landscaping and Old Town signage.



Figure 3-3 Potential Signage

- G. As part of a larger gateway design, the feed mill building could be designated as a community architectural asset, encouraging its retention regardless of use changes.

3.4.2 BUILDING ORIENTATION, HEIGHT & TYPES

- A. All buildings will be oriented to the street grid and sidewalk network with main entry(s) facing the street.
- B. Except for those areas of historical, smaller, non-contiguous development, a continuous and contiguous building façade will be maintained along the block front adjacent to street and sidewalk, avoiding gaps or “missing teeth” in the front block façade for parking. See Figure 3-4 *Old Town Block Façade Example*.
- C. All commercial buildings will have ground level transparent front facades oriented toward the pedestrian sidewalks. Figure 3-5 *Example Mixed Use Development* shows this concept.

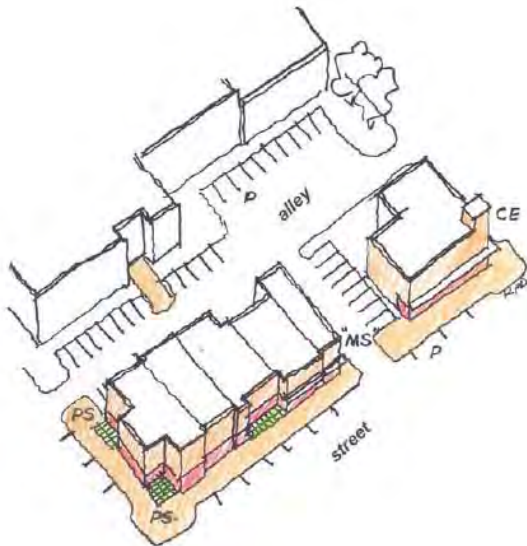


Figure 3-4 Old Town Block Façade Example

-
- D. Mixed-use development will not be required to have ground floor commercial uses on the building side facing Dyes Inlet.
 - E. No front or side yard setbacks will be required for new and remodeled buildings.
 - F. Reserve front yards, if any, for landscaping or outdoor consumer-related activities such as sidewalk cafes, coffee shops, restaurants, wine bars, etc., and when provided, do not exceed 12 feet in depth.
 - G. Provide pedestrian weather protection elements (awnings, marquees) where feasible along the front street/sidewalk area, at least 8 feet above the sidewalk.
 - H. New, remodeled or reconfigured buildings in the NC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 45 feet. In the RC zone, the maximum base height will be 35 feet, with an approved height increase not to exceed 65 feet.
 - I. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks.



Figure 3-5 Example Mixed Use Development

3.4.3 STREETS & PARKING

- A. No on-site parking requirements, excluding residential uses, for commercial and office uses will be required when a parking development fee, determined by a Business Improvement District (BIA) or, as an alternative, a “fee-in-lieu” program, is paid to a shared-parking fund.
- B. Land can be leased or purchased for shared parking lots using BIA funding mechanisms, as permitted by Washington State.
- C. Off street shared parking lot(s) with a use radius of 800 feet may provide consumer and employee parking for non-residential uses as a means to reduce or eliminate on-site parking.
- D. Parking lots will be landscaped with a 30 inch high minimum vegetated edge or buffer between the lot and adjacent sidewalk(s) or with a tree landscaped setback or both.
- E. On-site parking lots should be located to the rear or side yard or both, accessed by an alley driveway where feasible or by a side yard with a maximum 18 foot wide access drive.
- F. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.

-
- G. Driveway aprons on private property will not be required for alley access parking, enabling private parking space or garage to locate closer to the alley right-of-way line.
 - H. On-street parallel parking is allowed and encouraged on all streets.
 - I. Provide sidewalks on both sides of all streets, where possible.

3.4.4 STRAWBERRY CREEK PROTECTION & ENHANCEMENT

- A. The Strawberry Creek channel is contained by concrete walls and security fencing with little or no riparian area associated with the creek until its confluence with Dyes Inlet. The creek corridor should be opened and expanded to reduce the intensity of water flow and to expand the open space feature of the creek as a major physical identity or signature element of Old Town and Silverdale. Two examples for slowing the water flow and creating a physical amenity include:
 - a. Expanding the creek riparian area horizontally into Linder Field and along the southern edge of the creek, including east of Washington Street.
 - b. If horizontal expansion is restricted, a series of pools as a part of a larger cascade can be developed with a controlled spill way to hold water and release it more slowly into the lower creek, while providing fish access up the cascade.

See Figure 3-6 *Strawberry Creek Slough* as an example.

- B. A transparent security and safety barrier can be provided along the creek, specifications to be determined by appropriate engineering studies.
- C. Strawberry Creek and the Linder Field recreational facility comprise an open space corridor and amenity for the Old Town community, which are improved with a creek-side trail, informational signage and viewing areas.
- D. Strawberry Creek is a fish habitat to be protected and enhanced with side pools, riparian area shading vegetation, a gravel creek bed and other elements determined by fisheries biologists.
- E. The confluence of Strawberry Creek and Dyes Inlet should be a public access point and attraction with public access and viewing area.
- F. The expanded creek riparian area should be incorporated into new development on both the school administration site and library site with trails, viewing platforms and seating areas along the creek, with pedestrian connections from adjacent street to the creek area.

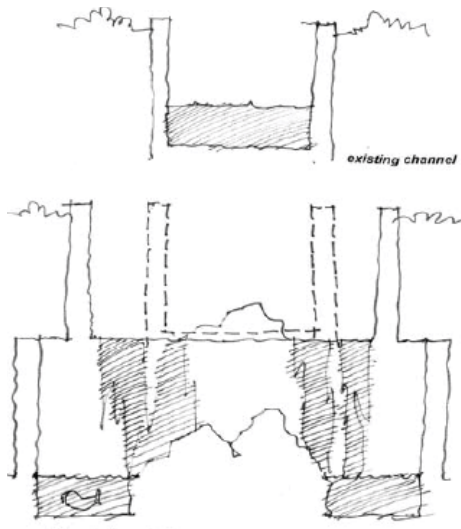


Figure 3-6 Strawberry Creek Slough



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

APPENDIX C: MEETING NOTES

PORT OF SILVERFDALE – RICE FERGUS MILLER – WALKABOUT – 1/12/2018

ITEMS:

1. Mission - Planning for entire waterfront area South of Byron
2. North end Pacific – clean up, future development
3. Wetlands – Potential boardwalk, possible rowing shell storage
4. Boat ramp – maintenance, usage
5. Parking – RCO constraints, parking issues
6. Grassy Area – recreation, public use
7. Beach between boat ramp and pump station – soft beach protection
8. Fixed pier and floats – repairs and upgrades
9. Dredging – 2018 – boat ramp and floats area
10. Non-motorized watercraft – launch, floats, usage
11. Sewer Pump station – history, change
12. Kitsap utility upgrades – easements requested, streetscape
13. Upland parking – history and issues
14. Upland usage:
 - a. Commercial opportunities
 - b. Boater oriented
 - c. Public gatherings
 - d. Public entertainment venue
 - e. History preservation
 - f. Festivals
 - g. Farmers Market
15. Old Town Planning
 - a. DCD
 - b. CKSD
 - c. Citizens Advisory Committee (CK and Port)

- CITIZENS ADVISORY
-
-
-

Port of Silverdale

Minutes of Regular Meeting

January 18, 2018

1. CALL TO ORDER

Commission chairman Henry Aus called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Lawrence Greaves; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Greg Jacobs and Steve Trunkey of the Kitsap Sailing and Rowing Foundation (KSRF); Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Elizabeth Barnes, Port tenant; Randy and Marvel Hunt of the Central Kitsap History Club; and Steve Rice of Rice Ferguson Miller Architects.

1.1. Agenda was approved as submitted (motion by Greaves, second by Scholfield, unanimous).

1.2. Regular meeting minutes of the December 21, 2017 Meeting were approved as read (motion by Greaves, second by Scholfield, unanimous).

1.3. Election of Officers – Commissioner Greaves was elected to serve as Chairman and Commissioner Scholfield was elected to serve as Secretary (motion by Scholfield, second by Aus, unanimous).

2. PUBLIC COMMENT

2.1. Port tenants – Elizabeth Barnes, tenant of 9020 Washington Avenue, explained that she was at the meeting on behalf of some of the Port tenants on Lowell Street. She said that several of the businesses rely heavily on foot traffic. Unfortunately with the Silverdale Way Widening Project, the normal foot traffic is at a minimum and some of the businesses are struggling to stay afloat. It was hoped the Port Commissioners would consider at least a twenty-five percent reduction in rent for those businesses. She went on to explain that her

business and the 3295 NW Lowell Street tenant's business is a bit more established than the 3332 and 3330 NW Lowell Street businesses. They are affected by the construction, but at this point aren't struggling as bad as the other two businesses. She would like to pass any possible reduction in her rent onto the other two businesses although at some point she may need to be considered as well. Commissioner Aus said that it is really bad right now because of all the construction and it's going to get worse when the Bayshore Project begins later in the year, but when it's over it's going to be extremely nice. So if the rents were reduced at this time maybe they should then be increased once the projects are completed. Commissioner Greaves agreed that it is inconvenient, but it's a County problem that even affects the Port. Randy and Marvel Hunt live on the west side of Lowell, so they have a front row seat to all the traffic and construction. Ms. Hunt said that some of the issues she has seen would be lessened if there was better signage, possibly detour signs. Ms. Barnes explained that several Old Town business owners have met with County Commissioner Ed Wolfe about the problems. Unfortunately, she was only given a one-day notification about the meeting so was unable to attend. One outcome of that meeting was that the County agreed to pay for a few "Old Town is Open for Business" banners. Ms. Barnes was disappointed that the banners didn't include the logos of Old Town businesses. She asked the Port Commissioners to consider holding a meeting with County Commissioner Ed Wolfe. It was agreed that the Port could host a meeting. Phil suggested that before a meeting is scheduled the business owners should make a list of their wants/needs from

Port of Silverdale – Minutes of Regular Meeting on January 18, 2018

the County. Commissioner Scholfield suggested they also approach the County at that time about the installation of a four-way stop at Lowell and Washington. Ms. Barnes agreed.

2.2. Whaling Days – Ms. Larson completed the Use of Port property agreement for Whaling Days 2018.

Steve Rice thanked the Commissioners for choosing his firm to prepare the Waterfront Architectural Design. Steve and Ron Easterday recently walked the Port waterfront area with Phil. Phil explained that he received the proposal from Rice, Fergus and Miller. It was very lengthy with a lot of unnecessary information, so Phil generated a Professional Services Agreement to be signed by Mr. Rice and the Port's Chairman. The agreement was reviewed. *It was agreed to enter into the Professional Services Agreement with Rice Fergus Miller, Inc. dated January 18, 2018* (motion by Aus, second by Scholfield, unanimous). Commissioner Greaves signed the agreement. Mr. Rice said that at this point they are working on solidifying some of the ideas that the Citizen's Advisory Committee generated. He has a meeting scheduled tomorrow with Angie Silva, the Senior Policy Analyst to the County Commissioners. Among other things he plans to update the County on the Port's plans to have his firm prepare a waterfront design and further discuss the County sewage pump station that sits on prime waterfront land. His firm will provide monthly updates on their progress.

3. UNFINISHED BUSINESS

3.1. Port Programs

a. Sailing – Steve Trunkey reported that a

sailing regatta is scheduled for March 3rd. In order for his team to be competitive by practicing they will need the sailboat float installed by February 24th. There is still some maintenance needed on the floats. Commissioner Scholfield said that Tim is aware of it and will be working on it. Mr. Trunkey said that the district championships are scheduled for May 5th and 6th. This will bring in a lot of people from out of town.

Greg Jacobs asked that the Port consider hiring a part-time employee to help out on the docks during the summer. He/she could be responsible to ensure boaters are paying, help them tie up, answer any questions and basically be the face of the Port. Last year a Safe Security guard was hired mainly to help direct individuals where to park, which everyone agreed made a positive impact. Safe Security will be notified that the Port would like to contract the "Port Host" position again this year. The job outline may be increased to include walking the docks and assisting boaters. Mr. Trunkey said that with the growth of the program, he believes it cannot sustain by using volunteers alone, eventually someone will need to be hired. Mr. Jacobs added that if they lost Mr. Trunkey the program would be in bad shape. Phil questioned if they have considered working towards creating a paid position. Mr. Jacobsen explained that they need sustained income before they can hire someone and since it is technically a Port program they hope the Port would eventually consider hiring an employee to work the program. Phil suggested they write down their goals and the necessary steps to meet those goals and bring that information to a future Port meeting for further discussion.

Port of Silverdale – Minutes of Regular Meeting on January 18, 2018

b. Rowing – Tim has plans to clean up the back area behind the Port office so Clam Island Rowing (CIR) can expand their fence. Phil received an e-mail from Bridget explaining that they are applying for a Recreation Conservation Office (RCO) grant through the “No Child Left Inside” program. She requested the Port provide a letter of support. Phil will prepare a letter for Commissioner Greaves’ signature. Commissioners Aus and Scholfield were provided copies of the e-mail so that they can send personal letters of support.

c. Leasehold Excise tax – Dave Gebauer the Washington State Department of Revenue (DOR) auditor determined the fair market value of the equipment leased to KSRF and CIR, which in turn determined the amount of leasehold/excise tax due from 2014 through 2017. He used the 2017 equipment values for each program from information provided to the Port’s insurance company; multiplied it by an 8% rate of return; divided that figure by two (since it is a seasonal operation he considered that the equipment was used for approximately six months out of the year); divided that figure by twelve to get the monthly fair market value; then multiplied by the leasehold excise rate of 12.84%. From 2014 through 2017 KSRF’s amount due totaled \$1,163.30 and CIR’s totaled \$821.76. Mr. Gebauer said that the Port doesn’t have to charge the fair market value but the leasehold excise tax must be paid on the fair market value. He also said that the Commissioners could decide to pay the leasehold excise tax on behalf of the entities since they do maintain the boats and spend hundreds of hours working the programs. Phil suggested the entities to stop gifting the equipment to the Port because the Port can

receive gifts, but cannot give gifts and technically the entities are purchasing the equipment with their own money and gifting them to the Port. Phil also said that maybe a lease isn’t the appropriate agreement. It was suggested Phil contact Kitsap County Prosecuting Attorney, Jacqueline Aufderheide, who co-founded the program and encouraged the Port to take it over from the County. Phil will reach out to Ms. Aufderheide to further discuss the situation.

3.2. Port Facilities

a. Dredging – tabled.

b. Sailboat Float – the meeting with Logan of Marine Floats has not yet taken place. Commissioner Scholfield said that he doesn’t think that there was a design flaw with the floats but rather they weren’t bolted down appropriately during the install.

c. Ladders – Tim provided the ladder specs via e-mail. Phil said that ladders can be ordered. It’s not necessary to fabricate them. Phil will talk further with Tim.

d. Center Timber Reinforcement Project – an estimate from Tim was reviewed. It totaled \$23,157.05. The Commissioners agreed that this is a maintenance project as it is maintaining the integrity of the docks and pier. ***Tim Knapp was authorized to perform the Center Timber Reinforcement Project as outlined in the TIKAR Service estimate 2018-1016 dated January 17, 2018*** (motion by Scholfield, second by Aus, unanimous).

e. Maintenance Inventory of Pier – tabled.

f. Port Rules – tabled.

Port of Silverdale – Minutes of Regular Meeting on January 18, 2018

3.3. Port Properties

a. 3215 NW Lowell Street

Suite 161/ACE Professional Services –

rent has not been paid for December or January. The tenant has been informed via e-mail and verbally over the phone. The lease has expired, so is currently a month-to-month. Commissioner Scholfield requested that the tenant be notified that if December, January and February rent is not paid by February 1, 2018 the lease will be terminated. A letter will be generated.

Suite 191/Attorney Deborah Lyons

called the Port office complaining about the noise caused by the contractor refinishing the upstairs office floors. She said that it interfered with her work and requested a reduction in her February rent. Mike explained that the work started on the 17th and is due to be finished on the 19th. Next week Suite 261 is scheduled to be painted. He agreed that it was very loud and between the noise and the chemical smell he agreed it interfered with the tenant's being able to conduct business. Ms. Lyons is the only tenant thus far who called in a complaint to the Port office. *It was agreed to grant a one-time \$50 reduction in rent to Deborah Lyons the tenant at 3215 NW Lowell Street, Suite 191* (motion by Scholfield, second by Greaves, unanimous).

Suite 261/Counselor Christine Wylde

has signed a lease effective February 1, 2018. Mike reported that the suite should be ready by that day and is optimistic that the Occupancy Permit will be in place.

b. 9004 Washington Avenue/DRC - Mike reported that Faithe Lester of the DRC invited him to a meeting scheduled for tomorrow morning. Mike has been waiting for the DRC

to provide him with more details of their remodel/upgrade plans. He plans to attend the meeting.

c. 3255 NW Lowell Street/Caties Creations isn't entering into a new lease at this time. She requested to be on a month-to-month as she is considering closing or moving her business to her home.

d. 3421 NW Byron Street/Bilingue – tenant came into the Port office to sign her lease. She mentioned that she has heard that another bilingual school is attempting to re-locate to the Old Town area and even possibly attempting to rent from the Port. She is a bit concerned because this could negatively interfere with her business. Phil explained that Connie Faulkner, who was a previous Port tenant, came into the Port office asking if there was any space available. Ms. Faulkner later called the Port and asked about the Port's future construction plans in Old Town. She was invited to attend a Port meeting and may possibly be attending in February. Commissioner Scholfield said that he feels the Port has an obligation to its current tenants at least giving them the first right of refusal to any new construction especially if they were forced to temporarily move because of the construction.

e. 3473 NW Byron Street/Old Town Pub – a hand written letter dated January 1, 2018 from former tenant, Fred Lanouette, was reviewed. His lease with the Port was prematurely terminated once the building was condemned by the County. The business was forced to close on November 3, 2017 although the lease was through October 21, 2018. Mr. Lanouette explains in the letter that his estimated loss of revenue from the date of

Port of Silverdale – Minutes of Regular Meeting on January 18, 2018

closure through the date the lease was scheduled to end was approximated at \$74,764.00. A spreadsheet showing the Pub's income and expenses for January through October 2017 was included with the letter. Commissioner Scholfield said that the lack of maintenance to the building over the last forty years, the duration of Lanouette's ownership of it, attributed to the County condemning it. Commissioner Greaves asked Phil if there was anything in the purchase and sale agreement that covers this issue. Phil will look into it further.

Marvel Hunt asked about the motor home that is parked behind the Pub building. It is believed a previous tenant, Mario, purchased the motor home from Mr. Lanouette. It is supposed to be moved soon. Ms. Hunt explained that Mario had provided them with an old newspaper that was in the Pub. He thought since they were part of the history club they would appreciate it, which they do. Ms. Hunt asked that if any other historical items are found in the building that the Port also consider the Central Kitsap History Club.

3.4. Olympic Outdoor Center (OOC) – the meeting with John Kuntz has not yet taken place. Phil reported that Mr. Kuntz stopped by the Port office to discuss the float that he would like to use from the Port's facility. He also talked to Phil about the Port possibly having a separate facility in the future that would accommodate non-motorized sports recreation. Mr. Kuntz believes it would set Silverdale apart. Commissioner Scholfield plans to look up OOC's website and possibly go to their Poulsbo or Port Gamble operation to get an idea of what their needs are for the Silverdale location. Phil reminded everyone that the

Recreation Conservation Office (RCO) will need to approve anything that will be attached to the pier and/or floats. He suggested the architects be asked to help determine the degree of need for a non-motorized facility.

3.5. Waterfront Architectural Design Project – was discussed during Public Comment.

3.6. Bayshore Drive Improvement Project – a letter dated January 4, 2018 from Phil to Rob McGinley of Kitsap County Public Works was reviewed. Phil was asking for clarification about the County's easement requests. Mr. McGinley has since stopped by the Port office and talked to Phil. The County originally requested temporary construction easements but changed the request to permanent right-of-ways. Mr. McGinley told Phil that the County will be meeting in early February to further discuss what exactly they will need. Barbara Zaroff from Kitsap County Public Works Department dropped off a set of the most up-to-date plans for the project. She said they were a ninety percent complete draft. Phil told Ms. Zaroff that the underground utilities might be an issue; she agreed and said they are still working on that.

Bob Ling the owner of Silverdale Auto Works approached Commissioner Scholfield about the possibility of his employees parking in the Port's vehicle parking lot during the construction of the new building. Mr. Ling said that the employees could put stickers on the cars to identify the vehicles associated with the shop, if necessary. There would not be any overnight parking. Commissioner Scholfield didn't see a problem with it, other businesses do it all the time, they just have never asked.

Commissioner Scholfield received a complaint from the tenant at 3425 NW Byron Street/Kitsap Art because they have a few evening events that draw a crowd and the patrons end up parking in the Port's vehicle lot. There have been times when the vehicle lot is full from Cash Brewery patrons.

4. NEW BUSINESS

4.1. Sea Scouts are planning to moor the Charles N. Curtis, an 80' vessel, at the Port on Saturday, January 20, 2018.

4.2. Central Kitsap Community Council (CKCC) representative, Phil Havers, e-mailed Phil asking if a Port Commissioner would make a presentation to the council regarding the future Old Town plans. Phil responded to Mr. Havers telling him it was a bit premature, but maybe something could be set up for April or May.

4.3. Silverdale Chamber of Commerce is preparing the 2018 walking map and has asked for the Port's participation as in years' past. *It was agreed to place a 1/8 page ad on the front cover of the 2018 Silverdale Chamber walking map totaling \$350* (motion by Scholfield, second by Aus, unanimous).

5. SAFETY – nothing to report.

6. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$26,076.10, checks numbering 11646 through 11674 and Electronic Transfer 2018-01 to the U.S. Treasury in the amount of \$1,871.16, were approved (motion by Aus, second by Scholfield, unanimous).


7. EXECUTIVE SESSION – None

8. ADJOURN

The meeting adjourned at 9:22PM (motion by Aus, second by Scholfield, unanimous).

Approved:


Henry Aus, Commissioner


Lawrence Greaves, Commissioner


Ed Scholfield, Commissioner

MINUTES OF SILVERDALE PORT MEETING ON FEBRUARY 6, 2018

A Special Meeting of the Silverdale Port Commission was called by Chairman Greaves to review the County's request for easements on Port property. The Waiver of Notice of Special Meeting was signed by all three Commissioners and a notice of the Special Meeting was taped to the Port office door a day prior to the meeting, both of which are attached hereto. The meeting started at 11:00AM at the Port office. Also in attendance were Commissioner Aus; Commissioner Scholfield; Port Attorney Phil Best; Port Administrator Theresa Haaland; and Rob McGinley of Kitsap County Public Works.

Mr. McGinley provided handouts that showed the County's intent to improve and in some cases install sidewalks along streets in Old Town. To do this the County is requesting permanent easements on several properties including Port owned property. The Port Commissioners have concerns over some of the requested easements and asked Mr. McGinley to attend the meeting today and take a walkabout to view and answer questions regarding the affected Port properties. At 11:10AM the walkabout began.


At 12:00PM everyone arrived back at the Port office.

The proposed easements at 3473 NW Byron Street and 9020 Washington Avenue will go nearly to the buildings on those properties. Mr. McGinley explained that the County generally requests an additional five feet from the sidewalks to allow room for utilities.

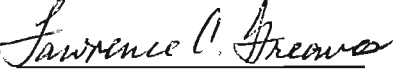
The proposed drawings for acquisition of Port property along the south end of Washington affecting the Port's parking lot areas were a major concern because it will interfere with the Port's utility and network systems. Commissioner Scholfield suggested the County reconsider the installation of a sidewalk in that area because it is very costly, affects the Port, and it would be more of a liability than a benefit to the public. Phil agreed. Mr. McGinley offered to talk about this issue with Gunnar Fridriksson the lead on this project. It was also suggested both Mr. Fridriksson and Dori Leckner of County Parks be invited to the Port's February meeting.

The stakes at 3332 NW Washington were not ten feet apart as outlined in the drawing. Mr. McGinley explained that originally the County was asking for a larger area for the easement. He believes it was staked using the original plans. He will inform Gunnar that it is staked incorrectly.


The meeting adjourned at 12:20PM



Commissioner



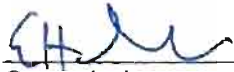
Commissioner



Commissioner

WAIVER OF NOTICE OF SPECIAL MEETING

The undersigned, Port Commissioners for the Port of Silverdale, hereby waive the requirement of notice in writing of the special meeting of the Port of Silverdale held on 2/6/2018, at 11AM Port office, is present at such meeting, and agrees to the conduct of the Port business as announced by the President in calling this meeting.



Commissioner



Commissioner



Commissioner



**KITSAP COUNTY
SILVERDALE PUMP STATIONS NOS. 3, 4, AND 19 UPGRADES DESIGN**

**STAKEHOLDER MANAGEMENT MEETING #1
MEETING MINUTES
February 27, 2018 at 10:30 A.M.
PUBLIC WORKS BUILDING, 3rd FLOOR LIBRARY**

Attendees:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Barbara Zaroff | <input checked="" type="checkbox"/> Dori Leckner |
| <input checked="" type="checkbox"/> Stella Vakarcs | <input checked="" type="checkbox"/> Angie Silva |
| <input checked="" type="checkbox"/> Lisa Horanyi | <input checked="" type="checkbox"/> Ron Easterday |
| <input checked="" type="checkbox"/> Rebecca Pirtle | <input checked="" type="checkbox"/> Tony Fisher |
| <input checked="" type="checkbox"/> Phil Best | <input checked="" type="checkbox"/> Carla Talich |
| <input type="checkbox"/> Steve Rice | <input checked="" type="checkbox"/> Andrew Nelson |

Discussion:

The following is our understanding of the subject matter covered in this meeting. If this differs with your understanding, please notify us. Minutes in bold denote an action item or decision.

1. Introductions

- a. Barbara Zaroff – Project Manager, Kitsap County Sewer Utility Division
- b. Stella Vakarcs – Senior Program Manager, Kitsap County Sewer Utility Division
- c. Lisa Horanyi – Education Outreach Coordinator, Kitsap County Sewer Utility Division
- d. Andrew Nelson – Public Works Director, Kitsap County
- e. Dori Leckner – Parks Superintendent, Kitsap County Parks and Recreation
- f. Rebecca Pirtle – Communications and Volunteer Services Coordinator, Kitsap County Commissioners' Office
- g. Angie Silva – Senior Policy Analyst, Kitsap County Commissioners' Office
- h. Phil Best – Port of Silverdale
- i. Ron Easterday – Principal Architect, Rice Fergus Miller (representing the Port of Silverdale)
- j. Tony Fisher – Project Manager, BHC Consultants
- k. Carla Talich – Principal Engineer, BHC Consultants

2. Opening discussion:

The project includes upgrading four pump stations due to aging equipment, capacity limitations, and noncompliance with current design standards. Pump stations (PS) to be

upgraded include PS 3, PS 4, PS 19, and PS 31. Collection and conveyance system improvements are also needed for PS 3 and PS 4.

The focus of the meeting is on stakeholder involvement for PS 3 Upgrade Project because of its location at Silverdale Waterfront Park, a site with multiple uses that will draw interest from a wide variety of stakeholders beyond the County's Wastewater Utility Division. Many stakeholders have already been identified, including the public, Parks Department, Port of Silverdale, users of the park for the Farmer's Market and Water Trails events, community groups, regulatory agencies, and others. An initial stakeholder list has been developed and will be shared with the meeting attendees for review and update. It will be important to continue looking for others that may be interested in the project.

The Port of Silverdale is updating their Master Plan and the plan will address the Silverdale Waterfront Park. There is an opportunity to work together with the Port to plan for the project through stakeholder and public outreach. The Port has a project scoping meeting scheduled for March 9.

3. Power Point Presentation by BHC (attached)

a. Overview of project

i. Locations and background information were presented for the planned PS and conveyance upgrades.

ii. Discussion:

1. PS 3 Upgrades:

a. Odor control – Odor issues may be related to PS; however, the low tide/saltwater environment may also be a significant contributor, especially in the summer when temperatures are warmer. Minimizing odor impact is necessary, but it may also be a natural occurrence. This will need to be evaluated during planning and design and communicated with the stakeholders.

b. Facility improvements and site options:

- i. Potential launch site for kayaks
- ii. Access in and out of the site needs improvement
- iii. Dumpsters need to be relocated
- iv. People will use the building and area around it
- v. Public restrooms are not desirable for Operations Staff; existing restrooms are available nearby
- vi. Facility should fit in as part of the property and old town character of the area
- vii. Visual/Open area is preferred
- viii. Port of Silverdale wants County to consider remote electrical service (e.g., Keyport PS) to possibly eliminate the onsite building
- ix. Ability to operate and maintain the facility is important for parks and sewer utility staff

- x. Beach armoring needs to be considered. Siting the PS further from the shore may alleviate the need for significant upgrades to beach armoring. Needs for bank protection will be considered and communicated with regulatory agencies.
 - c. Facility aesthetics/architecture were reviewed:
 - i. Rendering from the 2015 Alternatives Analysis
 - ii. Snohomish CSO Pump Station
 - iii. North City Pump Station
 - iv. Renderings of buildings and screening from design team
 - 2. PS 3 Conveyance Upgrades – Street disturbance was discussed. Washington Avenue will be disturbed during the Bay Shore Pipeline project. Tony and Barbara clarified that Washington Avenue (near NW Byron Street) is not planned to be disturbed again for the conveyance upgrades associated with PS3.
 - 3. PS 4 Upgrades – Additional items that are anticipated to be needed include: side walk connections, screening the PS from the roadway and adjacent properties (fencing or vegetation), and odor control (bio-filter bed or scrubber).
 - 4. PS 19 – A new traffic signal is planned at this location. BHC is aware of this and coordinating with Floyd Bayless.
 - b. Objective of public outreach program
 - i. General objectives: stakeholder and public outreach to present project purpose and design concepts, solicit input on how the property is and will be used, gather input related to PS aesthetics, and address stakeholder concerns.
 - ii. Discussion:
 - 1. Education – It will be important to inform public customers why a PS is required and why it needs to be upgraded.
 - 2. Signage at the park will be important. Signs should include an address to the project website where information and updates about the project are available.
 - c. Outreach tasks:
 - i. **Visioning – Develop a survey to introduce the project to the public and stakeholders and solicit input (see Draft Survey at the end of this document)**
 - ii. Stakeholder meetings (periodic throughout planning and design)
 - iii. Public meetings (periodic throughout planning and design)
 - iv. Booth materials for events (e.g., Cards distributed at Whaling Days, Farmer’s Market, etc.)
 - v. Informational brochures
 - vi. Materials will be available for posting on the project website
 - vii. Seek opportunities to combine similar efforts with the Port of Silverdale, such as holding joint meetings.

4. Stakeholder management approach

i. Stakeholder Identification –

1. Key stakeholders were identified as the agencies represented at this meeting. This group would coordinate to develop the Stakeholder Management Plan (authored by BHC). The title of the document may be renamed, “Stakeholder Outreach Plan”.
2. Draft list of All Stakeholders –
 - a. **BHC will submit to Lisa**
 - b. **Lisa will coordinate within the County to provide comments to BHC**
 - c. **BHC to provide updates**
 - d. **Lisa will distribute the Stakeholder List to the Key Stakeholders for review and comment**
3. It was noted that some groups within the stakeholder list have multiple representatives. It will be helpful to ask these groups to pick one or two spokespersons or leads that can make sure their group is receiving the information and providing their feedback.
Lisa will take the lead on this.

ii. Stakeholder Management Strategies

1. Stakeholder expectations – Gather input and analyze the impact on the project
2. Develop strategies for engaging stakeholders in project decisions
3. **County to share examples of prior projects (e.g., Bucklin Hill Bridge project)**

iii. Controlling Stakeholder Engagement

1. How will feedback be solicited? **Refer to item 3c. Outreach Tasks**
2. How will feedback be processed? **This will be further discussed by Key Stakeholders and documented in the Stakeholder Management Plan.**
3. Monitoring stakeholder involvement. The Stakeholder Management Plan will need to document stakeholder input/comments/feedback and responses.
4. Adjusting stakeholder involvement:
 - a. Periodic review and update of the Stakeholder List to check that interested parties are included. Stakeholder Management Plan will identify frequency for making updates (every quarter may be appropriate).
 - b. Schedule meetings to fit stakeholder availability.
 - c. Make sure meeting invitations and project updates information are reaching everyone on the stakeholder list.
 - d. Provide multiple opportunities for stakeholders to provide feedback.

5. Schedule

- i. A rough schedule for stakeholder management is included in the power point presentation. This will be revised as part of the Stakeholder Management Plan.
- ii. Planning, design, permitting, and construction of the project will cover a period of approximately 5 years. Public outreach related to construction is not needed at this time.

6. Next steps

- i. **Complete the Stakeholder List (All)**
- ii. **BHC to Prepare the Draft Stakeholder Management Plan for review by Key Stakeholders**
- iii. **Kick-off the Stakeholder Outreach with a survey (see draft below for review by All)**
- iv. Communications within the Key Stakeholder team can occur. Carbon copy Barbara on email correspondence.
- v. Port of Silverdale Master Plan meeting March 9, 2018
- vi. Next Key Stakeholder Meeting to be scheduled in about one month (April 2018)

Draft Stakeholder Outreach Survey

- Project Explanation Statement: Describe the purpose of the pump station, need for upgrades, and opportunities to provide input.
- Questions:
 - How do you currently use the park property? (check all that apply)
 - Walking
 - Biking
 - Children's Playground
 - Farmer's Market
 - Swimming
 - Kayaking
 - Other, fill in the blank _____
 - What is your favorite thing about the park?
 - fill in the blank _____
 - What time of year do you visit the park? (check all that apply)
 - Spring
 - Summer
 - Fall
 - Winter

- What part of the week do you typically visit the park? (check all that apply)
 - Weekdays
 - Weekends

- What time of the day do you typically visit the park? (check all that apply)
 - Morning (9AM to noon)
 - Afternoon (noon to 4PM)
 - Evening (4PM to close)

- What do you feel is needed at the park property? (check all that apply)
 - Better Lighting
 - Launch site for kayaks
 - Landscaping improvements
 - Site access
 - Meeting space (indoor)
 - Meeting space (outdoor)
 - Other, fill in the blank _____

- For the Upgraded Pump Station facility, what is important to you?
(check all that apply)
 - Necessary facilities held behind a fence
 - Necessary facilities held within a building
 - Landscaping to provide screening
 - Areas on or near the facility to display artwork
 - Kiosk to communicate upcoming events
 - Area of the building/enclosure that can be used by the public
 - Noise control
 - Odor control
 - Affordable and practical to maintain

- What theme do you prefer for the new facility?
(Ron to provide images for aesthetics)
 - Old Town Silverdale
 - Native American
 - Modern
 - Other, describe _____

- How would you like to receive project updates?
 - Online Survey
 - Via email through the Project Website
 - Public meeting during the weekday
 - Public meeting during the weeknight
 - Onsite open house during the weekend

- What setting do you prefer for providing your ideas and feedback about the project?
 - Online Survey
 - Via email through the Project Website
 - Public meeting during the weekday
 - Public meeting during the weeknight
 - Onsite open house during the weekend

MINUTES OF SILVERDALE PORT MEETING ON MARCH 9, 2018

A Special "Workshop" Meeting of the Silverdale Port Commission to discuss the Waterfront Design Project was held at 3:00PM at the offices of Rice Fergus Miller, 275 Fifth Street, Suite 100, Bremerton. The Waiver of Notice of Special Meeting was signed by all three Commissioners and a Notice of the Special Meeting was taped to the Port office door a day prior to the meeting, both of which are attached hereto. At 2:30PM Commissioners Aus, Greaves and Scholfield along with Port Administrator Theresa Haaland accompanied Attorney Phil Best in his vehicle to attend the meeting.


Steve Rice and Ron Easterday of Rice Fergus Miller and Ben Anderson of Art Anderson Associates met with Port of Silverdale representatives.

Mr. Rice provided an update of the firm's work thus far. Several ideas were discussed. He explained that he was aware that the Port had hired an engineer to provide a structural analysis of the building, but the engineer didn't go into the crawl space as he deemed it too dangerous. It seems that the analysis was therefore left inconclusive. Mr. Rice suggested a structural engineer used by Rice Fergus Miller be hired to provide a new consult. The Commissioners agreed. Mr. Rice will provide Phil with a proposal.

Mr. Rice requested a meeting be set up with the Port's Citizen Advisory Committee to touch base with the members and discuss the status of the Waterfront Design Project. The meeting was scheduled for March 26, 2018 at 3:00PM at the Port office. Notice will be e-mailed to the members.

Rice Fergus Miller representatives also plan to attend the upcoming Central Kitsap Community Council meeting scheduled for May 2, 2018 at 5:00PM at the Best Western Silverdale Hotel to provide council members with an update of the plan and answer questions.

At 5:35PM everyone arrived back at the Port office. The meeting adjourned.



Commissioner

Commissioner

Commissioner

WAIVER OF NOTICE OF SPECIAL MEETING

The undersigned, Port Commissioners for the Port of Silverdale, hereby waive the requirement of notice in writing of the special meeting of the Port of Silverdale held on March 9, 2018, at 2:30 PM, are present at such meeting and agree to the conduct of the Port business as announced by the President/Chairman in calling this meeting.

Lawrence P. Greaves

Commissioner

Hyun Sun

Commissioner

E.H. [Signature]

Commissioner



TRANSITIONAL

OLD TOWN GRID

**WATER DEPENDENT /
WATER ENJOYMENT**

WHAT DO YOU CURRENTLY DO AT PORT OF SILVERDALE FACILITIES/OLD TOWN SILVERDALE?

What activities do you currently use or partake in at Port Facilities?

(Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

- Attend Farmers Market
- Attend concert or activity sponsored by a local business
- Picnic or lunch break
- Pre-school Parent Group use
- Homeschool Parent Group use
- Attend Whaling Days
- Hike as the beginning /end of the Clear Creek Trail system
- Outdoor exercise on the lawn
- Launch your own kayak
- Rent kayak
- Launch your boat
- Tie up at pier and walk to local rest/coffee shop
- Moorage of watercraft
- If you use moorage services what services do you use: (check all that apply)
 - Shore power
 - Fresh water
 - Pump-out
 - Vehicle/trailer parking
 - Boat launch
 - Restrooms/showers
 - Fish from the pier.
- Walk out on the pier for relaxation/views
- Participate in Clam Island Rowing Club
- Participate in ASB or Kitsap Sailing and Rowing Foundation
- CK High School and Olympic/Klahowya sailing team use/NWISA.

What services adjacent to the Port do you use?

(Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

- Kitsap County Park – general day use
- Kitsap County Park Playground
- Kitsap County Park barbecues
- Kitsap County Park gazebo
- Kitsap County Park covered areas
- Kitsap County Park Restrooms/Showers/Laundry
- Old Town coffee shops/bakeries
- Old Town restaurants/lounges
- Old Town art galleries
- Old Town gift shops
- Other Old Town personal services _____
- Lodging
- Visit Old Town historical points of interest

HOW DO YOU USE THE FACILITIES?

When you visit the Port either by water or driving and wish to park once for the day, how far will you walk to access the nearby services/activities?

(Check one)

- 0-5 minutes
- 5-10 minutes
- 10-20 minutes
- Further than 20 minutes

When you visit the Port how do you get there?

(Rank 1 most frequent, 2 and 3 for 2nd and 3rd most used, 0 if never used)

- Drive auto

If arrive by auto, where do you park?

(Select all that apply)

- Port Parking lot
- On-street parking
- Other pay parking
- Other private business parking
- Do you utilize accessible parking stalls?
- Avoid going to the Port/Old Town Silverdale because I cannot find parking.

- Motorcycle
- Arrive as part of the Kitsap Peninsula Water Trail
- Arrive by personal watercraft
- Walk
- Bike
- Kitsap Transit
- Uber

When do you visit Port Facilities?

(Check all that apply)

- Summer only
- Year round
- Daytime
- Evening
- Rarely/never

How often do you visit Port facilities?

(check one)

- Once a year average
- Once a month average
- Once a week average
- More than once a week
- Rarely/Never

How long do you typically stay?

(Check one)

- Less than an hour
- 1-2 hours
- 2-4 hours
- Longer than 4 hours
- Rarely/Never

Who do you typically visit with?

(Rank 1 most frequent, 2 and 3 for 2nd and 3rd most used, 0 if never used)

- Myself
- Small group or family 2-4 persons
- Larger group or family 5-10 persons
- More than 10
- Rarely/Never

Port of Silverdale – Topics for Port Survey List

For inclusion in Kitsap County electronic survey with pump station survey

WHAT DO YOU THINK IS IMPORTANT?

Interest

(Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

- How important is having an active and useful Port facilities to you personally?
- How important do you think having an active and useful Port facilities is to Old Town Silverdale?
- How important do you think having an active and useful Port facilities is to the Silverdale community as a whole?

Level of Activity

(Y/N)

- Do you prefer Port services and activities as primarily quiet and passive?
- Do you prefer Port services and activities as primarily very lively and active?

WHAT DO YOU WANT TO SEE IN THE FUTURE?

What services would you like to see happen or would personally use if offered?

(Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

- More auto parking
- More boat trailer parking; if so what size?
- Electric vehicle charging stations
- Bike racks
- Motorcycle parking
- Kayak specific launch
- Kayak storage (secure)
- Rowing shell storage (secure)
- Rowing shell specific launch
- More motorized watercraft berths
- Guest laundry
- Sailboat storage (secure)
- Covered bandstand/performing arts area
- Camping – single night to support Kitsap Peninsula Water Trail visitors.
- Camping – General
- Foot ferry to WSF connections
- Foot ferry to other Kitsap County destinations
- Wetlands boardwalk
- Event center that can be rented for meetings, parties, weddings?
 - If yes, how many people and what services would you expect?
(Yes/No)
 - Up to 25 people
 - Up to 50 people
 - Up to 100 people
 - Over 100 people
 - Warming kitchen for a caterer or yourself to warm and serve food
 - Full commercial kitchen to cook and prepare food from scratch
 - Indoor Stage
 - Covered outdoor area
 - Other _____
- Public events _____?
- Grocery within walking distance.
- Lodging within walking distance.
- Other _____
- Other _____
- Other _____

What services would you not want to see offered or not take place?

What else would you like us to know?

Port of Silverdale

Minutes of Regular Meeting
March 15, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Tim Knapp of TIKAR Services; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Bob Bjornemo and Marvel and Randy Hunt of the Central Kitsap History Club; Faithe Lester of the Dispute Resolution Center (DRC); Connie Faulkner; and Kerri Horsley.

1.1. Agenda – *the agenda was approved* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The February 15, 2018 Regular meeting minutes were approved as corrected* (motion by Scholfield, second by Aus, unanimous).

2.2. *The March 9, 2018 Special “Workshop” meeting minutes were approved as submitted* (motion by Aus, second by Scholfield, unanimous).

3. PUBLIC COMMENT

3.1. Kerri Horsley introduced herself. She explained that she and her husband are interested in renting the 3255 NW Lowell Street building for a Montessori pre-school. Ms. Horsley was quoted \$1600 a month for the property, which is slightly higher than what the previous tenant was paying, but more in line per square foot with other Port properties. Ms. Horsley explained that she is aware that she will be required to obtain a change of use permit through the County and she is in the process of obtaining licensing through the State. She asked if a couple of the walls within the building could be removed and also the

landscaping in the front replaced with a fence to allow for a children’s play area. Connie Faulkner rented the building as a daycare years ago. She provided drawings of the layout at that time. Ms. Horsley would basically like to bring the building back to that design. It was explained that Edward Jones was a tenant after the daycare and they changed the layout of the building. Mike Vasquez said that if walls are being removed a major tenant improvement permit will be necessary. It was explained to Ms. Horsley that she would be responsible for any changes to the building with prior approval from the Port. Ms. Horsley asked if the rent could be reduced by half for two months in order to complete the necessary work and permits. *It was agreed to allow Ms. Horsley to rent the space at a reduced rate of \$800 per month for April and May and full rent of \$1600 beginning June 1, 2018. Discussion:* Phil informed Ms. Horsley that in keeping with the same schedule as other Port leases the Horsley’s lease would be through December 31, 2018. Ms. Horsley explained that she would prefer to have a longer lease. Phil suggested that the renewal be for an additional four years. *The motion carried unanimously.* In order to proceed, the lease will need to be signed along with a check for the last month’s rent (\$1600), damage deposit (\$500) and key change charge (\$100); totaling \$2200.

Phil cautioned Ms. Horsley, that just last week a resident at the end of Lowell Street came into the Port office to report an erratic driver along that end of Lowell Street. That is something to keep in mind with children in the area.

3.2. Faithe Lester of the DRC provided a building update list and proposed plan, which

were reviewed. Commissioner Greaves asked what the timetable would be on the project. Mike said that it would depend on the DRC as they will need to move out of the building for at least three weeks if everything goes as planned. Ms. Lester said that they have been discussing this and would consider as early as July and as late as September, with September being more ideal, but willing to do whatever is necessary. Mike rough estimated the costs to be at least \$30,000, which doesn't include new appliances as he thought that would be the tenant's expense. Mike had provided the DRC with an estimate last year, but explained that costs have increased substantially since then. Commissioner Scholfield said that a tenant improvement permit will be required. Phil explained that the financial responsibilities need to be determined. The tenant's responsibilities would then be amortized over a five-year lease. The DRC's current base rent is lower than other Port rentals so that will need to be considered as well. Ms. Lester asked if after the five years is it possible for the rent to actual decrease since the amortization would end. The Commissioners thought at that time the rent would probably need to be increased. Mike was tasked with preparing an updated cost estimate list for further review at next month's meeting. . Mike reported that there was no asbestos or lead paint discovered in the building.

3.3. Marvel Hunt explained that she has been writing down her thoughts and ideas about the future of Old Town for some time now and compiled them into a document, which she read aloud. Randy Hunt added that they reviewed the 2006 stakeholders feedback summary from the Comp Plan and it showed

many others had the same hopes for the future of Old Town. Ms. Hunt believes an overall vision for Old Town is missing and she sees the Port is in a tremendous position to lead in determining the vision. Ms. Hunt's compilation will be forwarded to Rice Fergus Miller for review and consideration. Everyone was informed that Rice Fergus Miller personnel are planning to attend a Citizen's Advisory Committee meeting on March 26, 2018 and then on May 2, 2018 Port representatives and Rice Fergus Miller personnel will be attending the Central Kitsap Community Council (CKCC) meeting for further discussion and update about the waterfront plans.

4. UNFINISHED BUSINESS

4.1. Port Programs

a. Sailing – an e-mail dated February 28, 2018 from Steve Trunkey was reviewed. They have recently replaced the 20hp engine as there is no longer a lower end on it. They thought they would place an ad on Craigslist asking for a donation for it or giving it to Clam Island Rowing, as they may have someone who can fix it. It was unknown what exactly happened to the engine. Since the engine is technically a Port asset, Mr. Trunkey will be asked to provide an incident report for the record and the Port's insurance company, Enduris, may be contacted to determine if it is covered. Mr. Trunkey will be informed not to proceed with the Craigslist ad, but since Clam Island Rowing is a Port Program they could have possession of it. Tim explained that in order for the Port to properly sell the engine it would need to go through Washington State's public surplus website.

b. Rowing – Phil reported that he talked with Bridget Burke about the extension of

Port of Silverdale – Minutes of Regular Meeting on March 15, 2018

Rowing Alley. Additional fencing is on order and she plans to contact Tim once they are ready to install it, so that he can remove the rest of the items in that area.

4.2. Port Facilities

a. Dredging – Phil reported that Marine Survey and Assessments is applying for the Joint Aquatic Resources Permit Application (JARPA). A portion of the proposed dredged area is outside of the Port's Department of Natural Resources (DNR) lease, so Phil contacted Don Olmstead of DNR and asked if the leased area could be expanded. Mr. Olmstead agreed. Commissioner Scholfield asked that the expanded area also include area west of the current leased area to include tidelands off Pacific Avenue. Phil will look into that. Phil reported that the Kitsap County Department of Community Development (DCD) agreed to waive the Substantial Shoreline Permit for the dredge. Phil contacted Recreation Conservation Office (RCO) personnel and confirmed that the Port is eligible to apply for grant funding for the 2018 round. The dredge would be on hold though because the window to perform the dredge is in August and grants aren't awarded until the end of the year for funding the following year. The JARPA may not be in place for the dredge to be completed this year anyway. Commissioner Greaves suggested that the Port proceed with the dredging for August 2018 regardless of grant funding, but apply for grant funding in case the 2018 deadlines aren't met.

b. Ladders – Phil provided Tim with the Notice of Award and Limited Public Works Contract for the fabrication and installation of the ladders. Tim said that the April 30, 2018 completion date was not enough time, so it

was changed to May 15th, which was penned into the contract. These items were signed.

Tim reported that the boat launch handling pier will be installed on April 2nd, weather permitting. He will begin the dock beams structural upgrade very soon.

c. Port Rules – sign has not yet been ordered.

4.3. Port Properties

a. 3255 NW Lowell Street/Caties Creations tenant vacated the premises on March 13th. The Silverdale Water District called the Port explaining that the water was going to be turned off if the account wasn't paid immediately. A check in the amount of \$146.14 has been processed for authorization at tonight's meeting. Since a lien could be placed on the property for the unpaid water bill it was agreed this would be paid. An e-mail prepared to be sent to the tenant was reviewed. It was agreed that she owes the Port \$373.31 and the e-mail will be sent to her tomorrow.

4.4. Pump Station 3 – Phil reported on the public outreach meeting he and Ron Easterday of Rice Fergus Miller attended at the County office. Several County employees were at the meeting. Phil urged County personnel to investigate the possibility of moving the pump station or at least the above ground electronics. Stella Vakarcs, Senior Program Manager of Sewer Utility, reiterated that it was just too costly to move the pump station. Phil reminded Andy Nelson, Director of Public Works, of the meeting he attended with Port Commissioners back in February of 2017 at the Keyport station, where he had agreed to at

Port of Silverdale – Minutes of Regular Meeting on March 15, 2018

least look into moving the above ground electronics of the pump station. Mr. Nelson said that they will need to conduct a study to that affect.

4.5. Silverdale Chamber Government Affairs Committee (GAC) – Commissioner Greaves recapped the meeting he attended on March 12th. An e-mail addressed to Commissioner Greaves, dated March 13, 2018 from Doug Newell, Executive Director of Business & Operations for Central Kitsap School District (CKSD) was reviewed. Mr. Newell also attended the GAC meeting and stated in the e-mail that CKSD is also starting to explore their future in Old Town Silverdale and he suspected there are some areas where CKSD and the Port could work together to improve it.

5. NEW BUSINESS

5.1. Argosy Cruises submitted an Application for Use of Port Property to pickup individuals on June 15th at 7:30PM and drop them back off on June 16th at 1:30AM. The Commissioners agreed to charge a one-time \$75 use fee and Commissioner Scholfield plans to be down at the dock upon Argosy's arrival, while they are loading. As a precautionary measure a Safe Security guard will be hired to be on the dock upon Argosy's return.

Accepting credit cards was discussed. It was explained that the State Auditor had cautioned Port staff about it. Phil said that other public agencies accept credit cards, including several local Port districts. This will be further investigated.

5.2. Dockwa an e-mail dated February 16, 2018 was reviewed. Dockwa is an online booking engine for marinas. The Port had no reservation requests in 2017, so Commissioners decided not to pursue it at this time.

5.3. Veterans Affairs outpatient clinic groundbreaking ceremony is scheduled for Monday, April 2, 2018 at 10:00AM at 9177 Ridgetop Boulevard.

5.4. Old Town Silverdale Listening Sessions – according to an email dated March 13, 2018 from the County, Commissioner Wolfe will begin having First Friday Listening Sessions in Old Town Silverdale every month (except July) from noon to 1:30PM at Monica's Waterfront Bakery.

6. **SAFETY** – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$34,974.36, checks numbering 11706 through 11732 and Electronic Transfer 2018-03 to the U.S. Treasury in the amount of \$1,709.02, were approved (motion by Scholfield, second by Aus, unanimous).

8. **EXECUTIVE SESSION** – at 9:01PM it was announced that the meeting would be going into Executive Session for approximately ten minutes to discuss potential litigation.

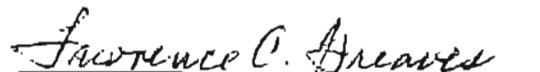
At 9:11PM the meeting returned to Regular Session.

9. ADJOURN

The meeting adjourned at 9:12PM (motion by Greaves, second by Aus, unanimous).

Approved:

Henry Aus, Commissioner



Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Port of Silverdale – Minutes of Regular Meeting on April 19, 2018

Port of Silverdale

Minutes of Regular Meeting

April 19, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Tim Knapp of TIKAR Services; Mike Vasquez of MTV Home Repair; Bridget Burke and Vanessa Allison of Clam Island Rowing (CIR); Greg Jacobs and Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Roy Sahali of Sahali Farms; John Kuntz and Forrest Wells of Olympic Outdoor Center (OOC); Monica Downen of Monica's Waterfront Bakery; Marvel and Randy Hunt of the Central Kitsap History Club; and Steve Rice of Rice Fergus Miller.

1.1. Agenda – *the agenda was approved* (motion by Scholfield, second by Greaves, unanimous).

2. CONSENT AGENDA

2.1. *The March 15, 2018 Regular meeting minutes were approved as corrected* (motion by Scholfield, second by Greaves, unanimous).

2.2. *Commissioner Aus' absence was excused* (motion by Scholfield, second by Greaves, unanimous).

3. PUBLIC COMMENT

3.1. John Kuntz introduced Forrest Wells, OOC's Program Director. Mr. Kuntz said that it is OOC's goal to move from the Port parking lot to the dock/pier this season. Mr. Wells provided diagrams showing potential placement of OOC's equipment on the viewing area of the main pier. Tim suggested the storage unit be placed along the east railing to avoid obstructing the view. Mr. Wells explained that they would build a storage unit to house their equipment. He anticipates it only taking two days to construct. Phil questioned if

a permit will be required. Commissioner Scholfield explained that since it is a temporary structure, no permit will be required. The storage unit will store eight single kayaks and six doubles as well as twelve paddleboards. Mr. Wells explained that they may remove the paddleboards each night to avoid possible theft. Mr. Kuntz asked if there is a security camera that would capture that area. Commissioner Scholfield said that the Port has one security camera located on a pole in the boat launch parking lot. It could be programmed to zoom in on the pier viewing area after hours. OOC would assist customers in bringing the rental equipment down to a raft that is attached to the dock via the gangway. At times the sailboat float may be used as well. Phil said that it could cause interference to individuals using the gangway. Mr. Kuntz assured that OOC personnel will be courteous to others using the facilities. Bridget Burke of CIR asked if a permit will be needed for the raft. Since it is a temporary fixture it is believed no permit is required. Steve Trunkey of KSRF explained that their goal is to have the Sailing Program running all season round. They aren't quite there yet with August being the downtime this season, but he is confident that the sailboat float will be used all season by KSRF in the future. Mr. Trunkey was concerned about OOC's raft being attached to the sailboat float as it could negatively impact the Program. He said that he really likes the idea of more non-motorized activity on the bay, but he is uncomfortable with the Port supporting a for-profit business that could negatively impact the Port Sailing and Rowing Programs. It was clarified that OOC's raft will not be attached to the sailboat float, but they will be using it when it is available. Commissioner Scholfield

Port of Silverdale – Minutes of Regular Meeting on April 19, 2018

explained that OOC is the Port's tenant. The Port is supporting it as they would any tenant and it is beneficial to the community to have multiple user groups using all the Port facilities. Mr. Kuntz asked Mr. Trunkey to contact him directly if there is a problem and/or if they need help with anything

Ms. Burke explained that the sailboat float is often used by swimmers to get out of the water as the lower freeboard makes for easier access. She warned Mr. Kuntz that OOC may encounter this problem as well. Because the Port accommodates motorized vessels, swimming is prohibited for obvious safety reasons, but that doesn't stop individuals from doing it. Tim reported that ladders are being installed on the dock, so hopefully that will help lessen the issue of individuals using the sailboat float for that reason. Commissioner Scholfield suggested 9-1-1 be called if individuals are swimming around the finger piers. It is dangerous and against Port rules, which if individuals don't adhere to the rules they can be trespassed.

Mr. Kuntz requested an addendum be added to the current contract. Phil explained that this is a totally different lease and asked about the terms. OOC would like to continue its Silverdale operations from June 1st through September 30th. Commissioner Scholfield suggested the rental amount remain the same as last year and renegotiate next year. Mr. Wells offered to create a proposal. Monica Downen of Monica's Waterfront Bakery offered to help advertise and plans to further discuss with Mr. Kuntz. OOC will not be open during Whaling Days. Tim suggested the new lease require OOC personnel to self-monitor the

equipment that is on the pier to ensure there are no tripping hazards. *It was agreed to have a new lease agreement with Olympic Outdoor Center prepared for further review at the May Regular meeting* (motion by Scholfield; second by Greaves; unanimous).

3.2. Roy Sahali said that he would like to continue his seasonal activities at the Port starting May 4th through October. Since OOC is moving onto the dock he asked to relocate his concession to OOC's vacated spot. It's closer to the power outlets and that would be of benefit to him and his concession. The Commissioners agreed that would be okay, but Commissioner Scholfield said that due to construction in the area he may be bounced back to his original area from time to time. Mr. Sahali said that he plans to be flexible especially this year as he realizes the County road projects will have an impact. Mr. Sahali asked when the sailboat float and handling pier will be removed for the season. Tim said that typically they are removed in mid-October depending on KSRF's need. Mr. Trunkey agreed that mid-October should be sufficient this year. *It was agreed to renew Roy Sahali's Waterfront Use and Management Agreement to begin May 4, 2018 through October 31, 2018* (motion by Scholfield; second by Greaves; unanimous).

3.3. Steve Rice of Rice Fergus Miller provided an update on the Waterfront Master Plan. He explained that in talking with Port Commissioners early on he determined that they all agreed that the open nature of the waterfront is not to be compromised. He provided a conceptual design of the area keeping that in mind. He explained that there

is possibly a unique opportunity on the horizon, as the Central Kitsap School District (CKSD) board members are contemplating the future of the Jenne-Wright building/property, but to discuss it now would be a bit premature. Mr. Rice was tasked with setting up a meeting to include Port representation and the Executive Director of Business and Operations for CKSD, Doug Newell. The parking situation in Old Town was discussed. Ms. Downed asked if Rice Fergus Miller had looked into turning the two blocks along Byron between Pacific and Washington into a one-way street. Mr. Rice was going to further investigate that possibility. At the Workshop held in March the Commissioners agreed to have a structural evaluation completed on the Old Town Pub/Emel building. The Letter of Agreement was reviewed. The evaluation will cost \$13,885. Phil suggested the Commissioners make a decision to either preserve the building or not, instead of spending money on it only to have it demoed. Mr. Rice explained that the evaluation will show the condition of the building as it stands. There will be more fees to determine cost estimates of the necessary improvements. He said that in total it will be approximately \$20,000. Phil said that if the Commissioners already know that they aren't in favor of refurbishing the building, then why waste another \$20,000 on it. Commissioner Scholfield suggested a decision be deferred until the planning process is further along. Randy Hunt said that it is a three-story building with much history. It's the building that will be written about in history books. He believes that if the Port were to refurbish the building it would add another layer of interest to the area drawing people to it. He suggested a fundraiser be started to cover the costs of the

evaluation. Marvel Hunt added that there are grants available to restore it. Mr. Rice said that if the Port decided to refurbish it, they are in good hands with Rice Fergus Miller because his architectural partner Dave Fergus has much experience in historical building restoration. He explained that the structural evaluation is where it starts. The Commissioners requested that this continue as an agenda item for the May meeting. Mr. Rice is scheduled to make a presentation on the Port's behalf at the Central Kitsap Community Council (CKCC) meeting on May 2nd. He thanked the Commissioners for their time.

4. UNFINISHED BUSINESS

4.1. Port Programs

a. Sailing – Mr. Trunkey reported that it wasn't one incident in particular that caused the lower end damage to the chase boat motor. It was an older motor that was well used. He explained that the motor is currently being stored at a KSRF volunteer's house. It was agreed it should be returned to the Port and stored in the Port office building's shop in the back. There had been discussion about CIR using the motor and fixing it, but Ms. Burke explained that CIR received a \$1,000 grant to put towards a new motor and are currently trying to raise money for that. The damaged motor fix is estimated at \$2,000. Ms. Burke is hesitant to spend that much money on a fix, as it may be more frugal to put that money towards a brand new motor. She mentioned that CIR has tried to sell the Coho motor, but there has been little interest. Tim suggested the damaged motor be sold via Washington State Department of Enterprise Services. The proceeds could possibly go towards a new motor. This will be investigated. Mr. Trunkey

Port of Silverdale – Minutes of Regular Meeting on April 19, 2018

reported that the Girl Scouts donated upwards of ten Toro boats to KSRF. They are temporarily being stored along the outside of KSRF's boat storage fence. Mr. Trunkey will determine where to store them permanently. The North West Interscholastic Sailing Association's District Championships Regatta is scheduled for May 5th and 6th. This will bring over 100 sailors and their families to the Silverdale area. Mr. Trunkey reminded Commissioners that in the past moorage fees were waived for the regatta support boats and asked the same for this year. Tim suggested Mr. Trunkey provide a list of the support boats WN#'s, so that letters aren't generated. Mr. Trunkey said that the Lyon's food truck will be onsite to feed all the sailors. He explained that it will need to be hooked up to power, so it will be parked next to Mr. Sahali's concession truck in the vehicle/boat trailer overflow lot. KSRF and CIR leases have expired. It is thought a lease may not be necessary since they are Port programs. Mr. Trunkey explained that KSRF had prepaid \$150 for 2018, so should be reimbursed if a lease isn't required. Greg Jacobs proposed that the Port authorize KSRF and CIR to run the programs. Ms. Burke said that the Mount Baker Rowing and Sailing Center has a similar partnership with the City of Seattle. Phil will request a copy of that agreement and possibly use it as a model.

b. Rowing – Ms. Burke reported that CIR is trying to raise funds for a second safety boat for the Juniors Program. The additional fence to extend rowing alley has not yet been ordered. She is aware that a call for locate is necessary prior to any digging. CIR recently secured a space in Old Town to house the ergs, which offer rowers the ability to work out

when the weather isn't cooperating. Ms. Burke requested a windsock be placed on the dock. Placement of a windsock was discussed. Tim suggested one be placed on a marker piling. Ms. Burke said that she was approached by a member of the local outrigger canoe club requesting storage in the non-motorized boat storage area for two single outriggers. They are 28' long and would require an additional rack in the non-motorized boat storage area. Commissioner Scholfield was authorized to spend up to \$1,000 for an additional rack. Ms. Burke asked if the Port could have rowing alley re-graveled. It was suggested she approach the contractor working along Silverdale Way for gravel. Tim cautioned that not all gravel is the same. In construction backflow gravel is generally used, which will create a big problem for CIR. Mr. Trunkey reported that there is a vessel in the non-motorized boat storage area that is so badly damaged, it is not usable. He requested the owner be asked to remove it. Ms. Burke said that a fence gate on rollers would be a good addition to the fenced area. It would make it easier to maneuver the boats in and out of the facility. Mr. Trunkey questioned how much the neighboring property owner, Olympic Photo Group (OPG), was paying to store the trailer near the boat storage area. Commissioner Scholfield explained that it was just a temporary accommodation.

4.2. Port Facilities

a. Dredging – Phil reported that Marine Survey and Assessments is still working on the Aquatic Resources Permit Application (JARPA). Commissioner Scholfield suggested delaying the dredge until 2019 and attempt to receive grant funding in November 2018.

Port of Silverdale – Minutes of Regular Meeting on April 19, 2018

b. Ladders – Tim is aware of the May 15th deadline.

c. Handling pier is back in the water.

d. Pier – the center timber reinforcement maintenance is complete. Tim said that the Commissioners should consider replacing additional decking boards as there was a lot more rot than anticipated.

c. Port Rules – and “No Wake” sign have not yet been ordered.

4.3. Port Properties

a. 9004 Washington/DRC – Mike will be working on generating an estimate list. He has been in contact with Faithe Lester of the DRC.

b. 3255 NW Lowell Street the Horsley's hadn't yet signed a lease, but were in the process of measuring the building and determining their next steps when another interested party, the Atwood's who also run a childcare center, inquired and immediately made payment and signed a lease. The Atwood's are hiring Mike to assist in the permit process and perform upgrades.

c. 9020 Washington/Elizabeth's House of Wax – the tenant asked what the timeline was for the wheelchair ramp she had requested months ago. She was reminded that it is on hold until the sewer situation is fixed and that depends on the County's sewer line upgrade project along Washington Avenue. Commissioner Scholfield reported that upon review of the County's plans for the project he saw that there is already an existing outlet there. He requested Mike to contact Robison Plumbing to hook up the building's sewer line

to it, so that the grinder in the basement of 9020 Washington can be removed.

d. 3215 NW Lowell Street, Suite 231 has been rented to a real estate agent.

4.4. Bayshore/Washington Upgrade Project – the County-generated acceptance letters, right-of-way deeds, encroachment easement and temporary construction easement had been received from the County. Phil reviewed them and said they were sufficient. *It was agreed to approve Right of Way Deeds for the following tax parcels Nos.: 4458-009-001-0009, 4458-013-001-0001 and 4458-012-004-0109; an Encroachment Easement on tax parcel No.4458-009-001-0009; and a Temporary Construction Easement for all three properties* (motion by Scholfield; second by Greaves; unanimous).

4.5. Pump Station 3 – the Port received an e-mail from the County dated April 19, 2018 informing that the County has been awarded funds through the Washington State Public Works Board for sanitary sewer improvements, which include upgrades to four sewage pump stations in the Silverdale/Central Kitsap area, of which one is Kitsap County Sewage Pump Station Number 3 that sits on prime waterfront property at the head of Dyes Inlet. The Director of Public Works, Andy Nelson, told Phil that a relocation feasibility study will be conducted. Maybe this additional funding could help.

5. NEW BUSINESS

5.1. Port Host – Safe Security will again provide a guard to act as the Port Host, Fridays through Sundays, starting Memorial Day weekend through Labor Day weekend. It was agreed that the guard should also plan to work the Memorial Day holiday and Labor Day holiday.

5.2. Last Day of CK Schools is Wednesday June 20th. It was agreed to hire two security guards to secure the Waterfront area.

Commissioner Scholfield suggested the vehicle parking lot be closed on June 15th to accommodate the Central Kitsap high school seniors parking while they go on the Argosy tour that night.

Safe Security will be asked to provide two guards for 4th of July service as in years' past.

Commissioner Scholfield explained that the sprinkler system was turned off for the season. He will be working on trying to locate a possible leak in the system. The last water bill prior to shut off was quite high indicating there was a leak. Tim offered to help.

6. SAFETY – Commissioner Scholfield reported that an individual made a call to Safe Security because a used needle had been found on Port property. The guard placed it in the garbage. Tim was notified that it was in there as a safety measure.

Commissioner Scholfield explained that the homelessness problem is escalating. He witnessed at least ten individuals that are believed to be homeless hanging out near the restrooms. Although the restrooms are on a timer and close after hours, it is believed that there are some individuals that use the deadbolt to hold the door open so that they don't get locked out after hours.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$68,318.23, checks numbering 11733 through 11761 and Electronic Transfer 2018-04 to the U.S. Treasury in the amount of \$1,621.72, were

approved (motion by Scholfield, second by Greaves, unanimous).

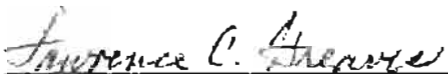
8. EXECUTIVE SESSION – None

9. ADJOURN – Commissioners were reminded of the Kitsap All Ports meeting scheduled for Monday, April 23rd, opening day of the Tuesday Farmer's Market on May 1st, and the Central Kitsap Community Council Meeting on May 2nd.

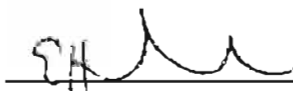
The meeting adjourned at 10:36PM (motion by Greaves, second by Scholfield, unanimous).

Approved:

Henry Aus, Commissioner



Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

PORT OF SILVERDALE

Comprehensive Plan Update
2017-2018

Steve Rice
Architect & LEED AP
Principal

RICE *fergus* **MILLER**

 **ART ANDERSON ASSOCIATES**
VALUE BEYOND ENGINEERING®

RFM/AAA Team Charge

- Update 2007 Port of Silverdale Comprehensive Plan
- **Create 'Concept Use Plan' focused on Port's waterfront properties**

- Public outreach
- Collaborate with Kitsap County

- Carry out projects 2018 - -

Master Plan Updates

2007 Makers 10-year Comprehensive Plan

2012 revision

2017 November RFM/AAA hired



Port of Silverdale Mission

- Promote economic activity
- Provide recreational opportunities and access



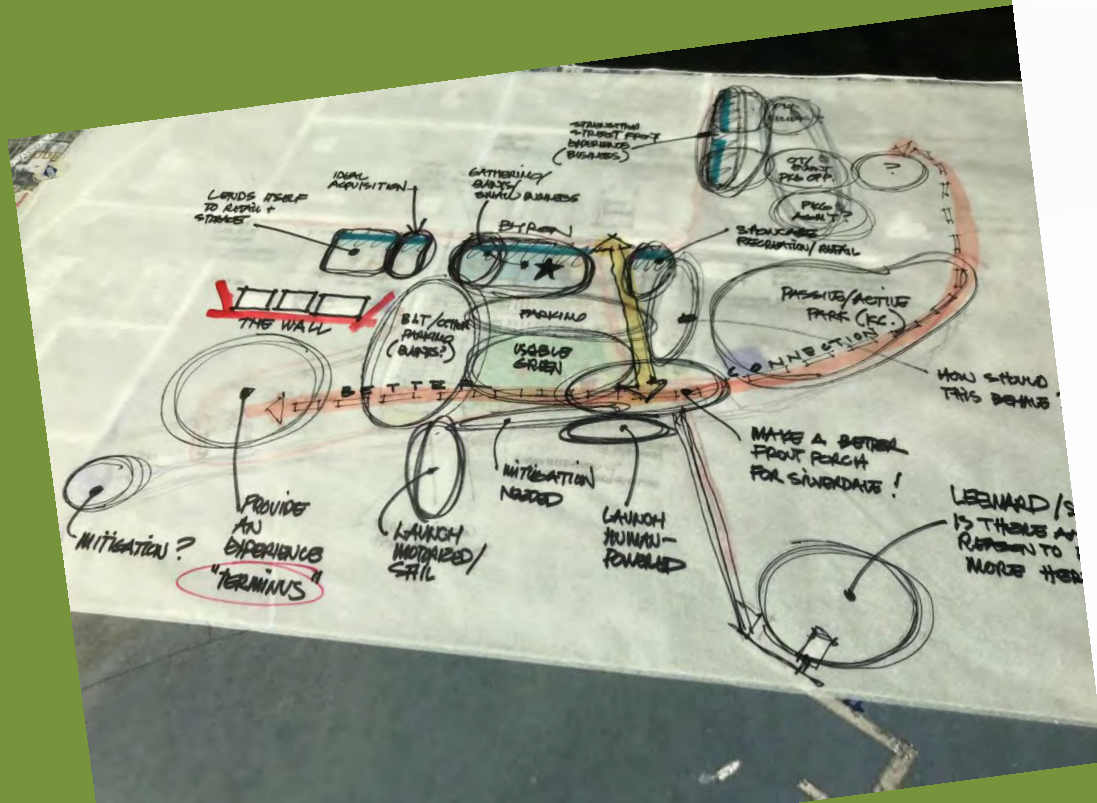


Port-Owned Properties

Activities to Date

- Met with Kitsap County Public Works - ongoing
- Co-authoring KC/POS electronic survey for Silverdale waterfront
- Held listening and idea session with Citizens POS Advisory Committee
- Hosted POS Commissioner Workshop
- CKCC visit today

Commissioner Workshop



PORT OF SILVERDALE
COMPREHENSIVE PLAN UPDATE

WORKSHOP AGENDA

- OPEN PUBLIC MEETING
- INTRODUCTIONS
- GOALS FOR THE DAY / RFM APPROACH
- SET THE SCENE
- THE WORLD AROUND US

WORKSHOP

- OUTREACH PLAN
- NEXT STEPS
- CLOSE PUBLIC MEETING

TRANSITIONAL

OLD TOWN GRID

**WATER DEPENDENT /
WATER ENJOYMENT**



Kitsap Sailing & Rowing Foundation Supports Sailing and Rowing at the Port of Silverdale

Mission

The Kitsap Sailing & Rowing Foundation believe that learning to sail and row teaches important life skills like teamwork, decision making, and appreciation of the natural environment. Competitive sailing and rowing requires tactical and strategic thinking and physical ability. With each experience on the water, young people gain self-confidence, self-esteem and a sense of camaraderie and accomplishment.

High School Recreational Rowing starts April 10

[More info](#)

Free Cox Clinic April 14

[More info](#)

Masters Try Rowing April 21

[More info](#)

Learn to Row starts April 24

[More info](#)

Masters Rowing starts April 16 - 18

[More info](#)

Concept Use Plan

Commissioner Priorities

- **Develop Silverdale's waterfront as a people place**
- Keep nearshore open – **“feel the expanse”**
- New facilities must support Port mission
- Respect & promote small business
- Keep Old Town scale and character
- Support non-motorized boating programs
- Balance parking vs. activity

Concept Use Plan

RFM/AAA Mindset

- More reasons to come here!
- Needs landmark/destination project
 - Consider unmet needs/opportunities
- Connect people to waterfront
- **Bring Silverdale a better ‘front porch’**
- Plan for synergies
- What can OT/WF be known for?

Concept Use Plan

RFM/AAA Mindset

- More reasons to come here!
- Needs **landmark**/destination project
 - Consider unmet needs/opportunities
- **Connect** people to waterfront
- Bring Silverdale a better **'front porch'**
- Plan for **synergies**
- What can OT/WF be known for?

in progress
Concept Use Plan

1 2 3 4

5

6

- 1 sail & small boat loft w/street retail
- 2 iconic corner business
- 3 mixed use community event center
- 4 human powered boat center
- 5 outpost
- 6 kayak and paddleboard launch float



Assets & Investments Connect the Dots?



Next Steps

- Meet with Central Kitsap School District
- Complete electronic survey
- Continue to develop use plan for Port properties
- Find good answer for Pump Station 3
- Decide on OTP evaluation
- Set project timelines
- Publish formal POS comprehensive plan update
- Work with Kitsap County on **'front porch' integration**

A scenic sunset over a body of water. The sky is filled with wispy clouds, and the sun is low on the horizon, casting a golden glow. A long pier extends into the water from the right side of the frame. The shoreline is covered in dark rocks and a dense forest of evergreen trees.

Thank you! Questions?

Steve Rice
Architect & LEED AP
Principal
srice@rfmarch.com

RICE *fergus* **MILLER**



Kitsap County Department of Public Works



SOLID WASTE

- Garbage & Recycling
- Hazardous Waste
- Closed Landfills

SEWER

- Sewage Collection
- Sewage Treatment
- Water Reuse

STORMWATER

- Reduce Flooding
- Prevent Pollution
- Restore Fish Habitat

ROADS

- Preserve Roadways
- Efficient Traffic
- Improve Safety & Capacity



Kitsap County Department of Public Works



Upcoming Sewer Capital Projects in Silverdale

- Pump Station 3 and Collection System upgrades
 - Replace equipment
 - Increase capacity
 - Improve look of facility
- Bay Shore and Washington Improvements
 - Replace and upsize outdated sewer gravity pipe
 - Add enhanced stormwater treatment
 - Pedestrian and traffic flow improvements



Kitsap County Department of Public Works



Pump Station 3 Design

- Pump Station 3 will be upgraded in same location
- Facility can blend into park as “kiosk” with displays
- Design contract in 2017
- More opportunities for input during design phase



Sample design by RH2 Engineers



Kitsap County Department of Public Works



Waterfront Park Improvements

- Part of Bay Shore and Washington Improvements in 2018
- New turnaround in parking area
- Opportunity for pavement art at turnaround
- Beach grasses and native vegetation soften view of pump station
- Restore native vegetation at Rip-rap



Design by RH2 Engineers



IDEAS

- 1 Port Development Site
- 2 Port parking layout per Port Plans
- 3 Turnaround with central circle accommodates Emergency Vehicles
- 4 Angled Parking
- 5 Existing Pump Station
- 6 Interpretive Opportunity
- 7 Surround wall with Gate
- 8 Lawn with Landscape Berm
- 9 Eventual Restored Shore
- 10 Clear Creek Trail Extension
- 11 Port Lawn
- 12 Enhance Park with Improvements

Opportunity to collaborate with Port of Silverdale on proposed landscape improvements

Waterfront Park Improvements



Kitsap County Department of Public Works



Project Timelines

- Pump Station 3 and Collection System upgrades
 - No proposed start date yet
 - Design contract in 2017
 - Tentative construction start in 2019
- Bay Shore and Washington Improvements
 - Construction bid in spring/summer 2017
 - Construction start in 2018



Kitsap County Department of Public Works



Questions?

Barbara Zaroff, P.E.

Capital Projects Engineer

Kitsap County Public Works – Sewer Utility Division

360-337-5777

bzaroff@co.kitsap.wa.us

Project information available at www.KitsapGov.com/pw

Port of Silverdale

Minutes of Regular Meeting
May 17, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Tim and Lee Knapp of TIKAR Services; Carla Larson of Whaling Days; Marvel and Randy Hunt of the Central Kitsap History Club; Anita White of Boy Scout Troop 1540; Ron Easterday of Rice Fergus Miller (RFM); Jackie Bosinger; Caleb and Vickie Reese; and Bridget Burke and Brian Burnham of Claim Island Rowing (CIR) arrived at approximately 7:25PM.

1.1. Agenda – *the agenda was approved* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The April 19, 2018 Regular meeting minutes and the April 23, 2018 Kitsap All Ports meeting minutes were approved as submitted* (motion by Scholfield, second by Greaves, passed – Commissioner Aus abstained as he was not in attendance at those meetings).

3. PUBLIC COMMENT

3.1. Roy Sahali was not in attendance.

Anita White with Boy Scout Troop 1540 submitted a letter to the Commissioners. The troop is hoping to provide cleanup services once again during and after the Whaling Days festival. *It was agreed to have Boy Scout Troop 1540 provide cleanup services on Port property during and after Whaling Days 2018* (motion by Aus, second by Scholfield, unanimous). Commissioner Scholfield requested the scouts coordinate with Whaling Days to place garbage in the dumpsters prior to Waste Management's pickup. Ms. White made note of that.

4. UNFINISHED BUSINESS

4.1. Port Programs

a. Sailing – the regatta went well.

Commissioner Scholfield explained that Kitsap County Parks was surprised by the number of individuals that attended the regatta and the demand it created on the County restrooms. County personnel mentioned that sani-cans should have been provided by KSRF. Commissioner Scholfield provided the Port's electrical spider box for the food vendor. Bobbie and Molly Atwood, the new tenants at 3255 NW Lowell Street, graciously offered the building to house a group of sailors, the Friday prior to the regatta, as they were unable to find local accommodations. The damaged motor that was used by KSRF was dropped off at the Port's shop. Tim will write up a description of the motor and what needs to be done to repair it. This information will then be used to list it with Washington State Department of Enterprise Services - Surplus Property Disposal. KSRF had prepaid for the 2018 lease. Since a 2018 lease has not been entered into and there has been talk about revamping the way the Port and KSRF and CIR are doing business, KSRF has asked for a refund of the \$150. Phil explained that he will be reviewing other sailing and rowing programs to determine how they are set up, but until then both KSRF and CIR should pay for 2018. The Commissioners agreed.

b. Rowing – Phil said that Ms. Burke had talked to him about the possibility of an on-site visit to Mount Baker Rowing and Sailing Center and Sail Sand Point both of which are located in Seattle.

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c. Non-motorized boat storage – the new kayak rack has been ordered. \$1,000 was authorized to purchase the new rack, but it actually cost \$1,478.95. *It was agreed to authorize an additional \$478.95 for the purchase of a kayak rack* (motion by Aus, second by Greaves, unanimous). At last month's meeting Ms. Burke mentioned that it would be beneficial to have a gate with rollers installed. Commissioner Scholfield said that with the gravel foundation, rollers are not a good idea.

4.2. Port Facilities

a. Dredging – Phil reported that Kitsap County is reviewing the permit. They have asked for information related to the 2004 dredge. Phil doesn't anticipate any issues as this dredge is within the parameters of the 2004 dredge. It has yet to be sent to the necessary State agencies.

b. Ladders – Tim explained that he was two days late in installing the ladders and reduced his bill by \$200 as outlined in the contract. Phil provided Attachment A to the contract, which showed the ladder requirements. The extra ladder was viewed and attachment A reviewed to insure that all the requirements were met. It was determined that within the attachment there was a discrepancy in the number of ladders to be fabricated and installed. Tim explained that he had not noticed this discrepancy until now. He had followed the instructions in the bolded "Project" line near the top of the document which stated "Provide six marine ladders and install five of them on Port floating docks, in Silverdale, Washington". The four steps were not flat, but round. Tim explained that he had discussed this with Commissioner Scholfield and they agreed that anti-skid strips would not

work best as sea life will grow on it. Tim proposed to swap out the ladders one at a time to apply an anti-fouling paint to them with an anti-skid paint on the steps. He would charge \$100 per ladder to complete this. The Commissioners agreed this should be completed. Tim explained that he didn't install the fifth ladder yet because he questioned where it should be installed. This was discussed. It was decided to mount it on the inside corner near the gangway. Carla Larson asked if the new ladders will affect how boats are tied up to the dock especially during Whaling Days. Tim said that there shouldn't be any impact.

c. Port rules/Windsock – Commissioner Scholfield has ordered the sign blanks to post the rules. Placement of the "No Wake" sign and windsock was discussed.

d. Sprinkler system – once it is turned on Commissioner Scholfield and Tim will try to determine where there is a leak.

4.3. Port Properties

a. 9004 Washington/DRC – Mike provided an e-mail dated May 16, 2018. It states that his best guesstimate for the project is \$125 per square foot and the project would cover 408 square feet, which totals \$51,000. This does not include costs for new appliances as he thought the tenant would purchase them directly. The costs would be amortized over the course of the lease. Commissioner Scholfield said that once the DRC agrees to what they actually want completed, it could go out for bid.

b. 9020 Washington/Elizabeth's House of Wax – Mike provided an e-mail stating that Robison Plumbing is requesting a copy of the

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County drawing that shows the sewer outlet for the building. Commissioner Scholfield will work on that.

c. 3255 NW Lowell Street/Central Valley Daycare – Mike has been working on obtaining the necessary permits to begin work on the building. His recent bill included reimbursement for County permit fees of \$468.20. Commissioner Scholfield explained that the tenant is responsible to pay for those fees and will be asked to reimburse the Port.

d. 3215 NW Lowell Street, Suite 161 tenant gave notice that they will be moving out by May 31, 2018.

e. Clean up of properties – for the past several years, mainly in anticipation of the Kitsap Water Trails festival, Mike and his crew have provided landscaping of Port properties. The Water Trails festival has been cancelled due to a lack of funding and it was agreed that since the majority of the Port's leases state that the tenant is responsible to maintain the property, Mike will be informed that the landscaping will not be necessary. Commissioner Scholfield plans to clean up the area near the Old Town Pub property. Apparently, someone has been cutting the shrubs behind the boat launch restrooms. Commissioner Scholfield and Tim explained that the shrubbery in that area should be left untouched as clearing it out creates a private area which is a draw for negative activity. It is thought that someone from the adjacent apartments may be responsible for cutting back the shrubs in that area. Commissioner Scholfield requested a letter be sent to the manager of the Bay Breeze apartments explaining the situation.

4.4. Waterfront Master Plan – Ron Easterday reported that the Central Kitsap Community Council (CKCC) presentation was successful. Commissioner Scholfield said that he was surprised that there was no mention of a non-motorized dock during the presentation. He said that it is a major component of the updated plan and should be included. Phil told Mr. Easterday that it was thought RFM would determine the needs of the community for such a dock. Mr. Easterday took note of that. He provided copies of the County/Port draft joint outreach survey. He explained that it will be live on June 1st for approximately three to four weeks and accessible through the County's website with links on other local websites. The Commissioners agreed that the survey seemed sufficient and RFM should move forward with it. Tim said that Morgan Johnson, the General Manager of the Silverdale Water District, is interested in talking with the Port Commissioners about possibly creating an educational water feature using recycled water somewhere within the new plans. Mr. Easterday took note of that. Commissioner Greaves and Phil along with RFM representatives plan to meet with Central Kitsap School District (CKSD) representatives on May 29th to discuss Old Town and the CKSD's plans for the Jenne-Wright property. Mr. Easterday said that an introductory meeting with the Kitsap Public Facilities District will also be set up as they are a possible funding source. He suggested the Kitsap Economic Development Alliance (KEDA) and Visit Kitsap be informed about the Port's Waterfront Master Plan update as it is important to have the local support.

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The proposed structural evaluation of the Old Town Pub was discussed. It was explained that the evaluation will not include cost estimates. That would be a separate agreement. Marvel Hunt said that she would hate for the Port not to move forward with the structural evaluation and estimates to restore the building. She urged the Commissioners to do their due diligence for the community and town. Phil said that he received a call from someone willing to move the building as an option. Jackie Bosinger said that there are a lot of people who are willing to make donations of time and money to save the building. She suggested a "Go Fund Me" account be created to pay for the evaluation. *It was agreed to enter into the agreement with Rice Fergus Miller for the structural evaluation of the 3473 NW Byron Street property totaling \$13,885 (motion by Aus, second by Scholfield, unanimous).*

4.5. Pump Station 3 – complaints about the foul smell emitting from the County's sewer system were received from the Tuesday Farmer's Market vendors on May 8th. An e-mail was generated and sent to County personnel. This raised the question of how many complaints the County might receive about the foul smell in the Old Town Silverdale area.

4.6. Olympic Outdoor Center (OOC) – Phil provided the new agreement between OOC and the Port. It was signed by Commissioner Greaves.

4.7. Port Host is scheduled to begin Friday, May 25th. There will be three individuals alternating as the Port Host. Ms. Hunt asked if this person has control over the parking lot. It was explained that yes he/she will have control over the parking areas as that is one of the

main reasons the Port Host position was created. They will also have the authority to tow and trespass if necessary.

NEW BUSINESS

5.1. Incident on dock – a boater was forcibly removed from the dock on May 15th by Sheriff deputies as the individual was threatening others in the area. He apparently was taken to the hospital for a mental review, but has now returned to his boat on the dock. He did pay for the first night of moorage. It was agreed that if there is another incident with this individual that he be trespassed.

5.2. Work boat – Tim stores the TIKAR work boat in the Port's shop. Julie Jablonski of KSRF questioned if a fee was being paid to store it there. When told that no fees were paid for the boat to remain there, she asked if the Washington State Auditor was aware of this. This was discussed. Tim explained besides the fact that if the boat isn't stored in the Port's shop he will have to charge mobilization charges every time the boat is used for maintenance and there have been plenty of times that he has used the boat to go out and remove large items that posed a hazard to the Port's facilities. With the boat being ready to go and in such close proximity to the Port facilities it assures a short response time, which in turn would lessen possible damages. The Commissioners agreed that in the long run it is cost effective for the Port to allow Tim to store the work boat in the shop.

5.3. Showers – from time to time non-boaters call the Port office asking for the code to access the showers. It was questioned if the showers are limited to just boaters. Commissioner Scholfield said that there have been issues of vandalism in the shower facility, where someone had unsuccessfully attempted to remove the coin machine from the wall. Lee Knapp said that with the history of damages he

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would limit the code being provided just to the boating community. Phil said that he has advised individuals to contact the YMCA as they have a shower program for the community.

5.4. 2019 Northwest Boat Travel – it was agreed to continue to advertise in the Waggoner.

5.5. Automated Pay Machine – this was discussed at the April Kitsap All Ports meeting and sounded like something that would benefit the Port, but with the initial setup costs nearly \$13,000 and monthly fees compared to the amount of revenue generated from moorage it was decided not to pursue it at this time.

Commissioner Aus said that he thought the Port should look into the Dockwa app. It was thought the app was aimed at just reservations, but apparently, boaters are able to make moorage payments by using the app as well. This will be further investigated.

6. SAFETY – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$38,205.71, checks numbering 11762 through 11785 and Electronic Transfer 2018-05 to the U.S. Treasury in the amount of \$1,726.40, were approved (motion by Aus, second by Scholfield, unanimous).

Commissioner Scholfield questioned if the Argosy cruise for the Central Kitsap High School students is still scheduled for June 15th. He had heard it was cancelled. An e-mail to Argosy confirming will be sent.

8. EXECUTIVE SESSION – None

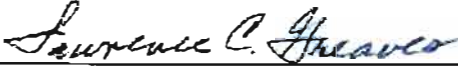
9. ADJOURN

The meeting adjourned at 8:38PM (motion by Aus, second by Scholfield, unanimous).

Approved:



Henry Aus, Commissioner



Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Meeting #: 1 of 3

Date: May 29, 2018

Project Name: Port of Silverdale Master Plan
CKSD Old Town Future

RFM Project # 2017098.00 (POS)
RFM Project # 2016066.00 (CKSD)

Attendees: Doug Newell – CKSD Asst. Supt. Phil Best – POS Attorney
Bruce Richards– CKSD Board Lawrence Greaves- POS Commissioner
David McVicker–CKSD Sup. Ben Anderson – AAA
Abigail Overton - RFM Steve Rice – RFM

Purpose: Facilitate conversation between Port of Silverdale and Central Kitsap School District regarding POS Master Plan and CKSD integration and support.

SR

- Having an updated Master Plan is a requirement for Port funding
- POS has quietly been obtaining properties in Old Town – How does this fit into the Master Plan?
- How does CKSD fit into the re-fashioning of POS Master Plan?

- After showing in-progress Concept Use Plan board -

- Landmark building/structure?
- Facilities for boaters?
- More usable public outdoor space?

DN

- What do we define as the border of Old Town? Wider border means more land to offer as inviting place.

DM

- Proximity to Strawberry Creek affects demolition and reconfiguration of Jenne Wright building/property.

DN

- Two years ago, CKSD looked into having discussion with Tribe and land assessors to determine possibilities.

DM

- How to we provide more green space around Old Town?

PB

- Citizen Advisory Committee commented on CKSD involvement in POS properties before RFM MP.

SR

- CKSD property has potential to feature Strawberry Creek as green space like Clear Creek Trail.

PB

- Interlocal Agreement: local governments can get together to improve freshwater sources.
- POS CAN receive donations – CANNOT give.

SR & DN

- North of Carlton, church and Scout Hall should stay; others not critical or historical
- Move Scout parking lot away from creek

SR

- Would POS see interest in residential properties in Old Town?

A: YES

- There is opportunity to develop so much more green space involving creek and waterfront.
- Longtime legacy of public use.

DN

- We have to determine how to make budget needs clear in order for more funding to follow.

SR

(Describes placement of developments in conjunction with creek)

- Event center? Marketed correctly and considerately, it could help with tourism as well as use by local citizens.

DN

- How does development happen if the Port owns these properties?

SR explains:

- POS can offer land to developers for specific-use projects.
- For event center, POS may be interested in development of facility with help from community organizations.

DN

- Any strategic investments POS is making or looking in to?

PB

- Thought about Bayshore building for boat storage but getting pushback from condo owners nearby.

DN

- What other ports have done something similar? Research their efforts in order to understand how best to monetize POS project?

PB

- Get county together to talk about zoning and how we consider traffic.
CKSD + POS – Collaborate w/Kitsap County to get ahead of market flip (will happen if downtown is improved) regarding permitting.

SR

- How do we get properties all around creek? Offer relocation trade?

BR

- Up density of restaurants and shopping in Old Town by relocating current business.
- What is the zoning in Old Town?

Answer from POS: Neighborhood Commercial Zoning

SR:

- Kitsap County design standards apply
- There could be a dramatic difference between rest of Silverdale and Old Town due to zoning regulations.
- What to do about the pump station?

PB

- DM, what do you envision for greenspace?

DM

- Town square with green space with apartments and green space surrounding. Concerts in the park.
- Is there space for an Amphitheatre - all-year performance space
- Educational space with green area
- Community space for connection
- There needs to be a visual draw from the highway/main road

DN

- How do we connect CKSD, POS, and waterfront with the rest of Silverdale? Rethink Silverdale.
- Mall won't last

DM

- Make Strawberry Creek accessible all the way up to Anderson Hill?

SR

- Bringing Steve Segoy in to look at mitigation opportunities
- Will help strengthen Strawberry Creek plan

PB

- On GPC committee
- Estuary organizations know how to obtain funding

DN

- Look ahead to obtaining properties to the future

PB

- Buy conservation easement on sides of creek?

BR

- What happens if Silverdale becomes a city?

PB answers:

-If a plan and permits are in place, cityhood would not impact plan.

PB

- Develop creek corridor protection plan?

DN

- How do we get to thinking about options? How tight is the box and what is our desired use?

DM

- Jenne Wright and field not talked of often in community

DN

- Field is used after school hours

SR

- How publicly are we ready to discuss land use?

DM

- We can wait until 900 Wing is viable, or we can say we are scoping 900 and offer public forum for POS MP.

SR

- Do we think about all of these entity owned properties as separate opportunities or as a whole?

DN

- We need options before we propose to community for opinion.
- What partnerships need to occur for redevelopment?
- Only a few investors in KC with enough capital.

BA

- Talk to other ports and marinas in area to learn and figure out public/private partnership?

PB

- Consensus seems like we want to think of properties as a group.
- Be careful not to make it public at this stage so plans don't get complicated and priorities stay clear.

SR

- John Rose understands public process. Have closed-door conversation?
- Sound West Group? Steve Sego works with SWG.

DN

- Would like to do research on what is and isn't working before we have conversations.

SR

- Wes Larsen with Nature Conservancy
- Not a lot of expertise in KC on plan of this size.

DN

- Can RFM help create next steps?
- Not a lot of noise about any of CKSD projects – so okay with proceeding

End goal for meetings: From each entity - What does Old Town look like in 10 years?

ACTION

SR bringing Steve Sego to POS on 6/5/18 from 11-1pm

END OF MINUTES

This is a summary of the items discussed. Please advise this office within 48 hours regarding any omissions or differences of understanding.

Prepared By: Abigail Overton, RFM

Port of Silverdale – Minutes of Regular Meeting on June 21, 2018

Port of Silverdale
Minutes of Regular Meeting
June 21, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Tim Knapp of TIKAR Services; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Julie Wurden-Jablonski of Kitsap Sailing and Rowing Foundation (KSRF); Marvel and Randy Hunt of the Central Kitsap History Club; and Caleb and Vickie Reese.

1.1. Agenda – *the agenda was approved* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The May 17, 2018 Regular meeting minutes were approved as submitted* (motion by Aus; second by Scholfield, unanimous).

3. PUBLIC COMMENT

3.1. Julie Wurden-Jablonski of the KSRF explained that she is currently the foundation's fundraising chairman. They are trying to raise funds in the amount of \$30,000 to purchase three additional sailboats that would stay on the trailer for regattas. This would allow the kids more time on the water without having to load and unload the boats. KSRF has been offered a donation of several hours of the Admiral Pete foot ferry beginning in the late afternoon of August 18th. KSRF would like to sponsor a dinner/auction on the dock during that time with the Admiral Pete moored on the outer dock. Ms. Wurden-Jablonski envisions tables set up on the dock for the food and fundraising participants to sit at and eat. She thought wine tasting could be set up on the

Admiral Pete so that alcohol is not on the docks. As a backup in case the weather doesn't cooperate they will reserve the County's shelter at the park. *It was agreed to allow KSRF to use the outer dock on Saturday, August 18th from 4:00PM to 8:00PM to hold a fundraiser dinner/auction* (motion by Scholfield, second by Aus, unanimous). Ms. Wurden-Jablonski thanked the Commissioners and said that she hopes this may be the first of many dinner/auction fundraisers at the dock.

3.2. Carla Larson of Whaling Days reported that she is working on the insurance for the festival. She questioned if Olympic Outdoor Center (OOC) will have their truck removed from the parking area prior to the festival. It was explained that John Kuntz of OOC had called the Port office explaining that they needed to park their trailer in the lot for a week, just until the shelter being built on the dock was complete, which will be prior to the festival. Ms. Larson said that during a recent meeting she attended she was asked if the Port is responsible to stock the loaner life jacket station at the boat launch as it is empty. Phil said that a business near the Poulsbo Sons of Norway has a lot of used vests. Tim offered to stock the jackets. Carla reminded everyone to complete the on-line County/Port survey.

4. UNFINISHED BUSINESS

4.1. Port Programs

a. Sailing – Tim is working on the description of the damaged motor so that it can be listed with Washington State Department of Enterprise Services - Surplus Property Disposal.

b. Rowing – nothing to report.

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c. Non-motorized boat storage –

Commissioner Scholfield reported that the new kayak rack has been received and is partially assembled. Once it is moved to the storage area he may have to move other items around in there to make room for the new rack.

4.2. Port Facilities

a. Dredging – Phil reported that the original dredge was to a ^{minus}10 in the moorage facility area and to a ^{minus}3 at the boat ramp. The permit for the new dredge was for a ^{minus}11 near the moorage facility and a ^{minus}3.5 at the boat ramp. Since the request didn't fit within the parameters of the original dredge the County suggested a State Environmental Policy Act (SEPA) analysis be conducted. This would be very costly and time consuming. Phil suggested a modification to the permit be made stating it will not exceed the original parameter. Phil will continue to work with the County and Marine Surveys and Assessments.

b. Ladders – Tim reported that all the ladders are in place. He will begin swapping the ladders out one at a time to apply the anti-fouling paint and anti-skid paint on the steps.

c. Dock maintenance – Tim explained that in order to maintain the dock the existing rub rails need to be replaced. He provided Estimate #2018-1026 and a sample of the material to be used as outlined in the estimate. The estimate also includes repair of wood on the floats and replacement of bolts. Tim explained that he spoke with Commissioner Scholfield about these two additional items and it was agreed that they would be billed by time and material as there is no way to pre-determine exactly how much time nor material

it will take to complete the job. Phil said that he has noticed at other Port of calls that instead of cleats they have a board along the dock for boats to tie up to, which is more convenient than cleats. Tim said that may be a good alternative using the high-density poly ethylene. Commissioner Scholfield said that between the wave action and tripping hazard it could be problematic.

d. Port rules/Windsock – Commissioner Scholfield has received the vinyl which the rules will be printed on and then assemble onto the metal sign blank. The windsock has also been received and it states "No Wake" on it. Both will be installed soon.

4.3. Port Properties

a. 9004 Washington/DRC – Phil sent an e-mail dated June 11, 2018 to Faithe Lester of the DRC. It included the breakdown of the upgrades that Mike had provided to the DRC months ago. Ms. Lester plans to bring it to the DRC Board meeting on June 26th and will inform Phil of the outcome.

b. 9020 Washington/Elizabeth's House of Wax – Commissioner Scholfield e-mailed Robison Plumbing the County drawing that shows the sewer outlet for the building. Mike said the he touched base with Robison and they said they are working with the County to determine exactly where the sewer outlet is located.

c. 3255 NW Lowell Street/Central Valley Daycare – Mike obtained the necessary permits and the work on the building is nearly complete.

Port of Silverdale – Minutes of Regular Meeting on June 21, 2018

d. 3215 NW Lowell Street, Suite 161 a construction company is planning to sign a lease next week.

e. 3473 NW Byron Street – the building was broken into last month causing approximately \$8,000 worth of damage. Trevor Ray Mercer was taken into custody on May 25, 2018 as he was found on the premises (K18-005511). A Victim Impact Statement and Restitution Estimate was received from the Kitsap County Prosecutor's office and has since been completed and returned.

4.4. Waterfront Master Plan – Phil reported that he, Commissioner Greaves and Steve Rice of Rice Fergus Miller (RFM) met with Doug Newell the Executive Director of Business and Operations of Central Kitsap School District (CKSD) to determine mutual interests in the re-development of Old Town Silverdale. A partnership between the Port and CKSD to restore Strawberry Creek was discussed. Phil and Mr. Rice later met with Steve Sego, who is a specialist in stream restoration. They went on a walking tour of Strawberry Creek and the Port's waterfront area. Along with the creek Mr. Sego was interested in creating soft beach protection along the waterfront heading east from the Port's boat launch towards the pier, which includes the County's property that houses the sewage pump station. This will be incorporated with the master plan that Rice Fergus Miller has been contracted to prepare.

Tim reported that the plans for the new Silverdale library have changed. It has been decided not to build on the corner of Blaine Avenue and Bucklin Hill Road. They are

working with the CKSD to relocate the library to the 900 building, which is on Dahl Road. There have been discussions about creating a lighted trail system that leads from the new library to the YMCA. Tim said that it made him think of Mr. Rice's idea of creating the Old Town focal point near the head of the pier with possibly adding a trail system leading to the Jenne Wright building.

4.5. Pump Station 3 – a County-sponsored public meeting is scheduled for Thursday, July 26th at 6:30PM in the conference room at the Silverdale Water District. Commissioner Aus suggested that the July meeting be continued to allow all three Commissioners to attend the July 26th meeting. This will be considered at next month's meeting.

4.6. Bayshore Drive/Washington Avenue Project – the County paid the Port \$3100 for the three easements. The checks have since been deposited into the Port's account.

4.7. Charles Lewis is the individual who was moored at the Port facility last month when Kitsap County Sheriff (KCS) deputies had to forcibly remove him after he threatened people on the dock and park goers. He was taken in for a mental evaluation. The next day he returned to the area and apparently began making threatening remarks to individuals at Cash Brewery and was trespassed from that property as well as from Port property. He left the area via his boat. The next week the Coast Guard apparently escorted him from the Illahee dock. On May 26th he was back at the Port of Silverdale. He was verbally told that he was not allowed to be on Port property. Safe Security called Commissioner Scholfield

Port of Silverdale – Minutes of Regular Meeting on June 21, 2018

informing him that Mr. Lewis was back on Port property. Safe Security was directed to call the KCS and Mr. Lewis was later arrested once again. It is believed he is currently in jail awaiting a court date. His boat continues to be moored at the Port and he currently owes \$240 in moorage fees. The boat was tied up on the far outside pier where it was damaged and also caused minor damage to the pier. The boat has since been moved to the inside finger pier. It is unknown who moved it. Commissioner Scholfield explained that he didn't plan to chain the boat because then it will become the Port's responsibility and it has very little value.

4.8. Last Day of CK Schools was yesterday, June 20th. Commissioner Scholfield reported that there were a lot of kids down at the park and Port area. For the most part they were well behaved. Safe Security guards did have to kick a few kids off the pier when they refused to comply with the rules. Up until a couple years ago, New Life Church sponsored the last day of school at the Port. It is unknown exactly why they stopped doing it, possibly funding issues. Commissioner Scholfield thought the Port might want to consider partnering with New Life in the future to promote and support the event. Tim said that it was his understanding that New Life sponsored a last day of school event at their new facility on Poplars Avenue.

4.9. 4th of July – two Safe Security guards will be on duty from 4:00PM to midnight. The Waste Management dumpster has been ordered. Tim said that he will pass out trash bags to the public who are participating in setting off fireworks. He has been doing this for several years now and is encouraged as

the attendees have really taken on the responsibility to clean up after themselves.

4.10. Port Host – Commissioner Scholfield reported that there are three guards who alternate the position. There is a slight learning curve, but it seems to be working out well.

4.11. Argosy Cruise scheduled for June 15th has been cancelled. It was agreed to refund Argosy the \$75 reservation fee.

4.12. Dockwa – it is believed this is more of a reservation system, but boaters are able to electronically pay through it. If the reservation portion of it can be eliminated it may work for the Port. Commissioner Scholfield plans to look into it further.

NEW BUSINESS

5.1. Chico Towing incident – on June 17th the vehicle/boat trailer lot was full. There were a few vehicles without boat trailers that were illegally parked in there, so Commissioner Scholfield tagged those vehicles with the parking notice that tells them the lot is for vehicles with boat trailers only and directs them to park in the vehicle lot on the corner of Washington Avenue and Byron Street. After an hour or so Commissioner Scholfield noticed there were boaters in their vehicles with trailers attached hunting for spaces to park, he called Chico Towing. Before Chico Towing showed up one of the illegally parked vehicle owners saw the notice on his car and re-parked it in the authorized area. Chico Towing arrived on scene and located the vehicle in the vehicle lot and continued to hook the vehicle getting ready to tow it. The owner noticed it and confronted the tow truck driver. The owner ended up paying an unhook fee; otherwise the car would have been impounded. The next day the owner called the Port office making the obvious

complaint. He was invited to attend tonight's meeting. An e-mail that explained the incident was sent to Chico Towing on June 19th. Kevin Ungren of Chico Towing replied to the e-mail and explained that they have two new drivers with a bit of a learning curve about how tows are handled with the Port. He said that Chico would be taking full responsibility and would contact the owner directly. Mr. Ungren has since met with Commissioner Scholfield and walked the parking lots to ensure Chico is on the same page as the Port with regards to towing. If cars re-park in authorized areas they are not to be towed.

5.2. Ball fields – Commissioner Scholfield reminded everyone of Mike Walton, the Executive Director of the Kitsap Public Facilities District (KPFD), who was in attendance at the Central Kitsap Community Council (CKCC) meeting where the Port Waterfront Improvement Project was presented. At that meeting Mr. Walton introduced himself and explained that the KPFD is looking for projects to fund. One of the requirements is that the project cost at least ten million dollars. Proposals for new projects will be accepted through December 31, 2018. Commissioner Scholfield thought the Port may want to propose a ball field project on property located near Greaves Way and Clear Creek Road. Phil was unsure if the Port could take part in such a venture. Randy Hunt thought that was the property that Chuck Haselwood used to dump fill dirt. Commissioner Scholfield said that it is part of the Clear Creek watershed and could maybe be used as a mitigation site. Phil will look into the legalities. Commissioner Scholfield might reach out to Mr. Walton.

5.3. Restoring Puget Sound Outdoor Displays - now include more educational information about Puget Sound. The cost for a 23-month listing is \$1,360. *It was agreed to list on the Restoring Puget Sound display map with a locator*

under the "Explore Puget Sound" section costing \$1,360 (motion by Commissioner Aus, second by Commissioner Scholfield, unanimous).

5.4. Enduris Board of Directors election ballot was reviewed. *It was agreed to vote for the two incumbents, Vicki Carter and Greg Brizendine, who were both running unopposed (motion by Scholfield, second by Aus, unanimous).*

5.5. E-mail dated May 30, 2018 from Phil was sent to the tenant of Suite B upstairs in the Port office building. There had been a complaint that his office was in disarray, which was later confirmed. The e-mail reminded the tenant of the provision within his lease that states at all times the property will be kept in a neat, clean and sanitary condition. The e-mail also explained that although the lease doesn't expressly prohibit pets it is suspected that his dog, that is often brought to the office, might be making it difficult to maintain the office. The tenant was receptive to the e-mail and has since picked up his area and seems to be no longer bringing his dog on a regular basis.

6. **SAFETY** – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$33,518.56, checks numbering 11786 through 11811 and Electronic Transfer 2018-06 to the U.S. Treasury in the amount of \$1,656.62, were approved (motion by Scholfield, second by Aus, unanimous).

8. **EXECUTIVE SESSION** – At 8:22PM it was announced that the meeting would be going into Executive Session for approximately fifteen minutes to discuss potential litigation.

At 8:30PM it was announced the meeting was returning to Regular Session. Several

individuals had stepped outside and were told the Executive Session didn't take quite fifteen minutes and that the meeting was returning to Regular Session although there was no more business to be conducted.

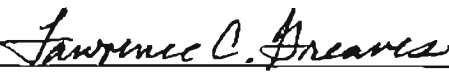
9. ADJOURN

The meeting adjourned at 8:31PM (motion by Aus, second by Scholfield, unanimous).

Approved:



Henry Aus, Commissioner



Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

OLD TOWN SILVERDALE FUTURE

CENTRAL KITSAP SCHOOL DISTRICT/PORT OF SILVERDALE

A joint discussion about the future of Old Town Silverdale through the lens of two public agencies with community missions, proximate real property interests, and the opportunity to coordinate efforts.

Meeting #: 2 of 3

Date: July 19, 2018

Project Name: Old Town Silverdale Future

ATTENDEES: Ben Anderson – AAA
Steve Rice – RFM
Ron Easterday – RFM
Abigail Overton - RFM
Phil Best – POS Attorney
Lawrence Greaves- POS Commissioner
Robin Shoemaker – CKSD Facilities Director
Doug Newell – CKSD Asst. Supt.

PURPOSE: Conduct a top-level “programming” exercise to bring out overall goals and objectives for the agencies’ planning efforts in Old Town – and see where they align or differ.

Old Town Silverdale Programming Exercise

1. What do you want Old Town to look like in 2068?
2. What is its ultimate and best potential?
3. What role does old town play for:
 - a. Silverdale?
 - b. Central Kitsap?
 - c. Kitsap County?
4. What changes need to take place?
 - a. What needs fixing?
 - b. What opportunities can we capitalize on?
 - c. What is holding ups back?
5. What is a reasonable timeline?
6. How do we build alliances and what partnerships/support do we need?

Meeting Notes: Please see attached pin-up board “notes” from the programming session.

This is a summary of the items discussed. Please advise this office within 48 hours regarding any omissions or differences of understanding.

Prepared By: Abigail Overton, RFM

PORT

GREATER
SENSE OF
HISTORY

WALKABILITY

MEMORIES
HISTORY

Recreation already
underway -
events too

CAPITALIZE
ON UNIQUE-
NESS OF PORT
DOCK/MARINA

Re gain
IDENTITY

SHIFT
FOCUS

"OLD TOWN" is
CORNER'S QUARTER
WHAT VIBRANT!!

REVITALIZE
A COMMUNITY
MEETING SPOT

A FASHION
NEARSHORE &
OUR WATER
IMPROVEMENTS TO CAPITALIZE

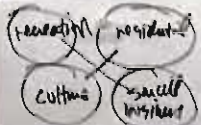
SMALL BUSINESS

bc its walkable, and
few changes are
+ creates VIBRANCY

LEVERAGE
HUMAN POWERED
FRATING

small bizs tend
themselves to
small business

GOVT LIFT?
- ISSUES BONDS?
- GRANTS / MATCH
- PFD



PHIL: something in
place w/in
5 YRS

MARKET STUDY
FOR GARY'S CTR
will it work?

ATION

LIKE

AL?

WHAT
NEED?

RICEfergusMILLER
ARCHITECTURE INTERIORS PLANNING VIZUAL

BOTH

WE WANT TO BE
INVOLVED B/C
a) WANT TO DO RIGHT FOR
COMMUNITY, and
(b) PREVENT PRIVATE FROM DOING
WRONG THINGS.

PROVIDE A ^{LAND} ^{TRAIL}
SOLUTION FOR
DAILY & EVENT
PARKING ^{aside} ^{from} ^{existing...}

CREEK AS
UNIFIER/CONNECTOR
NOT AS
DIVIDER

CKSD

SILV IDENTITY:

WHERE IT ALL
STARTED — NOT
JUST WALMART OR COSTCO

OLD TOWN
AS HEART OF
SILV.

CAPITALIZE
ON NON-MOTOR
IZED CONNECT-
(boats, pedestrian, etc.) ION

Makes it a
recommendation
HOUSING/VISIT

WANT
to live in
Silverdale

GATHER A
"COLLECTION"
AT OLD TOWN
TO DRAW COMM.

UNCOVER
OLD TOWN
(FOR VISITORS & RES)

STRENGTHEN
OUTDOOR
OPPS

CHANGE
PERCEPTION

NEW USES TO
CREATE MORE
USE & VISIBILITY

GATHERING
SPOT FOR
COMMUNITY

NO CIVIC CORE
IN SILV.
PORT SUPPLY
THIS?

"CIVIC
CENTER"

CIVIC COMPLEX
(GOVERNMENT)
OR _____??

IF 4000 HOURS.....
- 2YR PLANNING PER SITE
- 5YRS TO REALIZE IT
"DO NOT WANT TO LEAVE JERRY
WHILE IT EMPTY FOR 10YRS."

LAND OWN BUT STARTED
(HOW WE LEAVE) IS
TIED TO STRAWBERRY
CREAT. *RE WANTS TO
BE IN LAND ON
AND THAT STRAWBERRY
OWNERS



CKSD

EDUCATION



MISSIONS

PORT

ECONOMIC DEVELOPMENT
RECREATIONAL OPPORTUNITIES



BOTH SERVE THE PUBLIC

BOTH HAVE PUBLIC INTEREST AT HEART

BOTH NEED & DESIRE PUBLIC'S CONFIDENCE

Port of Silverdale
Minutes of Regular Meeting
July 19, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Tim Knapp of TIKAR Services; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; John Kuntz of Olympic Outdoor Center (OOC); Ron Easterday of Rice Fergus Miller (RFM); Marvel and Randy Hunt of the Central Kitsap History Club; and Caleb Reese.

1.1. Agenda – *the agenda was approved* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The June 21, 2018 Regular meeting minutes were approved as corrected* (motion by Scholfield; second by Aus, unanimous).

3. PUBLIC COMMENT

3.1. Jim Knapp was not in attendance. An e-mail from Mr. Knapp dated June 27, 2018 had been forwarded to the Commissioners for review.

3.2. Carla Larson submitted the Certificate of Liability Insurance for the 2018 Whaling Days festival. She explained that Carl Johnson will be the Grand Marshall of this year's Whaling Days Parade. Closure of Port facilities was discussed. *It was agreed to close the Port vehicle parking lot (Byron Street and Washington Avenue) at midnight on Tuesday, July 24th, 2018 and to close the boat launch parking area and boat launch (end of McConnel) at 5:00PM on Thursday, July 26th, 2018 with both facilities remaining closed until Sunday, July 29th, 2018 at*

9:00PM (motion by Aus, second by Scholfield, unanimous).

Since the Port owns several of the properties along Lowell Street from Washington Avenue to the water Ms. Larson wanted to make sure that the Commissioners knew that along with several other streets in Old Town that area will be closed starting Friday, July 27th at 3:00PM through Sunday, July 29th at 9:00PM.

4. UNFINISHED BUSINESS

Phil suggested that since Ron Easterday of RFM was in attendance Item 4.4 Waterfront Master Plan be moved up on the agenda.

4.4. Waterfront Master Plan – Commissioner Greaves reported that he and Phil met with Steve Rice and other RFM personnel as well as Central Kitsap School District (CKSD) personnel. There was discussion about the Port and the CKSD working together to improve Old Town Silverdale from Linder Field to the water. RFM is assisting CKSD with the future of the Jenne-Wright building/property. CKSD's goal is to spend no more than two years on planning and to be totally out of the building within five years. The Old Town Silverdale Waterfront Public Survey results have been received. Phil said that the survey may need to be expanded to obtain user input regarding the area from the ballfields to the water. A joint meeting with the CKSD and Central Kitsap Community Council (CKCC) is tentatively set for August with the actual date still to be determined. It was suggested the Port hold a Special Meeting so that all three Commissioners may attend. Randy Hunt said that he attended an informal CKCC meeting whereat County Commissioner Ed Wolfe

Port of Silverdale – Minutes of Regular Meeting on July 19, 2018

assured everyone that the Old Town Pub was going to be demolished. It is unknown why Commissioner Wolfe would jump to that conclusion, especially since the Port has ordered a structural evaluation of the building and has yet to receive the results. Mr. Easterday said that the structural site investigation is scheduled to take place on July 24th so the structural evaluation report will be available for review and discussion at the August Port meeting. Commissioner Scholfield voiced his concern about there being no mention of non-motorized watercraft docks/facilities within the Port's Waterfront Master Plan yet now there is discussion about redeveloping a large part of Old Town that isn't even Port owned property. He was under the impression RFM was tasked with focusing on Port-owned properties mainly the properties south of Byron Street. Mr. Easterday agreed there has been a bit of a detour. One reason is the timing of CKSD working on plans to determine the future of the Jenne-Wright building/property. It just seemed to make sense for the Port and CKSD to work together as it is thought both projects could tie in together. One problem that the Old Town area faces is a lack of parking and there has been talk about the Jenne-Wright building/property possibly helping to solve that problem. Mr. Hunt provided a hand-out showing a property in Old Town Silverdale, 8870 Shore Place, currently the Silverdale Shores Apartments owned by Larry & Barbara Schwoch. He said that it would be a great location for a parking garage. Marvel Hunt added that the results from the Old Town Silverdale Waterfront Public Survey shows that the public doesn't want more parking on waterfront property. The area of the Silverdale Shores apartments is good because

it is out of view yet still close. Commissioner Scholfield asked if the property was for sale. Mr. and Mrs. Hunt were not sure. They gave credit to Kara Talbolt, who owns property in Old Town Silverdale, for coming up with the idea of having a parking garage at the Silverdale Shores Apartment property. The Hunts thought it was a good idea and was worth mentioning to the Port. Commissioner Scholfield said that the Monroe property along Washington Avenue would also be a good addition for the Port and should eventually be pursued. Mr. Easterday said that they will visit with Kitsap County Public Facility District (KCPFD) regarding funding opportunities. Commissioner Scholfield said that ballfields off of Clear Creek Road and Greaves Way is another project that should be discussed with KCPFD.

Commissioner Aus said that when he was in Old Town last week the sewage smell was undeniable. It was reminded that the County's Pump Station 3 public meeting is scheduled for Thursday, July 26th @ 6:30PM at the Silverdale Water District offices.

4.1. Port Programs

a. Sailing – the damaged motor description will be generated and then the motor will be listed with Washington State Department of Enterprise Services - Surplus Property Disposal. Steve Trunkey came into the Port office a couple of weeks ago to report that a license plate on one of the boat trailers was stolen. The Department of Licensing has since issued a new plate and it was reported to the Kitsap County Sheriff's Department.

Port of Silverdale – Minutes of Regular Meeting on July 19, 2018

b. Rowing – will participate in the Whaling Days festival by having a booth directly in front of Rowing Alley.

c. Non-motorized boat storage – Commissioner Scholfield reported that the new kayak rack has been assembled and is in the non-motorized boat storage area. There are currently fifteen boats/kayaks in the non-motorized area. Eventually spots will be marked off to make it easier to track who is in there as they will each be assigned a spot.

4.2. Port Facilities

a. Dredging – Phil reported that the Army Corps has requested dredging alternatives, which is creating additional work for Marine Surveys and Assessments (MSA), which will cost up to \$4500. In order to keep the project moving along Phil signed off on a change of scope agreement between MSA and the Port. Phil reminded everyone that since the new dredge didn't fit within the depth parameters of the original dredge the County requested a State Environmental Policy Act (SEPA) analysis. Phil told the County that a modification to the permit should be made stating it will not exceed the original depth parameter to avoid the time and costs associated with a SEPA analysis. Commissioner Scholfield said that if an additional non-motorized dock is eventually added to the Port's water facilities a SEPA analysis will be necessary and a deeper dredge could be planned for at that time. Commissioner Scholfield questioned if the Department of Natural Resources (DNR) has been contacted regarding extending the Port's water boundaries. Phil plans to contact DNR.

b. Dock maintenance – Tim reported that once he received the material for the new rub rails the project will begin.

Commissioner Aus said that he viewed the new ladders and is concerned that small children would have a hard time gripping onto them because of the size of the pipe used. Tim explained that the ladders were built to the specs outlined in the contract, but he could possibly add an additional grab holder to accommodate smaller hands.

c. Port rules/Windsock – Commissioner Scholfield has assembled the Port rules sign and it will be posted. The windsock that has the "No Wake" wording on it will be installed. Commissioner Scholfield suggested the Port install buoys that state "No Wake" around the Port's water parameter. Phil will work with Tim to determine the approximate boundary.

d. Port Dumpster that normally sits at the end of Washington has been moved to behind the Old Town Pub building.

e. Olympic Outdoor Center (OOC) – John Kuntz explained that completing the construction of the concession stand on the pier is taking a little longer than anticipated. He thanked the Commissioners for their patience. Unfortunately, \$200 worth of paint they were using for the stand was left out overnight and stolen. Mr. Kuntz said he has received many positive comments about the new structure although there have been some complaints that it blocks the view. He explained that the operation of the business has improved with the move to the pier. They plan to have a rack for paddles and life jackets placed near the

Port of Silverdale – Minutes of Regular Meeting on July 19, 2018

concession stand as this should help with customers just leaving items along the pier/docks. He is hoping the Commissioners would allow a section of the dock be set aside for kayak/paddle board entry and exit, which would also allow OOC to attach a Kayarm, which is used to help balance kayaks, to allow for easier access. Mr. Kuntz asked that the section of the dock used for this be on the south side near the gangway. Phil explained that a boater, Al Szymanski, who reports to the Waggoner Cruising Guide, came into the Port office recently and requested that that same area be reserved for boaters to use the pump out facility. Also, the Recreation Conservation Office (RCO) may have issue with that area being reserved for non-motorized watercraft as it may interfere with motorized boats. Since the Kayarm has a metal bar sticking out under the waterline, this would be a safety hazard to boaters. Commissioner Scholfield suggested an area on the south side of the dock – the sailboat float side – be reserved for non-motorized boat access. Commissioner Scholfield and Mr. Kuntz will meet and further discuss. Ms. Hunt explained that another item that she noticed from the Old Town Silverdale Waterfront Public Survey results was that many local kayakers would like a place to safely secure their kayaks to go on land for a few hours. Phil suggested something along the beach be considered and this may be something to add to the Waterfront Master Plan. Mr. Kuntz said that for the time being if a paddler comes in while his business is open OOC would be more than happy to assist them.

OOC has a three-year lease with the Port from June through September 2018, 2019, 2020.

There was question of the timeframe for dismantling the structure at the end of each season. Mr. Kuntz said that in order to move the structure it would have to be destroyed as it's all nailed together. It was explained to Mr. Kuntz that the Port was under the impression that the stand would be located on the pier just for the four-month duration each year. Mr. Kuntz said that there is a misunderstanding because that was not his intent. It was explained to him that according to the lease he would be required to pay just for the four months, but that doesn't seem adequate if the structure is there year-round. He said he would be willing to pay an agreed upon amount during the offseason, if required. It was agreed to continue this as an agenda item.

4.3. Port Properties

a. 9004 Washington/DRC – Phil and Mike met with Faithe Lester and Mary Hancock of the Dispute Resolution Center regarding their request to upgrade/update the building. Phil suggested to DRC personnel to consult with local architect, Carol Longmate Smiley, to help them determine exactly what they want as they were still a bit indecisive. Until all the details are decided upon by the DRC there cannot be a discussion about the new lease terms and the project cannot go out for bid.

b. 9020 Washington/Elizabeth's House of Wax – Mike reported that last he had heard Robison Plumbing was talking with the County to determine exactly where the sewer outlet for the building is located. They may be waiting until the upgrades along Washington Avenue are in place. Ms. Larson said that she has heard that project is on hold until 2020.

Port of Silverdale – Minutes of Regular Meeting on July 19, 2018

c. 3255 NW Lowell Street/Central Valley Daycare – Mike reported that the Occupancy Permit has been finalized. The tenants are now working with the State directly in order to obtain the necessary permits/license to operate a daycare.

d. 3423 NW Byron Street/Old Town Massage – it was thought the building's air conditioner needed to be replaced but it was repaired.

e. 3473 NW Byron Street – Commissioner Scholfield and Mike have received subpoenas to appear next week at Kitsap County Superior Court regarding the case of State of Washington vs. Trevor Ray Mercer (No. 18-1-00812-18). Mr. Mercer was taken into custody on May 25, 2018 as he was found in the vacant Old Town Pub building, apparently stripping anything of value from the building creating approximately \$8,000 worth of damage. Commissioner Scholfield and Mike were eye witnesses as they had seen Mr. Mercer near/on the premises prior to his arrest. Mike reported that the roof on the southeast corner of the building is deteriorating.

4.4. Waterfront Master Plan – had been discussed earlier in the meeting.

4.5. Pump Station 3 – a County-sponsored public meeting is scheduled for Thursday, July 26th at 6:30PM in the conference room at the Silverdale Water District. There is a possibility of a quorum of Silverdale Port Commissioners at that meeting. Although there will be no decisions made regarding Port business a Notice of Special Meeting will be posted on the Port office door prior to the meeting on July 26th.

4.6. Charles Lewis is the individual who was trespassed from Port property last month as he was threatening the public. After the trespass he returned to Port property and had been arrested. His boat was moored at the Port facility and incurred \$260 in moorage fees. Upon release from jail he was once again on Port property and once again arrested. His boat ended up beached on Port property but has since been anchored out in the water. Commissioner Scholfield reported that it has since been listed as a derelict vessel with the State and Kitsap County Sheriff is now responsible for it.

4.7. 4th of July Commissioner Scholfield reported that there was a small crowd at the Port on the 4th. Tim said that he and his crew once again passed out garbage bags and wished everyone a Happy 4th and asked that they clean up after themselves. He again was very impressed with the willingness of the crowd and how well they policed themselves. A six-yard dumpster was packed full with fireworks debris. Tim did notice that the two Safe Security guards were not separated as in years past and not very involved with directing the crowd to the designated area to safely shoot off fireworks. Tim talked to them about it when he saw they were more busy talking amongst themselves. In the past one guard is normally dedicated to be on the pier and another in the parking lot and grassy area. Next year the request to Safe Security will be more detailed. Commissioner Scholfield added that he has noticed that the weekend Port Host guards spend a lot of time in their vehicles. Both issues will be mentioned to Jason, the Safe Security supervisor.

Port of Silverdale – Minutes of Regular Meeting on July 19, 2018

4.8. Dockwa – it has been verified that reservations are required to use this system.

4.9. Ballfields near Clear Creek Road and Greaves Way were discussed. Commissioner Scholfield clarified that he would like to talk with Mike Walton of the Kitsap Public Facilities District and explain to him that the Port would be in support of such a project. It wouldn't actually be a Port project.

NEW BUSINESS

5.1. Waggoner Cruising Guide – when Mr. Szymanski stopped in at the Port office, besides asking for an area of dock be reserved to allow for boats to easily access the pump-out, he asked how he can obtain the code for the showers. Commissioner Scholfield explained that the showers are open from 5:00AM through 9:00PM Saturday and Sunday. Other than that, if a boater needs the shower during the week they can call the office and be provided the code.

5.2. Commissioner Per Diem has increased from \$114 to \$128 per meeting/day.

5.3. Budget Report was reviewed.

5.4. Small Ports Seminar sponsored by the Washington Public Ports Association (WPPA) is scheduled for October 18th in Leavenworth, WA, which is a regularly scheduled Port meeting. It was agreed that the October meeting could be moved from the 18th to the 16th to allow Port Commissioners and staff to attend the seminar in Leavenworth.

5.5. Kitsap All Ports meeting is scheduled for Monday, July 30th at the Port of Brownsville. A potluck style barbeque will start at 5:00PM with the meeting to follow promptly at 6:30PM at the Brownsville Yacht Club meeting room.

6. SAFETY – nothing to report.

Commissioner Scholfield reported that he ordered barriers that attach to traffic cones so that the Port host can more effectively cone off areas to keep open for vehicles with boat trailers.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$31,846.52, checks numbering 11812 through 11840 and Electronic Transfer 2018-07 to the U.S. Treasury in the amount of \$1,645.62, were approved (motion by Aus, second by Scholfield, unanimous).

8. EXECUTIVE SESSION – None

9. ADJOURN

The meeting adjourned at 8:48PM.

Approved: ,


Henry Aus, Commissioner

Lawrence Greaves, Commissioner


Ed Scholfield, Commissioner



PUMP STATION 3 UPGRADES PUBLIC MEETING JULY 26, 2018



Kitsap County Department of Public Works
Sewer Utility Division



AGENDA

Introductions

Project
Overview

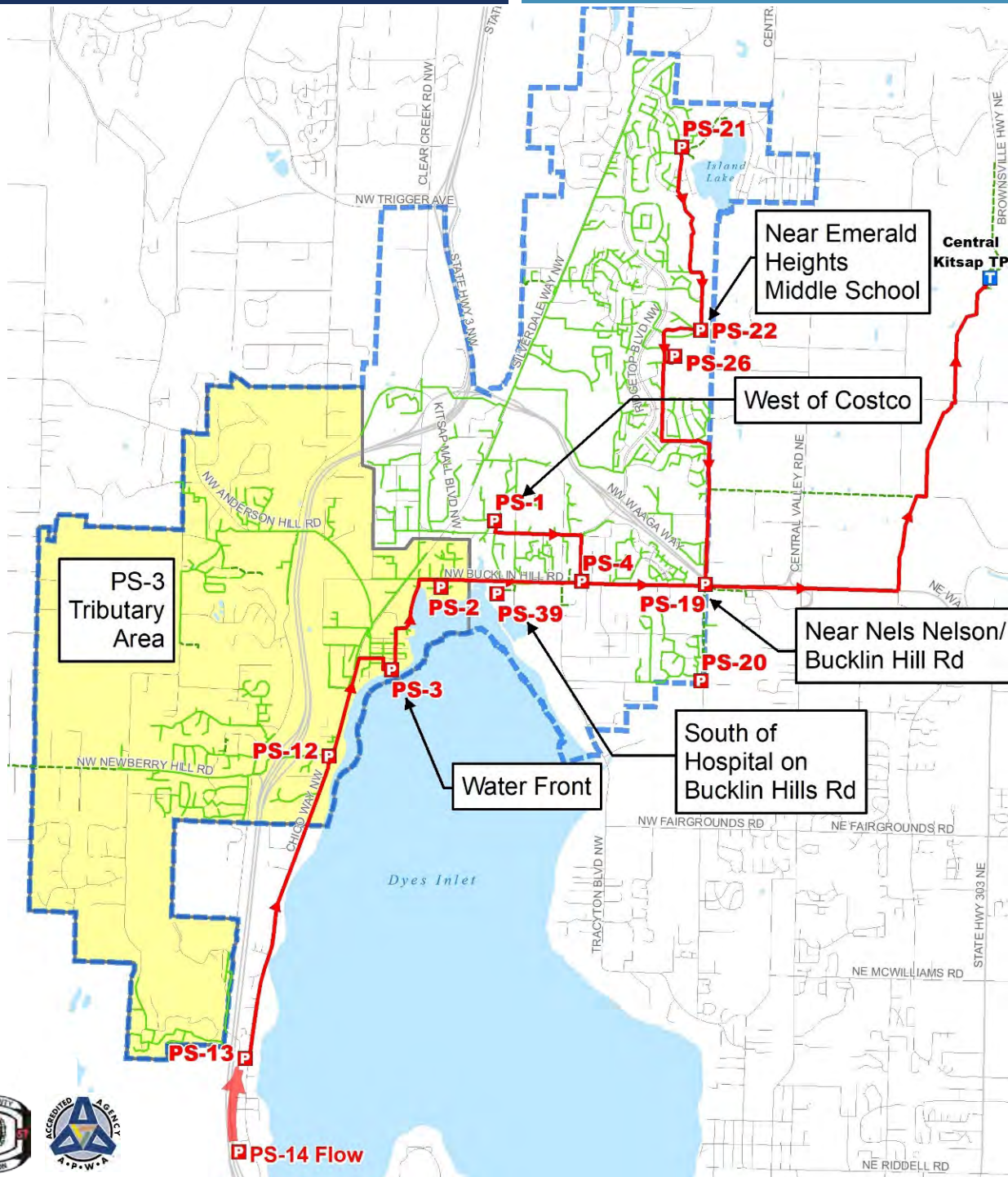
Project
History

Public Survey
Results

Proposed
Upgrades

Comments,
Questions,
and Next
Steps

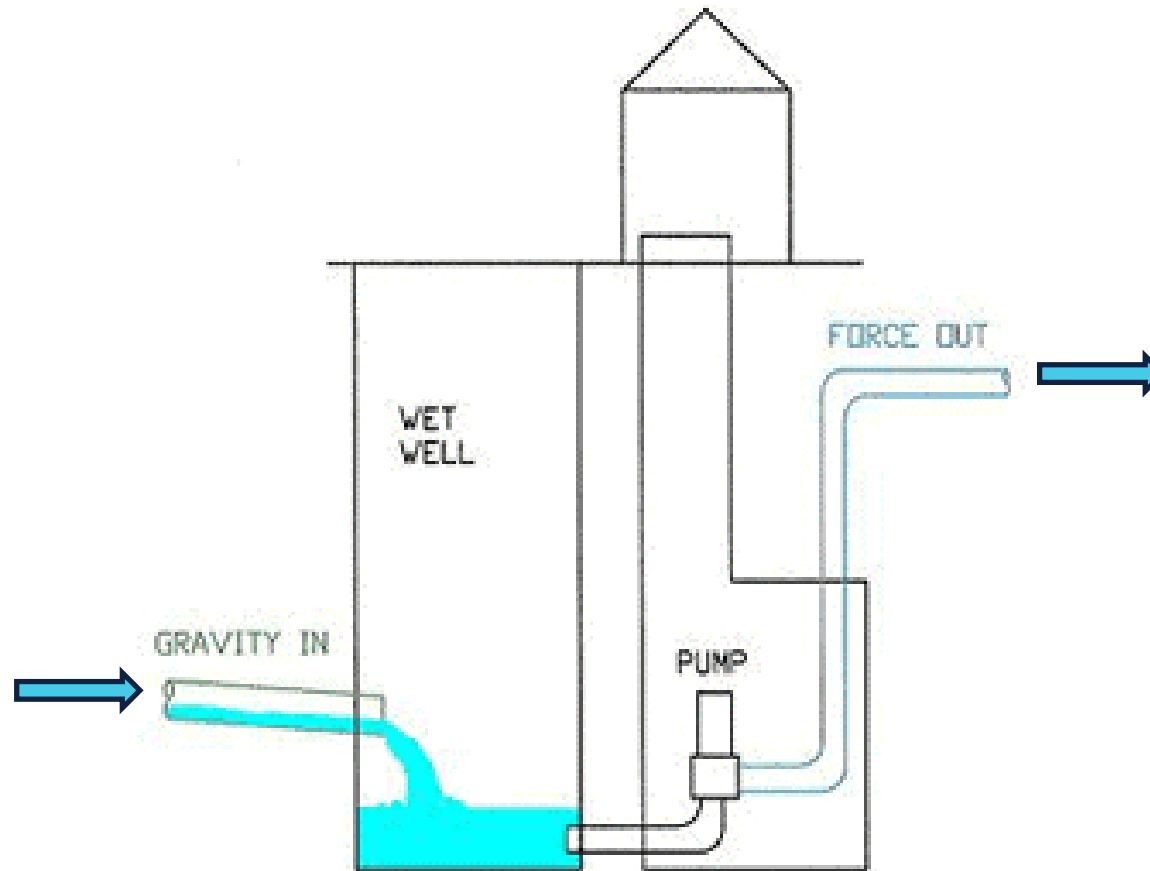




PROJECT OVERVIEW



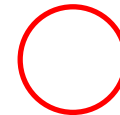
PUMP STATION OPERATION



PUMP STATION 3 – BACKGROUND

Old Town Silverdale

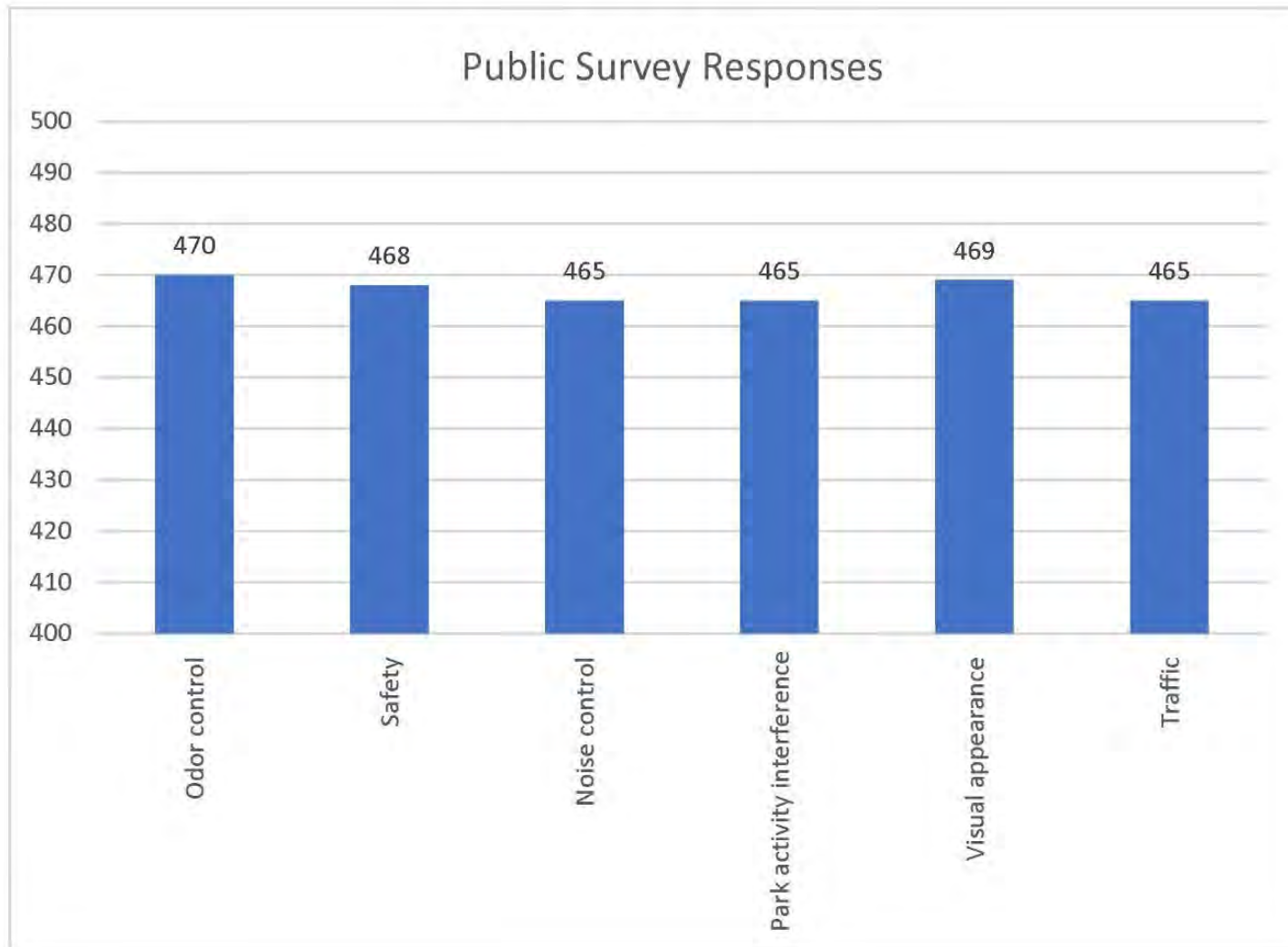
- Sewage Treatment Plant Prior to 1981
- Year Built: 1981
- Upgraded: 2005
- Capacity Limitations
- Obsolete Equipment



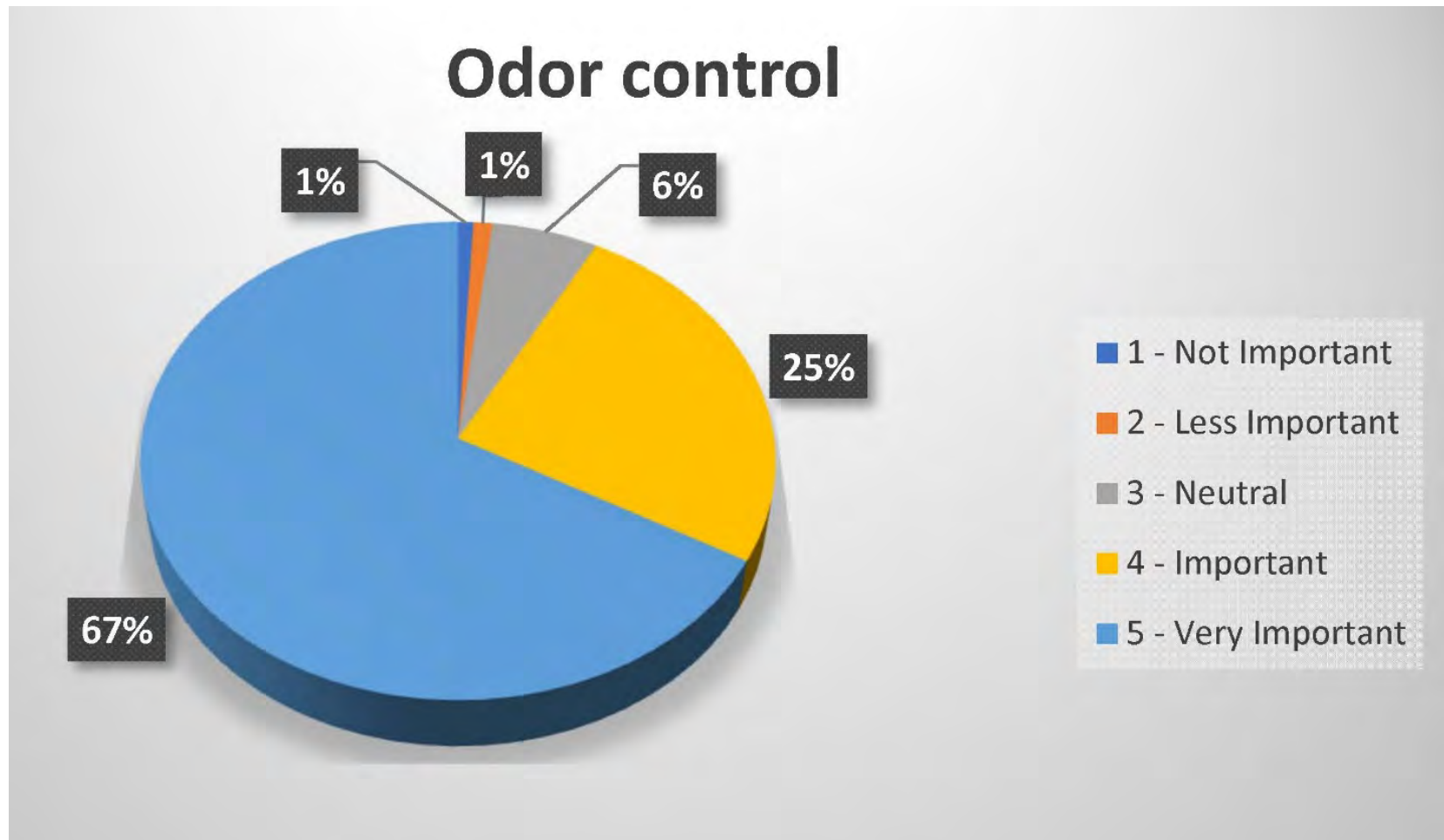
SITE EVALUATIONS – MID 2015



PUBLIC SURVEY RESULTS



PUBLIC SURVEY RESULTS



Weighted Average = 4.56

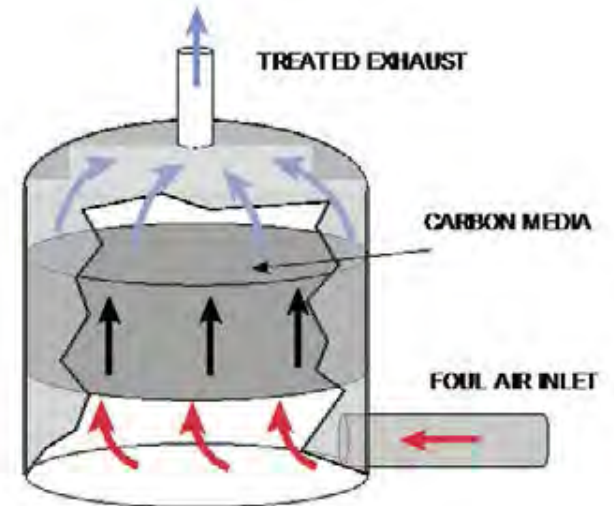
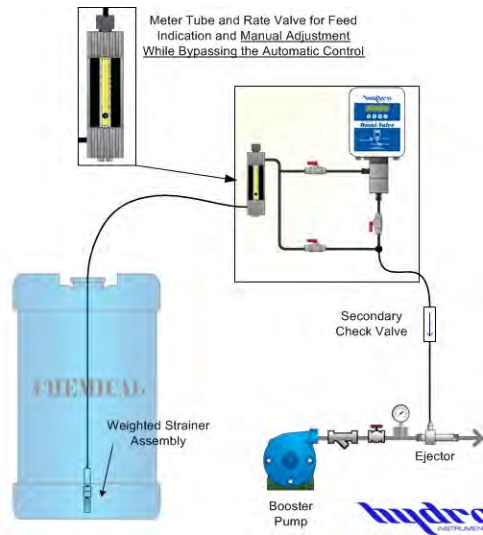
HOW CAN WE ADDRESS ODORS?



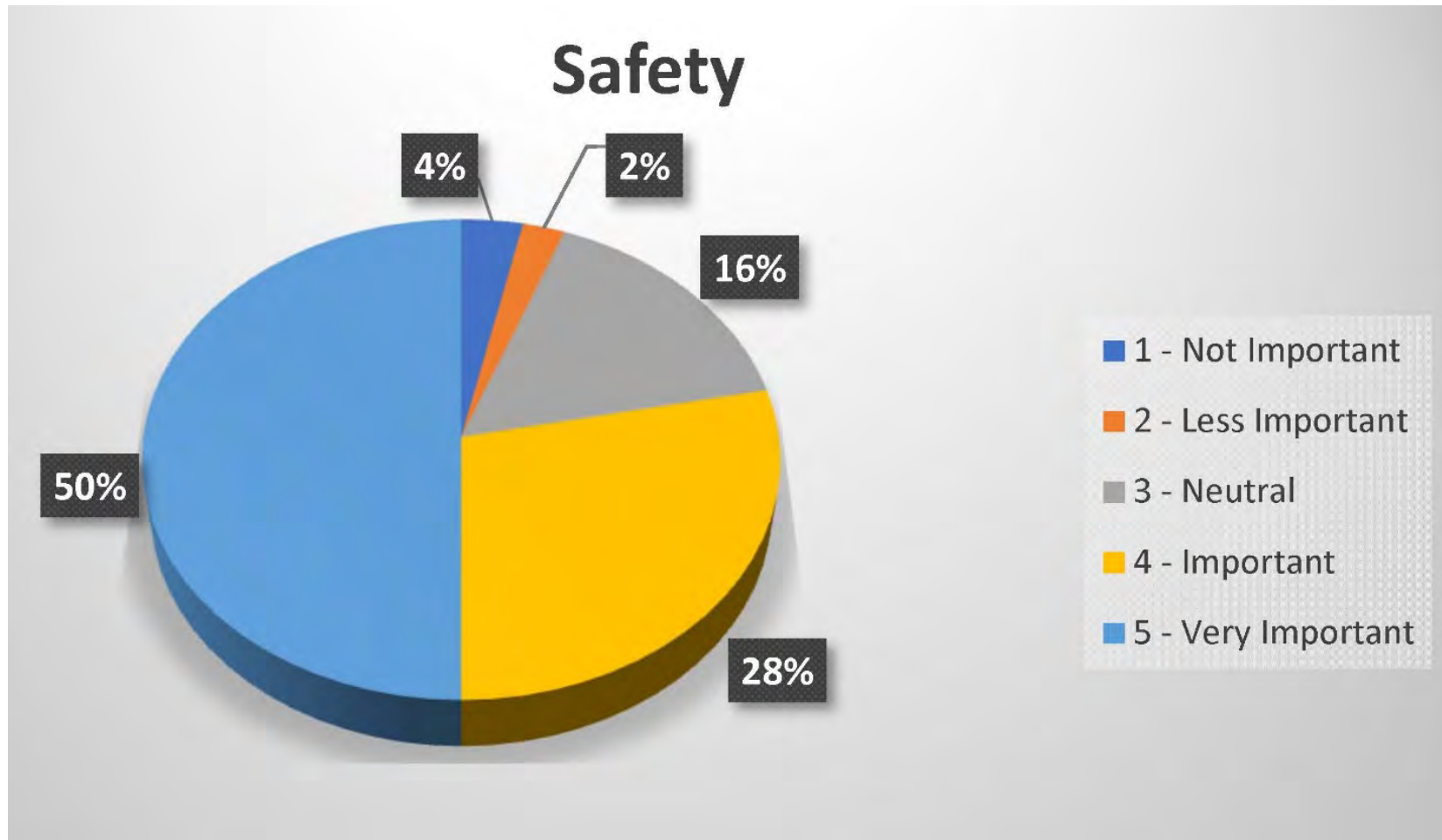
Chemical Addition



Air Scrubbers



PUBLIC SURVEY RESULTS



Weighted Average = 4.19



SAFETY – ACCESS & CONTROLS



Before



After

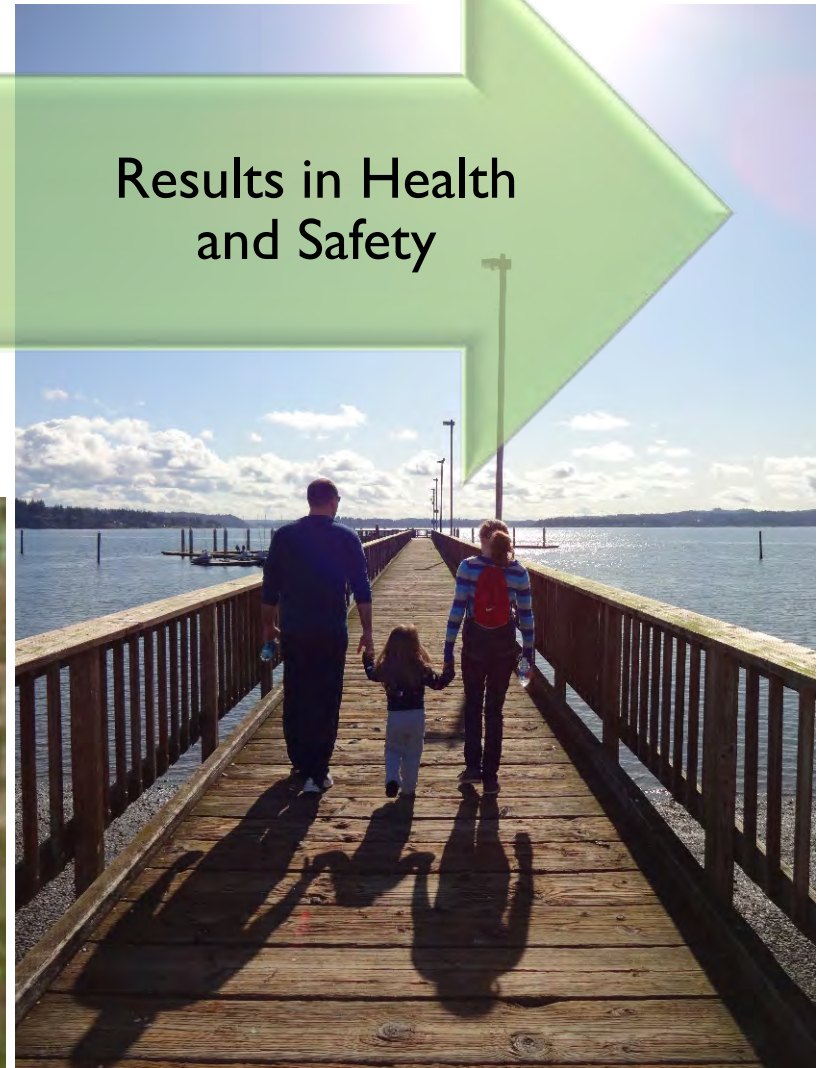
SAFETY – AVOIDING SPILLS



Designing
for future
flows

Avoids
Sewer
Overflows

Results in Health
and Safety



SAFETY – AVOIDING SPILLS



Proper Container and Anchorage

Clear Label

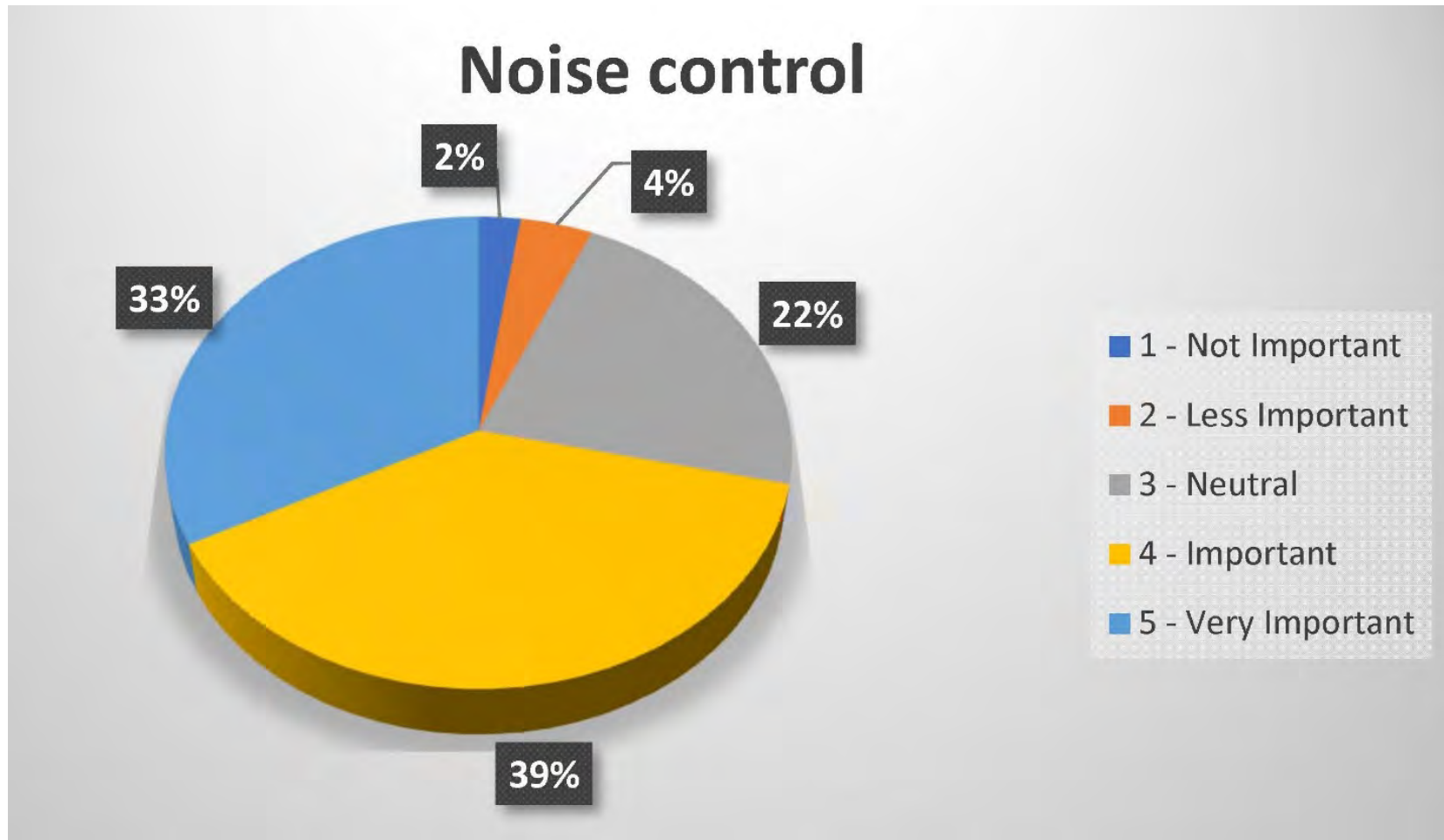
Clean Work Area,
free from Obstructions

✓ Routine Equipment
Inspection and
Maintenance

✓ Verify Compatible
Chemical Storage



PUBLIC SURVEY RESULTS



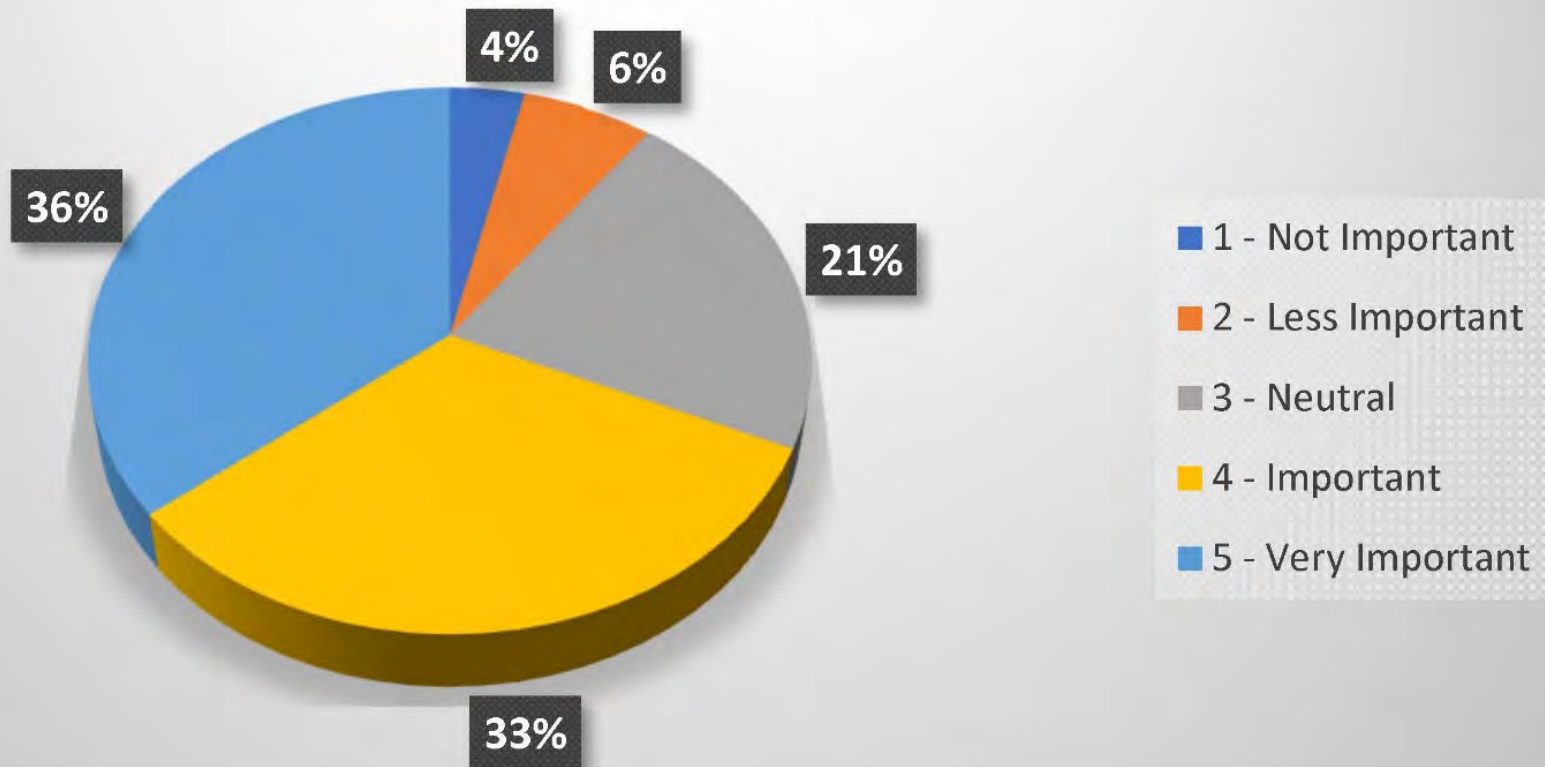
Weighted Average = 3.95

HOW WILL NOISE BE ADDRESSED?

Noise Source	Solution
<ul style="list-style-type: none">• Pumps	<ul style="list-style-type: none">• Locate below ground
<ul style="list-style-type: none">• Electrical Equipment/Panels	<ul style="list-style-type: none">• Locate inside building
<ul style="list-style-type: none">• Generator	<ul style="list-style-type: none">• Muffle exhaust• Locate in sound attenuated room in building
<ul style="list-style-type: none">• Odor control facilities	<ul style="list-style-type: none">• Locate inside building• Vent through roof
<ul style="list-style-type: none">• Ventilation fans	<ul style="list-style-type: none">• Locate inside building
<ul style="list-style-type: none">• Vehicular activity for maintenance	<ul style="list-style-type: none">• Muffled equipment• Normal maintenance during daylight hours

PUBLIC SURVEY RESULTS

Park activity interference



Weighted Average = 3.90

PROJECT SETTING

- Multi-use property
- Highly visible
- Scenic area



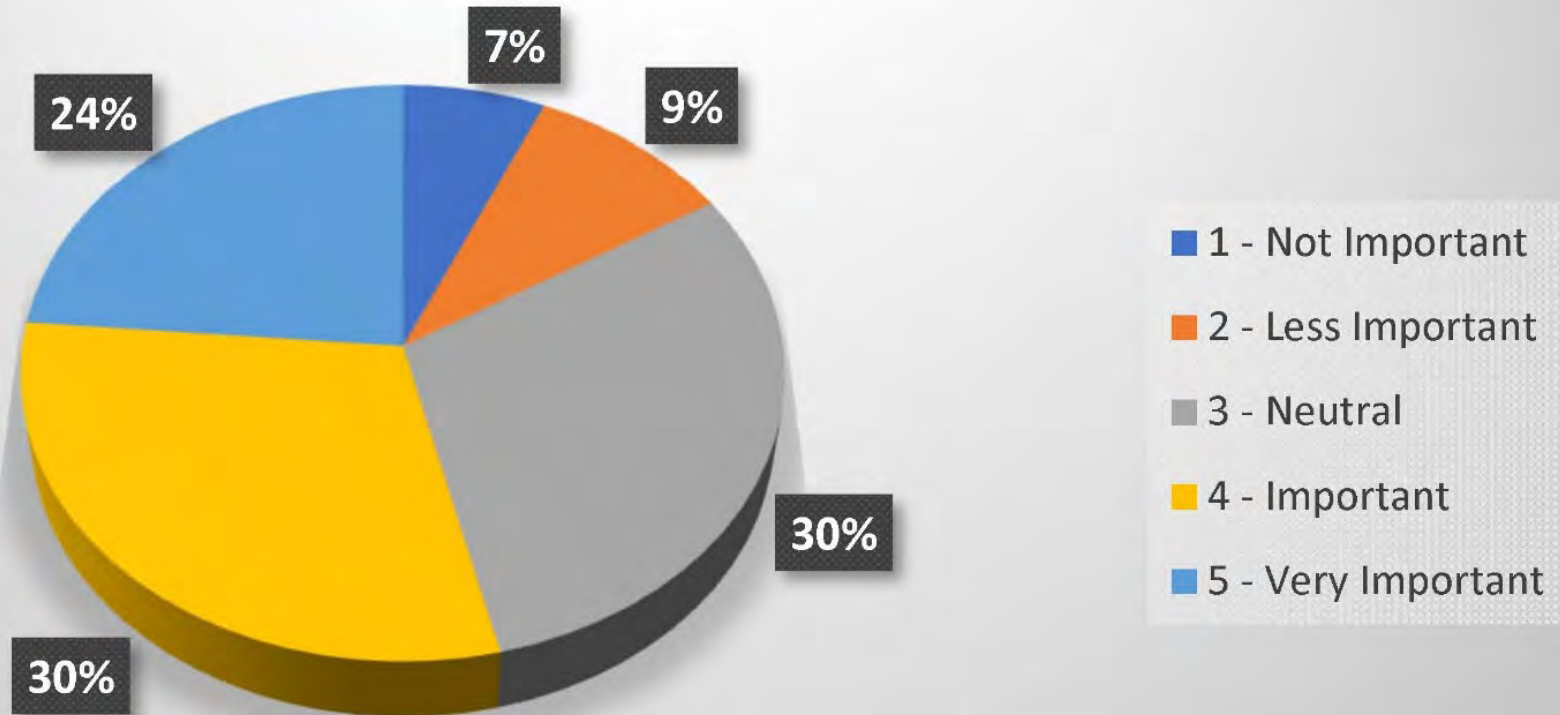


MITIGATING IMPACTS O&M ACTIVITIES



PUBLIC SURVEY RESULTS

Visual appearance



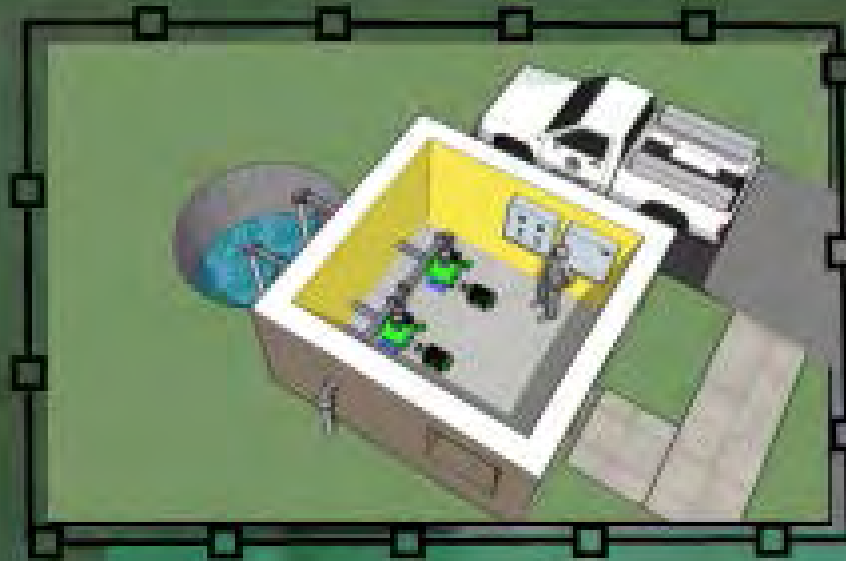
Weighted Average = 3.55

ADDRESSING VISUAL APPEARANCE

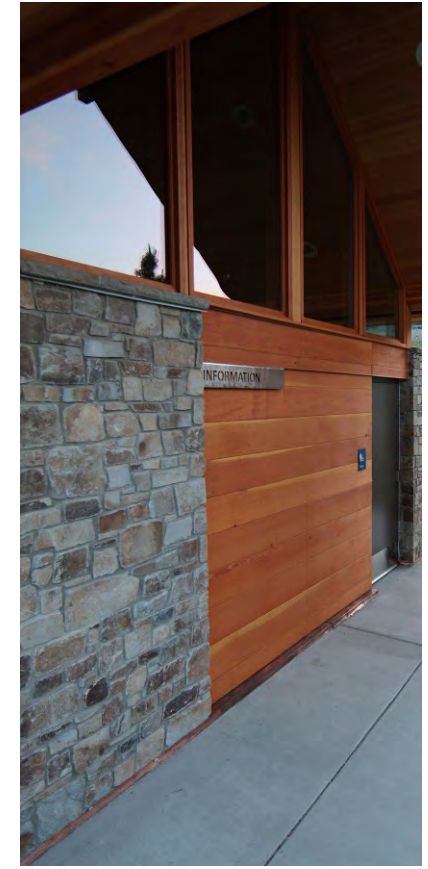
- Smaller building with fence (shown)

OR

- Large building enclosure

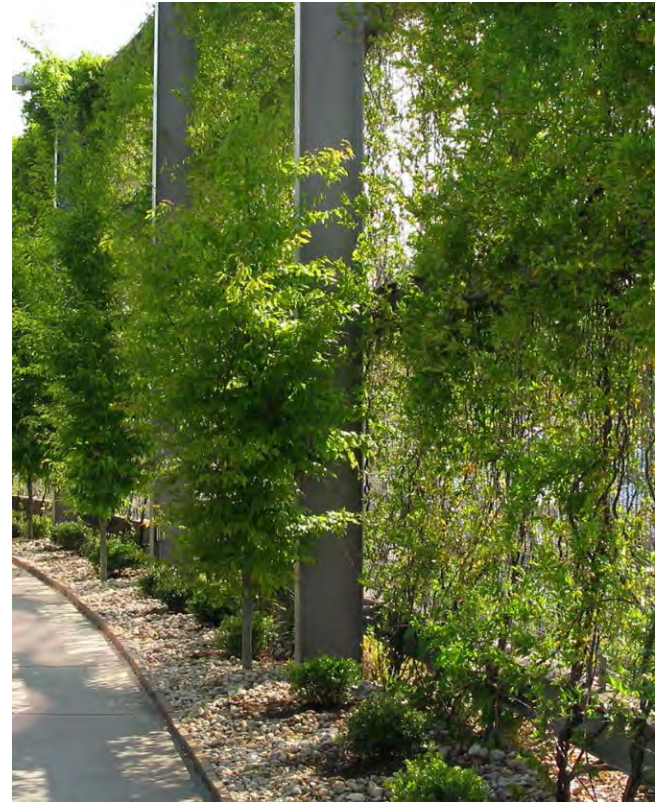


Future Landscaping & Screening



CREATIVE BUILDING MATERIALS TO BLEND INTO PARK

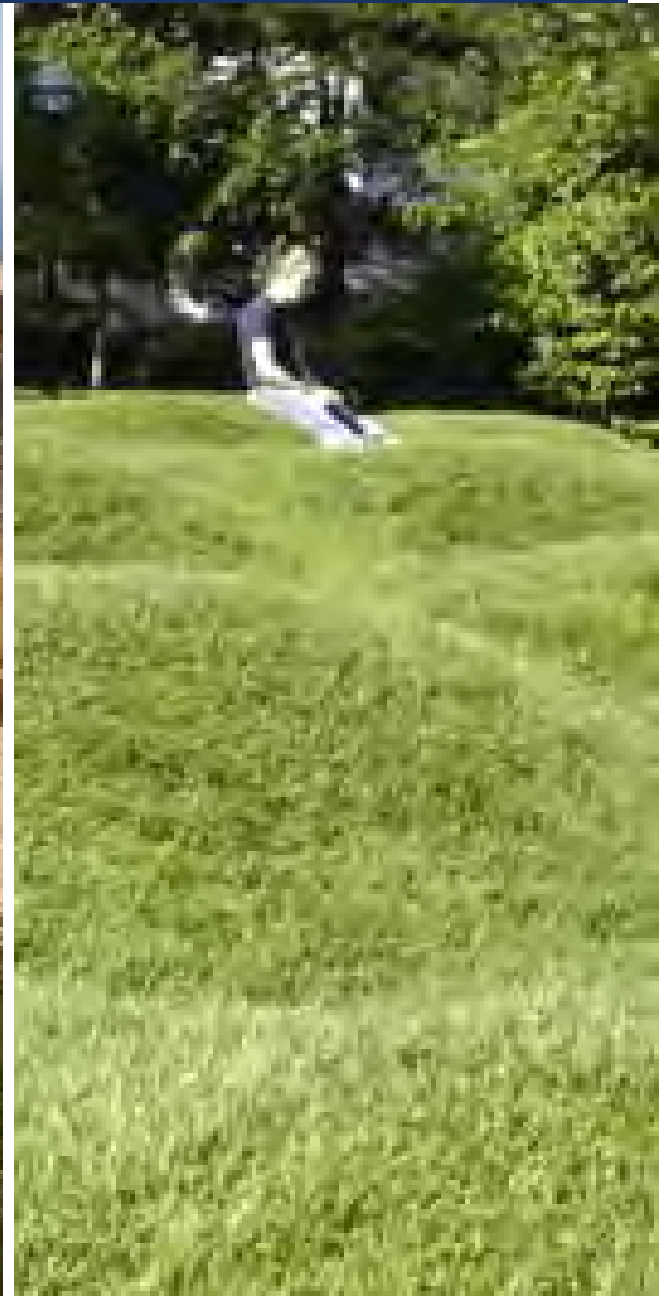




SCREEN WITH TREES, SHRUBS, & OTHER PLANTS

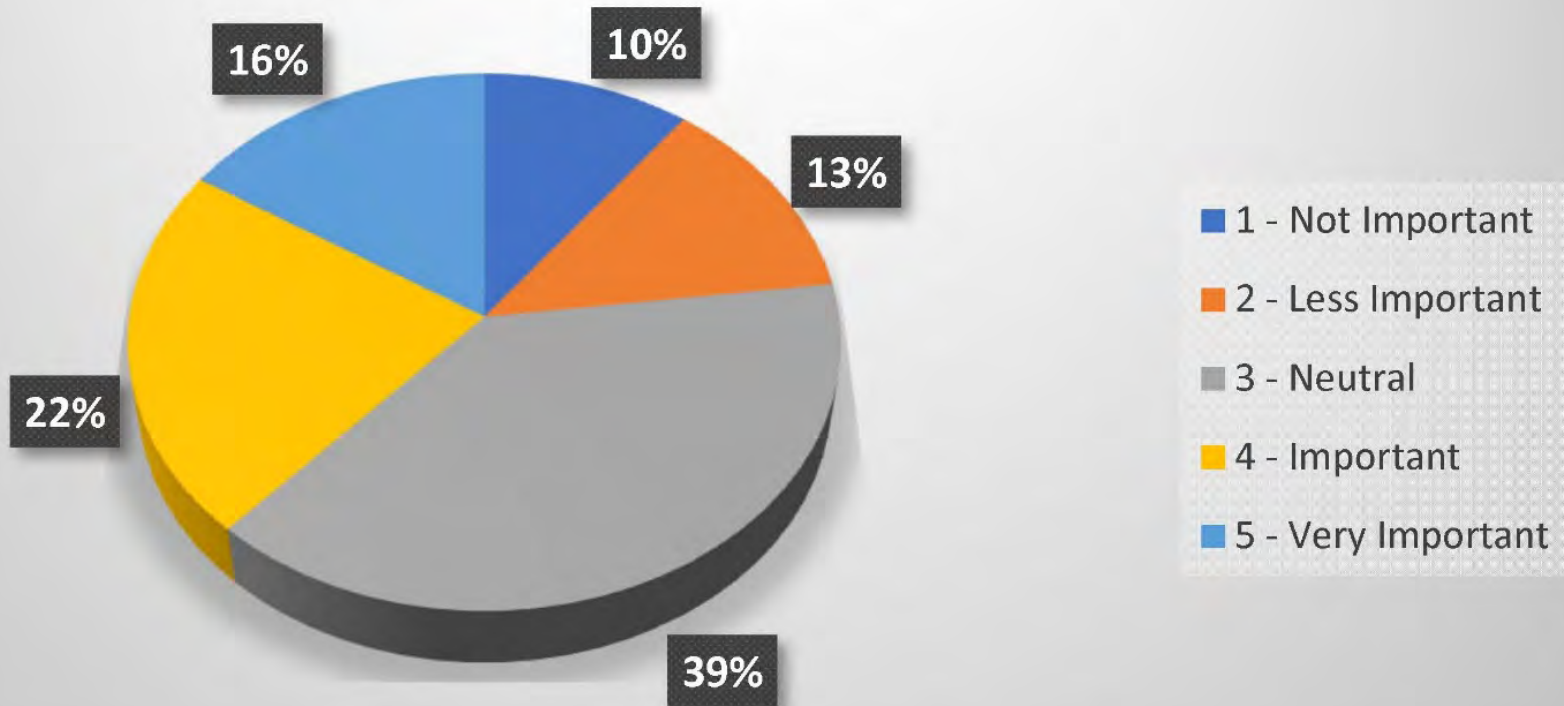


SHIELD WITH CONTOURS



PUBLIC SURVEY RESULTS

Traffic



Weighted Average = 3.21

MITIGATING TRAFFIC IMPACTS

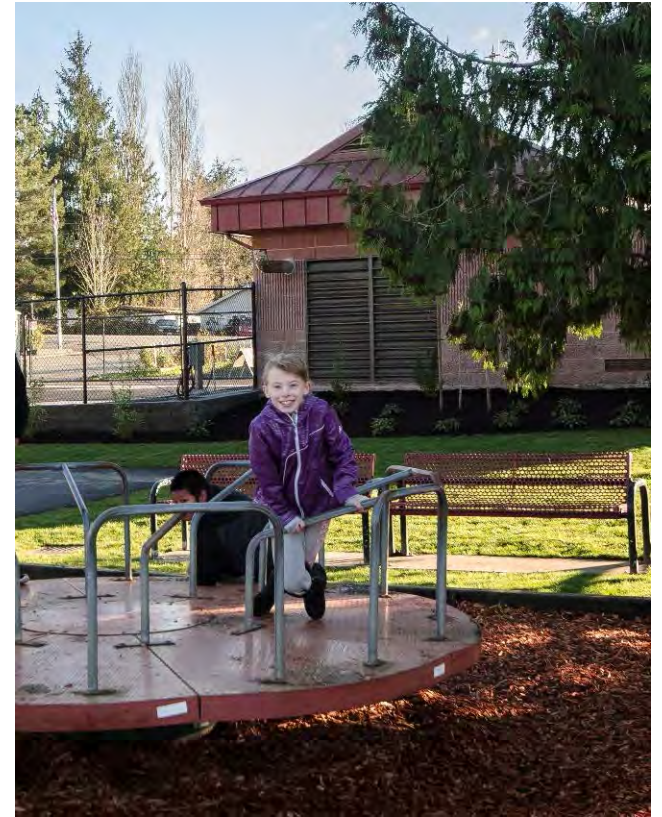
- Bayshore Upgrades
 - Traffic Circle
 - Upgraded Intersections
 - New Driveway Ramps, Curbs, & Sidewalks
 - Improved Parking Layout
- Pump Station Upgrades
 - Onsite Parking for O&M Vehicles



PROPOSED UPGRADES

- Visually Appealing Building
- Larger Capacity Pumps
- Odor Control System
- Landscape to Blend





KEYPORT





CITY OF SNOHOMISH

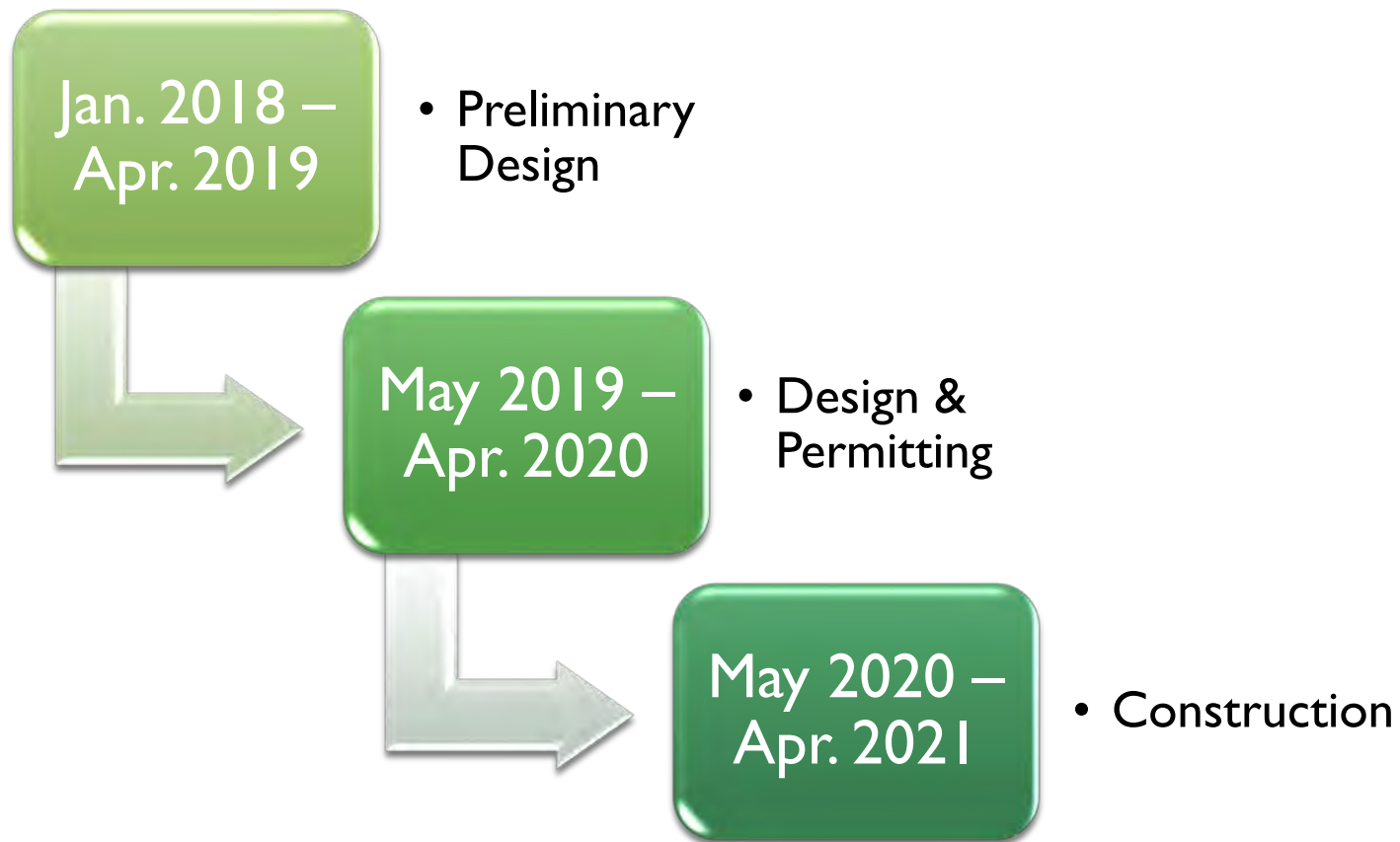




TOWN OF ENTIAT



PROJECT SCHEDULE



COMMENTS OR QUESTIONS



For more information, contact:

Project Contact:	Barbara Zaroff
Education/Outreach:	Lisa Horanyi
Project Website:	kcowa.us/ps3



Kitsap County Department of Public Works
Sewer Utility Division



Port of Silverdale – Minutes of Regular Meeting on August 16, 2018

Port of Silverdale
Minutes of Regular Meeting
August 16, 2018

1. CALL TO ORDER

Commission chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Bridget Burke of Clam Island Rowing (CIR); Steve Rice of Rice Fergus Miller (RFM); Marvel and Randy Hunt of the Central Kitsap History Club; and Caleb Reese.

1.1. Agenda – *the agenda was approved* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The July 19, 2018 Regular meeting minutes and the July 30, 2018 Kitsap All Ports meeting minutes were approved as submitted* (motion by Aus; second by Scholfield, unanimous).

Commissioner Greaves' absence was excused (motion by Scholfield; second by Aus, unanimous).

3. PUBLIC COMMENT – it was decided to allow Bridget Burke to be first on the agenda as she had a time constraint.

Ms. Burke reported that CIR is planning to install a hose rack on the side of the Port building in rowing alley. Commissioner Scholfield requested Ms. Burke obtain permission from the landlord prior to installing the rack since it will be fastened to the building.

Ms. Burke met with Commissioner Scholfield and Theresa at the non-motorized boat storage area just prior to tonight's meeting. She explained that it is a very successful Port program and the Commissioners should be proud of that. Soon there will be two more

canoes and two double kayaks, which will bring the storage area to full capacity. Ms. Burke said that she has been volunteering since its inception in 2013 and feels it is time for her to step down and find someone else to fill her position, but before she goes she would like to see the program formalized by creating actual spaces in the area and mapping it out, so that it is clear who rents which space. Ms. Burke will order a new sign for the non-motorized storage area that lists the Port's phone number. Commissioner Scholfield will order and/or with Mike's help make numbers for the spaces. Ms. Burke requested the Port clean up the underutilized lot that is adjacent to Kitsap Sailing and Rowing Foundation's (KSRF) lot and across the street from the Port office. It has several of the large tubs with Styrofoam that were used underneath the floats, which have met their life expectancy for the Port's use. There is other Port material in the lot as well. Commissioner Scholfield explained that the Port needs a place to store items and also needs a staging area. Since the back of the Port office was cleaned out to accommodate CIR's fence extension many of those items were moved to that lot. He suggested KSRF and CIR clean up the lot since they are already storing a rowing shell and several small boats in there without Port approval. This also was the case for the sails that are now stored in the upper area in the Port's shop. Ms. Burke agreed that they were not being very good partners by not communicating with the Port prior to utilizing the space. It's just that they are running out of space, which really is a good thing as it shows the Port-sponsored programs are successful and growing. She sees an opportunity to not just clean up the lot across the street, but make it available for the programs and other Port storage. She said it would be great if the Port could help them in cleaning it up. There is a large mound of dirt and several of the large dock tubs that are limiting the usable space in the lot. Ms. Burke asked that the bins be removed, which would

Port of Silverdale – Minutes of Regular Meeting on August 16, 2018

not only add a lot of additional space, but it would also rid the eyesore along Old Town's main thoroughfare. Selling the bins on Craigslist was discussed; however, they have very little to no value as there are holes in them. It was agreed it may be best to take them to the dump. Commissioner Scholfield explained that at times the photography company, which is located next to the Port office, needs a place to park their trailer. He allowed them to park it in that lot as it benefits the community by freeing up parking along the street. If the Port doesn't have an on-site area for staging then off-site storage fees could be incurred, so it is important for the Port not to lose that lot across the street to its programs. Ms. Burke explained that if that mound of dirt was leveled out and tubs removed and other items rearranged, she believes there would be enough space for both the programs and the Port. Commissioner Scholfield explained that if it were a shared area the Port would require the cement slab to be available for Port use at all times, which could pose a problem as it would block the entry gate. Ms. Burke suggested that in that case the Port contact CIR and KSRF prior to items being delivered or stored there. Commissioner Scholfield said that the onus should not be on the Port to do that as there will be times when it is impossible to know exactly if/when things are being dropped off there. KSRF and CIR would need to plan accordingly and not store essential items in that lot and be sure not to encroach on the Port's staging area. Commissioner Scholfield explained that Tim will be using heavy equipment on August 29th to clean out underneath the boat ramp. Tim will be asked to also use the equipment to level out the mound of dirt in the lot, if time permits.

Ms. Burke reported that CIR held two weeks of teen summer camp. The first week was too windy to row, but the second week was fantastic. Also, twenty-two middle-schoolers received their Physical Education (PE) credit in

July by attending CIR Summer Rowing Camp. It was a very positive experience.

3.1. Waterfront Master Plan – Steve Rice updated everyone on RFM's efforts regarding the updated Waterfront Master Plan. RFM is rethinking their initial ideas by scaling down that vision as parking will dictate the size of the facilities. Reid Middleton's structural evaluation of the Old Town Pub was reviewed. Mr. Rice explained that the building would have to be raised in order to accommodate a new foundation and footing system. Basically, it requires a total structural rebuild, which is not totally a surprise considering the age of the building. It would be renovated from the bottom up and the final product would be a new building with the existing beams. Mr. Rice explained that they do not have actual numbers but from experience RFM believes the labor intensity of such a project would end up costing more than a demo and rebuild. The next step if the Commissioners are considering the renovation would be to order a cost estimate report, which will cost anywhere from \$3,000 to \$5,000. Randy Hunt explained that there are State grants available for historical building renovations. The building isn't currently on the State or County historic building registry, but Mr. Hunt said that is because no one ever pursued it.

It could be significant for the community. It was explained that renovating the building would limit what can be done to it compared new construction. It was questioned that if all the required renovations were made what would it actually be in the end. Marvel Hunt said that the big box stores and the mall show the newer Silverdale, but there is so much history in Old Town. It is basically the only area of Silverdale

that actually shows the Silverdale of yesteryear. Ms. Hunt asked if the Port Commissioners are really interested in saving Old Town. She said that the water is a big draw to this area, but there needs to be more. Mr. Hunt said that he doesn't want another Old Town Pub. Back in the day after it was Emel's livery stable the building was turned into a restaurant. The Port now has the opportunity to renovate it into something for the community. Phil said that even if it were to be rebuilt as a waterfront facility the history can still be saved and passed on by showcasing historic pictures of the area along with written history. Ms. Hunt said that Mr. Hunt would love to see it renovated, but she doesn't mind if it were to be rebuilt as long as it is designed with an Old Town feel. She has heard from many people that the new construction of the Silverdale Auto Works is out of touch with Old Town and she hopes the Commissioners will take that into consideration if the decision is to rebuild.

4. UNFINISHED BUSINESS

4.1. Pump Station 3 – Mr. Rice said that he was happy to see Commissioners Aus and Scholfield at the County sponsored pump station meeting held on July 26th. It was agreed that the meeting was more of an informative meeting, where the County communicated what they have planned for the pump station upgrade not necessarily asking for input. Mr. Rice said that the County needs a vision for the waterfront and until then they are kind of stuck on the idea of just upgrading the building, but there is such an opportunity to make something so much better. One point that Mr. Rice found encouraging from the meeting was that the County said they were willing to work

with the Port. A joint facility could be a win/win for everyone involved including the public. He provided pictures of Skansie Park in Gig Harbor. It is a new dual use facility of a sewer lift station and restroom facility. There is a viewing platform from the top. It is a great example of creative thinking, which is what is needed for the Silverdale pump station. Mr. Rice offered to reach out to the County and attempt to set up a meeting to further discuss the pump station 3 upgrade.

Commissioner Greaves and Phil attended meetings with Mr. Rice and Central Kitsap School District (CKSD) personnel. Most of the CKSD staff at the Jenne-Wright building have moved to their new facility off of McWilliams Road. CKSD is working with RFM to determine what to do with the building and property once they are totally moved. They are interested in what the Port is doing in hopes to work together to better the Old Town area. The next meeting is scheduled for August 23rd. It is thought Commissioner Greaves may be planning on attending along with Phil.

Mr. Rice requested a meeting be held on September 12th at 2:30PM at the Port office so that RFM personnel can provide the Commissioners with an update on the Port's Waterfront Master Plan. The Commissioners agreed. Since more than once Commissioner will be attending this meeting it will be a Special Meeting and notice will be placed on the door accordingly.

Ms. Hunt asked if the Commissioners decided to move forward with ordering a cost estimate report for the Old Town Pub. The Commissioners decided to table the item until

Port of Silverdale – Minutes of Regular Meeting on August 16, 2018

next month's meeting when all three Commissioners are present.

4.2. Port Programs

a. Sailing – the damaged motor description will be generated and then the motor will be listed with Washington State Department of Enterprise Services - Surplus Property Disposal.

b. Rowing – discussed under Public Comment.

c. Non-motorized boat storage – discussed under Public Comment

4.2. Port Facilities

a. Dredging – Phil reported that with the Army Corps' request for dredging alternatives both Marine Surveys and Assessments (MSA) and Coastal Geological Services (CGS) are providing additional work for the Port. CGS submitted an invoice in the amount of \$3,075 which is being paid tonight.

b. Dock maintenance – the rub rails have been installed. There are two areas where Tim plans to replace rotten wood and secure the new rub rail, but other than that it is complete. Lester Burk, a boater who frequently moors at the Port, contacted the Port to report that the maintenance crew at the dock damaged his boat. Tim is working with Mr. Burk directly on this although disagrees as the timing of the damage doesn't align with when his work crew was on the docks.

c. Port Rules/Windsock – once the water boundaries are determined Tim will be working on placing "No Wake" buoys out in the Port's outer waters. There was a complaint from a

boater that was moored at the facility about an incoming boater that created a large wake when coming into the Port facility in "plow" mode. It really rocked his boat so he confronted the other boater, which apparently didn't go well as he later left a message on the Port's message machine concerned that he might be vandalized sometime in the night. No vandalism was reported. Other boaters came into the Port office to also complain about boats creating wakes and was happy to hear that the Port was considering installing buoys with the wording "No Wake". They complained that after hours the docks are busy with teenagers/young adults. They questioned if the security company is aware that non-boaters should not be on the docks after 10:00PM. They were also concerned about individuals swimming in the motorized boating area. It was explained that the security guard randomly patrols the docks after hours and if he witnesses anyone not following the Port rules (i.e. non-boaters on the docks after hours or individuals swimming in the moorage facility) he/she will ask the individuals to comply to the rules. If individuals refuse the security guard will call the sheriff, which could and has led to individuals being trespassed.

d. Olympic Outdoor Center (OOC) – at last month's meeting it was determined there was miscommunication in regards to OOC's use of the pier to hold its concession stand. Port Commissioners thought the structure would be erected in June and removed at the end of the season in September for 2018, 2019 and 2020. At last month's meeting OOC's owner, John Kuntz, said that he intended for the structure to be there year-round through the life of his three-year lease, although he only would pay

Port of Silverdale – Minutes of Regular Meeting on August 16, 2018

for four months out of the year.

Commissioners discussed charging OOC the monthly rate, while the structure remains on Port property. Mr. Kuntz is currently out of the State until mid-October. He will be asked to attend an upcoming Port meeting.

4.4. Port Properties

a. 9004 Washington/Dispute Resolution Center (DRC) – there has been no word from DRC personnel regarding their request to upgrade/update the building. Commissioner Scholfield was approached by the owners of Silverdale Autoworks, the adjacent property owner, asking if the Port would be willing to share the costs to pave a portion of the alley between the two properties. Phil said that may be a problem as the Port is required to follow the Public Works laws and use the Small Works Roster. Commissioner Scholfield agreed.

b. 9020 Washington/Elizabeth's House of Wax – Mike said that the last he had heard Robison Plumbing contacted the County asking for a copy of the video recording that would show exactly where the sewer outlet for the property is located.

c. 3255 NW Lowell Street/Central Valley Daycare – there is a credit of ^{479.91} on MTV's invoice this month. It is a refund for the permit fees that the Port had originally reimbursed to MTV, but should have been paid by the tenant. Silverdale Water had reported a possible water leak at the property; it has since been fixed. The tenants are working to obtain necessary State permits to run the daycare.

d. 3473 NW Byron Street/Old Town Massage – Mike reported that the southeast corner of the building is quickly deteriorating. Commissioner Scholfield reported that Trevor Mercer, the individual taken into custody after being found on the premises, made bail, but has failed to attend his court appearance.

4.5. Department of Natural Resources (DNR) – Phil will be working on requesting an expansion of the Port's outer water boundaries.

4.6. Whaling Days ^{4.6.0} *it was agreed to pay Boy Scout Troop 1540 \$800 for their cleanup efforts during and after Whaling Days 2018* (motion by Aus, second by Scholfield, unanimous).

^{4.6.0} An e-mail dated August 1, 2018 was received from Carley Bayer. Ms. Bayer claims to have sustained a broken bone in her foot, when she stepped onto the wheel track of the Port's gangway. She states that she was not able to see the wheel track of the ramp because it was "pitch black" at the bottom of the ramp. She requested compensation for her medical bills and lost wages due to the injury. The Port's insurance company, Enduris was notified. A letter along with a Claim for Damage form was mailed to Ms. Bayer. She is required to return the completed form to the Port and then a copy will be sent to Enduris. ^{4.6.0} Commissioner Scholfield was harassed on the Sunday of the festival as he was moving a boat from the "reserved" area to accommodate a boat affiliated with the Duck boat. A woman on another boat started telling him he couldn't do that. He identified himself and said he had authority to move the boat. She continued to take pictures of him and yell at him. A Safe Security guard was on duty on the pier, but it was a new guard that didn't know

Port of Silverdale – Minutes of Regular Meeting on August 16, 2018

Commissioner Scholfield, so he couldn't identify him. Commissioner Scholfield requested the guard ask for Sheriff assistance. Two deputies arrived and were also skeptical of Commissioner Scholfield's actions as he had nothing identifying him as a Port authority. The boat was moved. Commissioner Scholfield has ordered new safety vests with "Port of Silverdale" on the back. It was agreed shirts for Port Commissioners with the Port logo and name should also be purchased.

NEW BUSINESS

5.1. Discrepancy in moorage fee payment – Mr. Barry Fretwell contacted the Port because he received a past due moorage fee letter. He suggested the police be involved because money must be missing as he paid cash for each of the two nights he moored his boat. It was verified that Mr. Fretwell did pay for two nights, which confirmed no money was missing and the nightly boat report confirmed that Mr. Fretwell's boat was moored at the Port for three nights (July 24 – 26). He was adamant that he only stayed for two nights (July 24th and 25th). Upon review of the nightly moorage reports for July 25th and 26th the boats listed were identical, which seemed strange. Safe Security's supervisor, Jason, was contacted. He talked to the security guard on duty that night and they apologized as they concluded that there must have been a glitch in their electronic system. A response was sent to Mr. Fretwell, who seem to understand.

5.2. Expansions Yoga is holding yoga sessions on Fridays in August in the grassy area. They were told to contact Roy Sahali to coordinate.

5.3. American Legion Bathtub Races – are scheduled for September 1st and 2nd. The Use of Port Property Application was signed.

6. **SAFETY** – nothing to report.

Commissioner Scholfield reported that the boat ramp has been scraped and it went well. Underneath the handling pier and ramp will be cleaned out on August 29th.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$49,107.78, checks numbering 11841 through 11866 with 11866 being void and Electronic Transfer 2018-08 to the U.S. Treasury in the amount of \$1,704.38, were approved (motion by Aus, second by Scholfield, unanimous).

The Commissioners agreed that a water cooler could be ordered for the Port office, which will not only benefit Port staff but the public as well.

8. EXECUTIVE SESSION – None

9. ADJOURN

The meeting adjourned at 10:03PM.

Approved:


Henry Aus, Commissioner

Lawrence Greaves, Commissioner


Ed Scholfield, Commissioner

PORT OF SILVERDALE 8/16/18

AGENDA

OLD TOWN PUB STRUCTURAL ANALYSIS – SUMMARY

KC PUMP STATION #3 / SEE NEW GIG HARBOR PROJECT

PARKING/STREETS

CONCEPT USE PLAN REVISIT -- “show us what you think we should do with our properties”

- Old Town North / Old Town South --- FOCUS
- Scheme A
- Scheme B
- Discuss Direction

NEXT STEPS

PORT OF SILVERDALE 8/16/18

NEXT STEPS

OLD TOWN PUB STRUCTURAL ANALYSIS – ACCEPT DRAFT AND PUBLISH

- Q re cost estimate?

IF SCHEME B: MEET WITH KITSAP COUNTY PUBLIC WORKS, SHOW VISION FOR SILVERDALE’S “FRONT PORCH” AND COMBINED FACILITY

- Andrew Nelson, Director KCPW
- Barbara Zaroff, PM

POS/CKSD MEETING #3: Next week 23rd, 2:00 – 4:00 at Jenne Wright.

- This is essentially a programming exercise
- Present Old Town North-South Focus based on tonight’s POS Commissioner input
- Present updated POS Concept Use Plan based on tonight’s POS Commissioner Input
- Trial Project/Owner/Funding Source exercise

POS CONCEPT USE PLAN: publish elements by October 15, 2018

POS MASTER PLAN UPDATE: publish updates by November 15, 2018

- Include project briefs: Owner/Partners, Project Data, Funding Source(s)

PORT OF SILVERDALE 8/16/18

OLD TOWN PUB STRUCTURAL EVALUATION

RFM/Reid Middleton

DEFICIENCIES SUMMARY

- Improvised foundation/settlement/lack of connection
- Severely deficient gravity loading capacity
- Severely deficient lateral loading capacity

PATH TO STRUCTURAL SOUNDNESS

- Remove and replace entire foundation with spread footing or, better, piling system
- Re-sheath majority of exterior walls
- Establish top-to-bottom shear walls at approximately 50% of exterior perimeter
- Build complete new column-and-beam support for second floor
- Re-sheath main, second and upper floors to become effective shear diaphragms
- Provide proper connections between all horizontal and vertical shear diaphragms
- Basically: a total structural rebuild

PORT OF SILVERDALE 8/16/18

SHELLHOUSE

- Shared Facility: Port/Kitsap County
- Multi-Use Facility: Shellhouse & Training, PS3 Above-ground, Small retail/vendor office, public viewing, small events
- 6,300 SF footprint + partial upper and viewing platform
- Shellhouse can be used with Plaza and Green for small events
- Meet Old Town design standards, nod to Heritage
- Landmark anchors important corner, recreational focus

BOATYARD

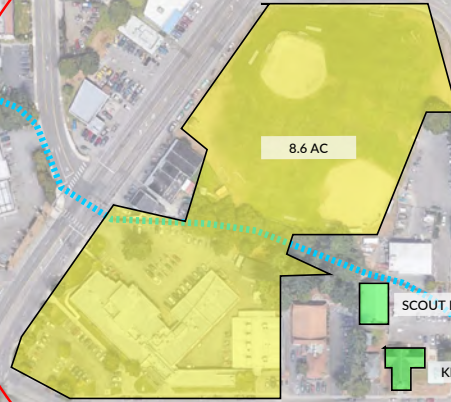
- Dual-use Facility: Street retail + boat storage
- 4,000 SF footprint + high loft overhang for cover
- Small boat storage at good location
- Boat storage two levels
- Strengthens and activates Byron Street edge
- Store launch float?

OTP REBUILD

- Drinking/dining on main floor, or both floors?
- Small office or apartments above?
- Two stories, 3,500 SF total (OTP 6,400 total)

NORTH FOCUS

CIVIC
STREAM RESTORATION
OPEN SPACE / PASSIVE RECREATION
MF RESIDENTIAL
PARKING
ENVIRONMENTAL EDUCATION
LARGER EVENTS



CARLTON

SCOUT HALL

KNIT N STITCH

LOWELL

OLD TOWN
PUB

BYRON

WATERFRONT
PARK

NON-MOTORIZED

MOTORIZED

PS #3

PRESERVE

SOUTH FOCUS

WATER RELATED RECREATION & ENJOYMENT
SMALL BUSINESS, MIXED-USE
COMMUNITY FESTIVALS, MARKETS ETC.
WALKABILITY
PRESERVATION & MITIGATION



SCHEME A

🕒 1" = 40' - 0"

PROPOSED UPGRADES

- Visually Appealing Building
- Larger Capacity Pumps
- Odor Control System
- Landscape to Blend



SHIELD WITH CONTOURS



ADDRESSING VISUAL APPEARANCE

- Smaller building with fence (shown)
- OR
- Large building enclosure



Future Landscaping & Screening

PUBLIC SURVEY RESULTS

Visual appearance



- 1 - Not Important
- 2 - Less Important
- 3 - Neutral
- 4 - Important
- 5 - Very Important

Weighted Average = 3.55



PUMP STATION 3 UPGRADES PUBLIC MEETING JULY 26, 2018

Kitap County Department of Public Works
Sewer Utility Division

SAFETY - ACCESS & CONTROLS



Before

After

PROJECT SETTING

- Multi-use property
- Highly visible
- Scenic area



HOW WILL NOISE BE ADDRESSED?

Noise Source	Solution
<ul style="list-style-type: none"> Pumps Electrical Equipment/Panels Generator 	<ul style="list-style-type: none"> Locate below ground Locate inside building Muffle exhaust Locate in sound attenuated room in building
<ul style="list-style-type: none"> Odor control facilities 	<ul style="list-style-type: none"> Locate inside building Vent through roof
<ul style="list-style-type: none"> Ventilation fans Vehicular activity for maintenance 	<ul style="list-style-type: none"> Locate inside building Muffled equipment Normal maintenance during daylight hours

SITE EVALUATIONS - MID 2015



Photographs



Photograph 1 Exteriors of building



Photograph 2 Another exterior view of building



Photograph 3 Interior view of building showing ceiling and lighting



Photograph 4 Interior view of building showing concrete floor



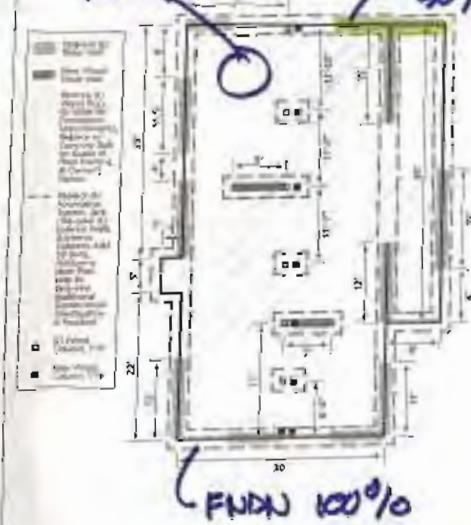
Photograph 5 Exterior view of building showing foundation



FLOOR DIAPHRAGM

100%

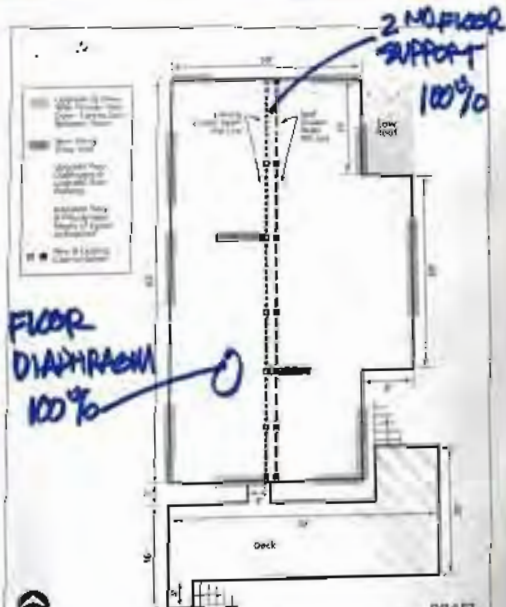
Strong walls 50%



FNDN 100%

2 NO FLOOR SUPPORT 100%

FLOOR DIAPHRAGM 100%



FLOOR DIAPHRAGM 100%



Gig Harbor's Welcome Plaza Wins State Honors

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Skansie Park Development Named AIAWA Project of the Year



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Construction begins Tuesday (Sept. 22) on the new Gig Harbor Welcome Plaza. It's a \$2.5 million project in Skansie Brothers Park in downtown Gig Harbor. Andrew Blomquist / [blomquist@news11.com](#)

04/22/24 9:01 AM

Welcome Plaza construction begins Tuesday in Skansie Brothers Park

VIDEOS

[Key Peninsula to transplant for '24](#)

[Key Peninsula to transplant for '24](#)

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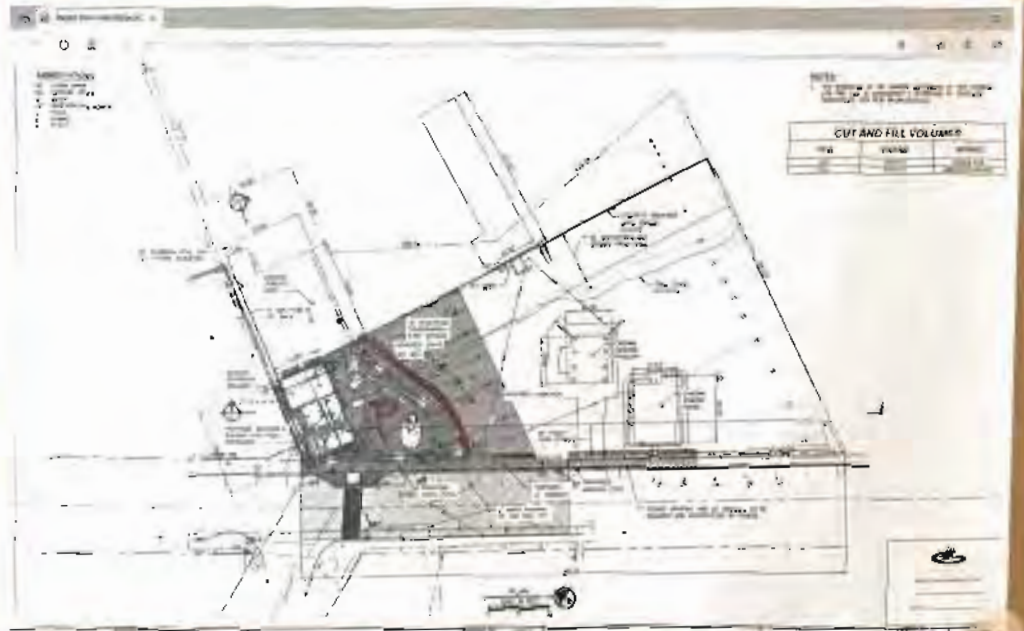
[Newly appointed District leaders tackle big issues](#)

[Newcomers to L see success in p](#)

[Police invite public academy](#)



LIFT STATION 4: CONCEPT #1



OLD TOWN SILVERDALE FUTURE

CENTRAL KITSAP SCHOOL DISTRICT/PORT OF SILVERDALE AND INVITED GUESTS

A joint discussion about the future of Old Town Silverdale through the lens of agencies with community missions, proximate real property interests, and the opportunity to coordinate efforts.

Meeting #: 3 of 3

Date: August 23, 2018

Project Name: Old Town Silverdale Future

ATTENDEES: Ben Anderson – AAA	Phil Best – POS Attorney
Steve Rice – RFM	Lawrence Greaves- POS Commissioner
Ron Easterday – RFM	Robin Shoemaker – CKSD Facilities Director
Abigail Overton - RFM	Doug Newell – CKSD Asst. Supt.
Steve Segó – WMP	Angie Silva – KCC Senior Policy Analyst
	Stacey Saunders – CKCC

Overview of previous meetings and intent. Steve Rice explains agenda and why RFM is involved and the evolution of the CKSD/POS partnership and asks both CKSD and Port of Silverdale representatives to explain their history and investment in this project.

Doug Newell: CKSD has long history in this area and with that, CKSD reflects the community so we want to honor and respect that and hold up ideas about where we are and where we should be going. We own 60 acres on the hill above town and have completely redeveloped the CKHS/CKJHS campus. How does this improvement allow us to think about the Jenne Wright complex? RFM is helping us with the library. What do we do with Jenne Wright? We feel we have a responsibility to set the stage for a better Old Town area, not just make a profit on the property. How can we as a public agency set the stage for private and public entities to make Old Town into a hub for Central Kitsap. We have not seen much investment in the properties down here and we would like to see more residential and events and shopping merge together. When we think about what Old Town could be, what can the Jenne Wright property offer this area?

Phil Best: Since the 1970s the Port has been acquiring properties – people have been selling to the Port. We acquired what were residences along Lowell and as property became available along the waterfront we worked with the County and jointly applied for grants to develop marina/dock. We acquired wetlands and the Old Town Pub soon after. From there we wanted to develop a Comprehensive Plan, so we had a citizen team come up with ideas before involving RFM. Interest has been developing from boating groups and we want to make sure we make this

plan with them in mind. CKSD then came into the picture with RFM's help which further benefited the comprehensive plan development.

Steve Rice explains POS and CKSD common missions.

Ron explains programming exercise at last meeting and takeaways

Doug adds to CKSD mission: Want to change the perception of Silverdale from Kitsap Mall and Costco to Old Town and a more full, balanced community.

Angie: County in agreement with all these takeaways.

Stacey: We don't know what the vision is yet, but we need to make sure there's enough positive information from the get-go to help with citizen dissent.

Steve S: Old Town is still Silverdale – still evokes historical Silverdale. There are windows in our lives, some are small some large. This Old Town window is wide open. My exhortation would be: think big about what you could do. These opportunities don't come along often.

Steve R. explains Old Town grid and North/South drawing and why the Old Town grid being intact is beneficial to our efforts. What properties exactly are we including? Make sure Jenne Wright property is included in new Old Town.

In our work with the Port, we are in the process of considering what a Comprehensive Use Plan (CUP) is. When we began bringing CKSD in the conversation, it's almost like we stepped back a bit and looked at the broader area and saw opportunities for an area/district, not just the waterfront. In previous meetings we discussed the idea of an event center as one option, but when we started thinking about it like the North/South focus, we realized that we would need a regional sized parking facility and that it may be more conflicting than complimentary. We can have a North, South, and East focus. The Port is the South focus with connections to boating groups and waterfront. How can we establish a focus between all three areas? With a slight separation of Old Town, we can become clearer in our focus. South Old Town can be so much more.

Doug: Because Stacey mentioned it, the Farmer's Market has a problem where people don't linger. There need to be opportunities for people to move through Old Town.

Steve Rice: Mitigation opportunities are abundant with Strawberry Creek and where open space possibilities are. Looking at this we know there is an opportunity for multi-family residential. Is Parking a concern for Old Town? If so, how do we address it? Street? Public or private property? Environmental education, public events, cultural facilities, residential, and hospitality opportunities have all been mentioned. We are looking for feedback on these ideas/plans.

Team designates properties and owners on aerial.

Steve S: Has anyone conducted a study on Strawberry Creek?

Doug: It is a salmon run and there was some initial work with the county.

Steve S: Any tribal conversations about the Creek?

Doug: Yes, met a few years ago and have a meeting in the next week or so. What we want to do is look at what the parameters are and see where the culvert would be. We have plans from 1920s of where and what stream used to be. CKSD wants to maximize the value of our properties, but in a way that benefits the community. If we can get a study on Strawberry Creek that allows us to have some green space, that would be a priority for CKSD.

Steve S: Foundational ideas on how you would approach monetization and creation of project involving the stream. There is not one part of the creek that stands on its own. It flows out into bay and up to the mouth. It is an ecosystem with potential temperature issues and impacts by development. If there was a way to create a plan around culvert and upstream with easements and GPC that says we're going to enhance and preserve certain things and how we're part of a bigger picture, it would be an excellent way to get someone to pay for it. If you have some holistic approach on how the creek fits into Old Town, there are ways to take advantage.

Doug: Clarifies monetization – right now being along Strawberry Creek is not a desirable place. There are no recreational areas. If we can create an environment where people want to be near the creek we can create positive investments.

Steve S: I see CKSD being a very valuable partner in Strawberry Creek aspect.

CKSD: We want to have a controlling stake in what happens in Old Town.

Stacey: It's always struck me on how little relationship there is with the Tribe in North, South, and Central Kitsap. We have these roots and there are no ties.

Angie: I see this like a Chico-esque partnership to make the watershed work. I see this as two transitional areas.

Steve R: In my mind, if Strawberry Creek has a great answer and is healthier to salmon, it stands to be a centerpiece but south of the creek feels more connected to Old Town than the north of it. We colored the green buildings on the map because we feel they are historically significant and feel they create places for people – what are the opportunities for pedestrian transition? This feels like a key to connecting Old Town.

Steve S: Chico has an opportunity to extend above the highway – bridge involvement. Because of the change opportunities it gave the tribe an opening to make creek healthier. In Old Town, whenever you do anything to add value to function you'll be asked to diminish human interaction with waterway, so it's a fine balance.

Doug: I agree. Hopefully we can make this better. Just about anything is better than the way it now. How do we take steps to better this area?

Stacey: Is this parking where it needs to be? We need to be strategic about parking because it will affect how people spend their dollars. Farmers Market should be located where parking is abundant and easy.

Phil: Park was developed without any parking. Parking has been a headache for many years.

Steve S: How do we think about offsite parking/transit connections?

Doug: First step needs to be a *reason* to get here. Second step is *how* do you get here. Think about Poulsbo transit system and their loop bus that hits all the major Poulsbo sites. Could Old Town have something like that?

Phil: Our experience is that if you make it a destination they might come, but Silverdale waterfront is very difficult to access.

Robin: Steve R.- have you thought about how views line up with properties Port owns? Are there more visibility opportunities? Is there a connection opportunity along Washington Avenue?

Steve R: Is Waterfront Park thought of as a busy place? Any discussion about expanding it?

Angie: Yes, it is busy. No, no discussion of expansion.

Doug: Immense trail opportunities.

Steve S: I've always felt that the Bayshore connection could be the head of the Clearcreek trail. While this isn't all about making money, private funding does help pay for advancement. Involving private investors could be important.

Doug: We want to guide private money. When is the time to have the discussion about involving private entities for funding?

Phil: The citizens committee and others have discussed preserving old Town and keeping it in the framework its in now, but there's some concern that if it was purchased by private investors, it could change overnight.

Doug: Can you describe what people think Old Town is now? How many houses are actually still residential. Do we want our business to look like houses or do we want to get to a point where there's a distinction?

Phil: We would need to designate design standards.

Angie: There is an outdated Old Town design standard, but it has unrealistic expectations.

Steve S.: Are there issues that, because of the historic function, make it hard to create balance between new investment and the Old Town feel?

Doug: How do we incentivize development and preservation while keeping it on the appropriate scale? Do we try to encourage the building of lease opportunities?

Meeting # 3
Project: Old Town Silverdale Future
Date: August 23, 2018
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Steve R: If we make good decisions, property values will increase. As property increases in value the desirability for others to invest increases.

Doug: For other communities like us, what are the steps that they took and what are the things we should be doing and steps we should take to realize this? Let's build a road map.

Steve S: Put together specific objective of looking at building guidelines. As far as how you put this together, the two most important parts are POS and CKSD. The stream piece needs attention and information before something can continue.

Steve R: Downtown Renton, Bothell are great examples.

Doug: My assumption is that we do not have a lead agency since Silverdale is not a city. How do we work together to make a plan? Maybe we need to explore developing a charter?

Steve S: The task is to invent a city-type jurisdiction without being a city.

Doug: Next steps? Should we think some more and then come together for another meeting?

Steve R: Next steps will definitely involve structuring. RFM will be having a planning partner in civic design. We are going to figure it out together.

Steve S: The one danger of creating something like what we need to create is that it's easy to forget the roadmap.

Doug: Maybe a smaller group gets together to discuss the roadmap and then the rest of the team thinks and contributes. POS and CKSD do have a mission and what we can spend money on is our property – there's a limit to how much we can lead. We can sign on a charter.

Phil: Sound West Group as a partner?

Steve S: SWG's bandwidth is pretty stretched, but it's an idea.

ACTION: Schedule a smaller group meeting to come up with a clear roadmap and plan for next steps.



TRANSITIONAL

OLD TOWN GRID

**WATER DEPENDENT /
WATER ENJOYMENT**

OLD TOWN SILVERDALE VISIONING
TAKEAWAYS
FROM PREVIOUS SESSIONS 1 & 2

CKSD

- Identify new, complimentary uses for inclusion in Old Town.
- Values publicly-accessible open space as a balance to much of present-day Silverdale.
- Let's create more opportunities for people to live in Silverdale.
- Can we imagine a "civic core" for Silverdale somewhere in Old Town?
- Create more outdoor opportunities.
- Strawberry Creek's future will determine a lot about what will happen on the rest of CKSD's property.

OLD TOWN SILVERDALE VISIONING
TAKEAWAYS

FROM PREVIOUS SESSIONS 1 & 2

PORT OF SILVERDALE

- Increase Old Town vibrancy by boosting water-focused recreational activities and facilities (follows Port's mission)
- Keep nearshore improvements at a scale that can be supported by available parking. Don't sacrifice ability to hold festivals and markets.
- Public meeting space is of interest but on a modest scale.
- Explore shoreline mitigation opportunities.
- Continue to promote opportunities for small business to thrive in Old Town.

OLD TOWN SILVERDALE VISIONING
TAKEAWAYS
FROM PREVIOUS SESSIONS 1 & 2

**PORT OF SILVERDALE & CKSD
BOTH**

- Old Town needs a stronger identity. Let's boost its perception.
- Old Town should display a greater sense of its past as the historical birthplace of Silverdale.
- Together, agencies can better guide change that is in the best interest of the public.
- Want to see Strawberry Creek as a unifier, not a divider.
- Covet Old Town walkability and trail connections.



MOUNT BAKER ROWING AND







MINUTES OF SILVERDALE PORT MEETING ON SEPTEMBER 12, 2018

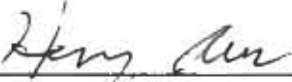
A Special Meeting of the Silverdale Port Commission was called to review and discuss the Waterfront Master Plan. The Waiver of Notice of Special Meeting was signed by all three Commissioners and a notice of the Special Meeting was taped to the Port office door two days prior to the meeting, both of which are attached hereto. The meeting started at 2:30PM at the Port office. Attendees included: Commissioner Greaves; Commissioner Aus; Commissioner Scholfield; Port Attorney Phil Best; Port Administrator Theresa Haaland; Carla Larson of Whaling Days; Steve Rice and Ron Easterday of Rice Fergus Miller (RFM); Vern Schager and Andrew Thorsen of Art Anderson Associates; and Steve Sego of Waterman Mitigation Partners.

Steve Rice explained that RFM was tasked with including non-motorized watercraft access from the current Port facility within the Waterfront Master Plan. He provided several options that Art Anderson Associates personnel helped to determine. These were discussed and an additional option was discussed and agreed upon. Mr. Rice plans to update the current designs to include the new option. Commissioner Scholfield said that he sees the implementation of this portion of the Waterfront Plan as the first phase with the timeframe in about two years.

Mr. Rice explained that on September 25th RFM personnel will be meeting with Kitsap County personnel to further discuss the placement of the County's sewage pump station. It is hoped the County will be willing to work with the Port by constructing a new multi-use building to the north of where the current building sits. Mr. Rice made note of Phil's suggestion of informing the County that the Port would like an upper level to the building to be used possibly as a meeting room with covered deck areas. Mr. Rice will provide the Port with a summary of that meeting.

Phil explained that Alison O'Sullivan, biologist for the Suquamish Tribe, has asked to have a meeting to discuss the Port's plans for the waterfront. Phil plans to set up the meeting with Ms. O'Sullivan, Port Commissioners, RFM personnel, Art Andersen Associates personnel and Steve Sego. Mitigation was discussed. Mr. Sego plans to draw up a matrix showing a range of what can be done and have it available for that meeting. The meeting will be scheduled sometime after September 26th to accommodate everyone's schedule.

The meeting adjourned at 4:10PM



Commissioner



Commissioner



Commissioner

WAIVER OF NOTICE OF SPECIAL MEETING

The undersigned, Port Commissioners for the Port of Silverdale, hereby waive the requirement of notice in writing of the special meeting of the Port of Silverdale held on Sept. 12, 2018, at 2:30 pm @ Port office, is present at such meeting, and agrees to the conduct of the Port business as announced by the President in calling this meeting.

Lawrence C. Greaves

Commissioner

Henry Ren

Commissioner

GA

Commissioner

Port of Silverdale Comprehensive Plan

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**Replace with new Use Plan and
 Recommended Projects
 (12-20 pages)**

Appendices

A.	Authority of the Port
B.	Stakeholder Feedback Summary
C.	Socio-Economic Analysis
D.	Preliminary Concept Cost Estimates
E.	Revenue and Rate of Return Estimates

Add new Appendices:

- A. 2018 ON-LINE PUBLIC SURVEY
- B. 2018 MEETING NOTES

Port of Silverdale
Minutes of Regular Meeting
September 20, 2018

1. CALL TO ORDER

Commission chairman Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Bridget Burke, Ashley McNeil and Ellen Strong of Clam Island Rowing (CIR); Greg Jacobs of Kitsap Sailing and Rowing Foundation (KSRF); Bob Ling of Silverdale Autoworks; Caleb and Vickie Reese; John West; and Tim Knapp arrived at 8:30PM.

1.1. Agenda – *the agenda was approved as prepared* (motion by Scholfield, second by Aus, unanimous).

2. CONSENT AGENDA

2.1. *The August 16, 2018 Regular meeting minutes were approved as corrected* (motion by Scholfield; second by Aus, passed – Commissioner Greaves abstained from the vote as he was not in attendance at the August meeting).

The September 12, 2018 Special meeting minutes were approved as submitted (motion by Scholfield; second by Aus, unanimous).

3. PUBLIC COMMENT –

3.1. Eagle Scout Service Project - John West explained that he is a boy scout and in hopes to receive his eagle scout badge. In order for that to happen he needs to complete a service project. He is also a sailor with KSRF and so is hoping to complete his service project for the Port. One of the requirements for the project is that it benefits the community. A very common project is for a scout to build a bench although, several years ago a scout designed and built the bridge over the Port's bio-swayle. Mr. West

came into the Port office earlier in the week inquiring about projects the Port might have for him to complete. He was told that adding a life jacket loaner station on the pier might be a possibility. Phil explained that he recently moored his boat at the Port facility and witnessed young children on the docks without life vests and without adult supervision. The Port rules state that children under age 14 on pier or floats must wear personal floatation devices or be accompanied by an adult. Having an additional loaner station on the pier could really serve a safety purpose. The Commissioners agreed that it would be beneficial to add an additional life jacket loaner station to the pier near the gangway. Mr. West explained that his next step is to get approval from the scouts. He was asked to prepare a budget and project design and bring it to the October Port meeting for further discussion. Commissioner Scholfield and possibly Tim will plan to meet with Mr. West to determine the placement of the station. *It was agreed to allow John West's proposed life jacket loaner station Eagle Scout project pending further information* (motion by Aus, second by Scholfield, unanimous).

3.2. Silverdale Auto Works – Bob Ling, the owner, explained that he is planning to pave a portion of the alley that is between his property and Port property along Washington Avenue. He was hoping the Port would consider partnering to complete the work, but is aware that the Port has to follow the Public Works laws. It was agreed that paving the alley would be beneficial to the Port's tenants in that area. Phil explained that the Port could send the job out to bid using the Small Works Roster and once the project is complete require Silverdale Auto Works to reimburse the Port for the costs of their portion of the paving. Mr. Ling agreed. Since the alleyway is a County easement Phil will contact the County to determine the necessary permit requirements.

3.3. Silverdale Tree Lighting ceremony is scheduled for November 24, 2018. Carla Larson requested the Port sponsor the firepit and refreshments similar to last year. The Commissioners agreed up to \$50 could be spent on refreshments for the event.

Ms. Larson asked about the status of Carley Bayer's claim against the Port. Ms. Bayer claims that she broke a bone in her foot when she stepped on the roller tract at the dock side of the gangway, while she was attending Whaling Days. A Claim for Damages form was mailed to Ms. Bayer. An e-mail dated August 23, 2018 from Ms. Bayer was reviewed. She explained that she has received the form and once she obtains all the necessary information she will return everything to the Port. From there a copy will be sent to the Port's insurance company, Enduris. Phil asked that further discussion be reserved for Executive Session.

4. UNFINISHED BUSINESS

4.1. Rowing – Bridget Burke reported that the rowing chase boat, the Coho, has been moored at the Port facility and has been untied several times. Unfortunately, the security camera footage doesn't cover the area where the Coho is moored. Thankfully, the boat is secured to the dock not only with rope, but also a cable and a lock so it did not drift off. This also happened while the Coho was tied up to the boat launch handling pier. Onlookers later told Ms. Burke that they witnessed a young male teenager untie the boat. An adult stopped him and they left together. Ms. Burke believes the young man may have a mental condition and he may be the same person who has untied the Coho from the dock. The steering cable of the Coho recently broke and now needs to be towed to the boat ramp in order to trailer it to get it repaired. Commissioner Scholfield will be working on adding the newest security camera footage, which shows a portion of the moorage facility, to the Port's website. Greg Jacobs said that

would be beneficial not only to view boats that are moored but also to get an idea of the weather conditions.

Ms. Burke explained that the Programs would really benefit from having access to a water spigot near the boat ramp, so that they would be able to wash off the salt water from the Port's boats and equipment. They have been using the spigot outside the Port office, but it is inconvenient. Commissioner Scholfield said that there is one by the restroom facility in the middle of the flower bed, but it was damaged and is not usable. It was placed there for a temporary purpose anyway. He suggested they use the spigot that is located on the outside wall of the boat launch restroom facility. They will need to purchase a key for it. Mike said that he might have an extra key that will work.

Ms. Burke thanked the Commissioners for getting the mound of dirt across the street leveled as it makes it much easier for CIR to remove and replace the rowing shell. She reported that there is a massive piece of dock on the beach which is causing a hazard because when the tide is high it isn't visible. Commissioner Scholfield will check on it. Ellen Strong asked about the derelict sailboat that is out in the water not too far from the boat launch. Commissioner Scholfield explained that the Department of Natural Resources is the lead on that. Since the owner has been trespassed from Port property and was jailed for failing to stay off of Port property, it is best that DNR continues as the lead even if the boat drifts back into the Port's water boundaries. Commissioner Scholfield suggested CIR call DNR and/or the County Department of Community Development (DCD) to further discuss.

Sailing – Mr. Jacobs reminded everyone that there will be a small one-day regatta on Saturday, September 22nd after that the boats

Port of Silverdale – Minutes of Regular Meeting on September 20, 2018

will be mothballed for the winter. The small wooden boats, given to KSRF by the Girl Scouts, need to be moved out of the weather. Mr. Jacobs asked if there was any room in the Port's shop. Commissioner Scholfield said that there wasn't and there are a lot of sails from the March regatta still stored up in the loft of the Port shop and are in the way. Commissioner Scholfield suggested items be stored in the Pub. As long as someone from the Port is there when placing or removing items it should be okay.

Mr. Jacobs asked about the Port's Waterfront Plan update. Phil explained that at the most recent meeting with Rice Fergus Miller personnel it was decided to include non-motorized watercraft access from the current Port facility. Adding an additional gangway to a non-motorized dock facility will be added to the plan and elongating the farthest finger pier. Mr. Jacobs asked that when designing the gangway Americans with Disabilities Act (ADA) requirements be met. He is unable to get down to the Port docks from the gangway with his wheelchair. One issue is the placement of the treads on the gangway. He suggested that if treads are used they be placed in the middle, allowing for the wheels of the wheelchair to not be affected by them. Mr. Jacobs said that he noticed the Olympic Outdoor Center (OOC) personnel having a hard time getting the kayaks down to the docks from the gangway. They are able to do it, but it's a struggle. He suggested they use a cart or something to make it easier. OOC has not talked to the Port about any issues.

Ms. Strong reported that this summer she was approached by many people asking for the codes to the showers. She realized they asked her because she was on the "Port of

Silverdale" chase boat. At times the Port office was closed, so there was no way for the individuals to get the code. Ms. Strong contacted the Port office and reported the issue. She was given the code to tell boaters if they asked. Commissioner Scholfield explained that the showers were open Friday through Sunday from 6:00AM to 5:00PM and the Safe Security guard that was on duty from Friday through Sunday for the season knew to give boaters the code if they asked. The showers are now shut down for the season. Mr. Jacobs explained that some places, including his own waterfront home, offer an outdoor shower. It would be beneficial in case of an emergency with someone suffering from hypothermia.

Non-motorized boat storage – rearranging and numbering the area has not yet been completed.

4.2. Port Facilities

a. Dredging – a letter dated August 31, 2018 from Kitsap County DCD was reviewed. The County determined the project is exempt from a Shoreline Development Permit and states State Environmental Policy Act (SEPA) Determination of Non-significance (DNS) was issued on August 16, 2018. Phil said that soil sampling is required for the dredge and the Army Corps is currently determining where the samples will be taken. It should be conducted in October. Phil is setting up a meeting with the tribal biologist, Allison O'Sullivan, to discuss the dredge and the Waterfront Master Plan update.

b. Port Rules/No Wake/ Windsock – Tim installed the windsock that states "No Wake". The Port rules were discussed. There is

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currently no mention of no rafting. *It was agreed to add “C.9. No Rafting” to the Port rules that were adopted on August 17, 2017 via Resolution 2017-02* (motion by Aus, second by Scholfield, unanimous).

c. Olympic Outdoor Center (OOC) – an e-mail was sent to OOC’s owner, John Kuntz, inviting him to attend a future Port meeting to further discuss the concession stand remaining on the pier year-round.

d. Vacant lot across from Port office – as mentioned the mound of dirt has been flattened out and the tubs and Styrofoam will be thrown in the Port dumpster over time as room allows.

4.3. Port Properties

a. 9004 Washington/Dispute Resolution Center (DRC) – there has been no word from DRC personnel regarding their request to upgrade/update the building.

b. 9020 Washington/Elizabeth’s House of Wax – there was no update on the sewer line connection. The tenant came into the Port office to pay rent and said that before entering into a new lease at the end of the year she would like to negotiate for new floor covering and exterior paint. Mike reported that there is a lot of wear and tear on the floors. It was unclear if the tenant was asking for new flooring throughout the entire building or just the hallway. Details will be determined. Commissioner Scholfield said that both the 9004 and 9020 Washington buildings will be painted next year.

c. 3255 NW Lowell Street/Central Valley Day Care opened for business on September

4, 2018. Mike has recently installed an outdoor light.

d. 3473 NW Byron Street/Old Town Massage – in order to determine the costs to refurbish the building a cost estimate report would need to be ordered. Such a report is estimated to cost anywhere from \$3,000 to \$5,000. At this point there has not been a decision to save the building and spending up to \$5,000 on a report that may not be needed doesn’t seem prudent, but at the same time the Commissioners haven’t decided not to save the building. It was agreed that it was too early to decide one way or another. Phil suggested waiting on a decision until RFM provides the conceptual design of the waterfront. The Commissioners agreed.

e. Lease Renewals – nearly all of the Port’s leases are up for renewal at the end of the year. A spreadsheet showing current rents and square footage costs was reviewed. The price per square foot for the 3295 NW Lowell Street property is lower than the other properties. *It was agreed to raise the total monthly rent including Leasehold/Excise Tax from \$1375 to \$1475* (motion by Scholfield, second by Aus, unanimous).

4.4. Department of Natural Resources (DNR) outer water boundary expansion - nothing to report.

4.5. Whaling Days Incident will be further discussed during Executive Session.

4.6. Old Town – Commissioner Greaves and Phil attended a meeting with RFM and Central Kitsap School District (CKSD) personnel on

Port of Silverdale – Minutes of Regular Meeting on September 20, 2018

August 23rd. Commissioner Greaves reported that RFM has been hired by CKSD to help determine what will be done with the Jenne-Wright building and property once CKSD is totally moved. They would prefer not to sell it to a developer but rather create something that will benefit Silverdale and the Old Town community. They are currently determining the boundaries of Old Town and brainstorming its future and how that might tie in with the Port's Waterfront Master Plan. One item they have discussed is to restore Strawberry Creek, which Phil agreed is something the Port has statutory authority support.

4.7. Waterfront Improvement Project – RFM is working on making the updates that were discussed at the Special Meeting held on September 12, 2018.

4.8. Pump Station 3 – RFM personnel are scheduled to meet with County personnel on September 25th to discuss the opportunity of a shared facility within the vicinity of the current pump station.

4.9. American Legion Bathtub Races were held on September 1st and 2nd. It seems to have gone well.

4.10. WPPA sponsored Small Ports seminar is scheduled for October 18th and 19th in Leavenworth. It was agreed to reschedule the October meeting from Thursday, October 18, 2018 at 7:00PM to Tuesday, October 16, 2018 at 5:00PM to allow Commissioners Aus and Scholfield as well as Port staff to attend the Small Ports seminar. Notice will be placed on the Port office door.

NEW BUSINESS

5.1. 2019 Preliminary Budget is scheduled to be approved at next month's meeting. The costs for the dredge project were discussed. Phil will look into possible Recreation Conservation Office (RCO) grant funding.

5.2. Life jacket loaner station was discussed under Public Comment.

5.3. Waggoner Cruising Guide – *it was agreed to renew the 1/3-page ad in the 2019 Waggoner Cruising Guide totaling of \$1,200.00* (motion by Aus, second by Scholfield, unanimous).

5.4. Enduris – the Port's insurance premium is being paid at tonight's meeting. It has increased over ten percent from last year.

6. SAFETY – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$50,839.38, checks numbering 11867 through 11892 and Electronic Transfer 2018-09 to the U.S. Treasury in the amount of \$1,723.88, were approved (motion by Scholfield, second by Aus, unanimous).

Tim reported that the dredge under the boat launch went well and next year's dredge should bring it down to where it should be.

8. EXECUTIVE SESSION – At 8:56PM it was announced that the meeting would be going into Executive Session for approximately fifteen minutes to discuss possible litigation and real estate matters.

At 9:11PM the meeting returned to Regular Session.

9. ADJOURN

The meeting adjourned at 9:12PM (motion by Aus, second by Scholfield, unanimous).

Approved:



Henry Aus, Commissioner

Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Ron Easterday

From: Steve Rice
Sent: Wednesday, September 26, 2018 5:25 PM
To: Phil Best Attorney
Cc: Ron Easterday; Ben Anderson (nanderson@artanderson.com); 'Steve Segó'
Subject: Pump Station 3

(Phil, would you please forward this to the Commissioners? Thanks much.)

Dear Phil and Commissioners,

Yesterday, I met on the waterfront with four Kitsap County public works representatives about our ideas around the pump station's location and the possibility of a joint facility with you. Joining me were Andrew Nelson (Director), Stella Vakarcs (Senior Program Manager), Dave Tucker (Assistant Director), and Barbara Zaroff (Project Manager).

This was a very productive meeting. In so many words, Public Works is very interested in our proposal. They like the idea very much.

THE MEETING (held Tuesday Sept 25, 10:30 – 11:45)

Here are the takeaways from our conversation:

- They like the idea of combining the pump station into a nice facility that can grace Silverdale's waterfront. They accepted this very readily, as if they were relieved to have a solution that doesn't leave them out there on their own, facing more scrutiny. Andrew Nelson said on the spot "we love it."
- They mentioned that the public meeting they held at the Silv Water District didn't go as they had hoped, but one of the takeaways was the suggestion from us to visit Gig Harbor's facility. They did, and met with GH's PW director. That visit opened their eyes to possibilities for a joint project in Silverdale, on that really is a benefit for the community.
- They think the plan location as presented fits their budget profile – as in, the distance to shift the above-ground pieces and still connect them to the well.
- They are prepared to work with the Port on how the schedule, land arrangements, permitting, construction package, and maintenance plan will need to be sorted out. They realize this is a joint facility, which presents many issues that need to be discussed and agreed upon. A major piece of this will be working jointly with RCO's Kim Sellers, as Dave Tucker thinks both the Port and the County signed original agreements when the land was dedicated as "recreational" forever. He has been working on this anyway, and invites participation from the Port. (See his email sent to Phil already.)
- They like your offer of making it possible for their infrequent maintenance visits to park off the roadway – it makes sense to them.
- They didn't respond to an offer to consider upper floor space, if beneficial, for them. (In their own planning so far, there has been no thinking for a two-level facility.)
- They mentioned that their Washington Avenue improvement project to add parking and a traffic circle by the dock will be ready to go to bid at the end of the year. I asked if it was too late for me to ask if they would consider pulling the traffic circle back a bit to the north, losing a couple of parking stalls, so to create more width between it and the dock to accommodate better dock access in the future, a better pedestrian trail, and a better connection for the plaza we envision: so, no pinch point. They said yes, they could do that. I was really happy to hear them say this. This is really important to the overall plan, and they seemed to get it. It was helpful to be on site for them to see all this in real space.
- This is a side note, perhaps, but worth discussing: they mentioned that Parks Director Jim Dunwiddie is interested in looking at the possibility of removing their bathroom/shower facility. (I bet I can imagine what you

are thinking.) I know you want to be careful with the whole bathroom business, and I mentioned that we'd welcome a discussion with Parks – just to listen to their ideas, as they have listened to ours. Who knows? Perhaps they are thinking ahead about mitigating that old bulkhead

- They took a copy of our print and will introduce it to the Commissioners.
- I told them that our plan is the result of several rounds of discussion at the Port, but that the Port, too, needs to validate if they want to go this direction. Andrew said he understood.

THOUGHTS AND NEXT STEPS

I think they want to do this. Obviously, we need to meet. As we are updating the master plan, we should be considering the big picture things:

- Confirm that you want to undertake this project and would operate a joint facility
- Confirm that the schedule they propose is one that you can agree with. (Your stated desire at our last meeting for new float as Priority #1 in the 1-2 year timeframe and this building as Priority #2 in the 3-4 year timeframe seems to fit pretty well with their timeline, at least on the surface.)
- Plan a visit to Gig Harbor
- Assign roles for the permitting effort that KC and POS will need to undertake to do this
- Discuss delivery method for the building, and phasing pros and cons
- Discuss funding strategy and how KC participates
- Discuss establishing a joint working relationship between KC and POS to do this project, and what would that entail?

I will be in Minneapolis on October 18th, the next regular Port meeting. Phil, should Ron and I arrange to come in sooner and talk with you about some of this, after you get Commissioner input? Or, we can have Ron there the 18th (and might anyway).

Overall, I am really pleased with the progress on this critical piece and the direction it's headed.

Thanks,

Steve

Steve Rice, Architect & LEED AP
Principal

Building Community for 30 Years

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C 360-271-2329

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Ron Easterday

From: Steve Rice
Sent: Wednesday, October 3, 2018 5:27 PM
To: Phil Best Attorney; 'Steve Segó'; Ben Anderson (nanderson@artanderson.com)
Cc: Ron Easterday
Subject: Meeting 9/2 with WDFW & Suquamish Tribe

Phil, Steve, Ben,

Thought I would jot down my takeaways from yesterday's meeting as they pertain to our Master Plan Update task:

MEETING ATTENDEES

Phil Best and Lawrence Greaves, POS
Alison O'Sullivan, Suquamish Tribe
Nam Siu, WFSW
Steve Segó, WMP
Steve Rice, RFM

TAKEAWAYS

- Generally, they like what the Port is contemplating on the waterfront
- Suggest we quantify mitigation opportunities along the shoreline
- Hope to see shoreline restoration east of boat launch to mimic what exists west of boat launch. Alison will send pics of recent project as indicator of what they'd like to see. Talked a lot about beach protection, shoreline stabilization against sea level rise, etc.
- Reacted negatively to loop trail in the wetland
- Like idea of pedestrian walk along restored beach, back a bit; love idea of KC shifting traffic turnaround at end of Washington farther away from waterfront
- Hope to see existing swale improved to rain garden and able to filter motorized boat wash-off; POS can get credit for this during shoreline permitting
- Didn't question idea of proposed new non-motorized float and finger pier extension; did question size of float. If grated, storing boats out there part of year negates grating, creating shadows – consider this when calculating float. Same for any "boat cover" out there. Can see float being permanent. Suggested we look at existing dock's solid surface; Nam noted that we don't get much overwater credit for mitigating shoreline; overwater mitigation brings more credit.

I know Steve S has thoughts on quantifying mitigation opportunities.

In general, this was a very positive meeting.

Steve

Steve Rice, Architect & LEED AP
Principal

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Port of Silverdale – Minutes of Regular Meeting on October 16, 2018

Port of Silverdale

Minutes of Regular Meeting

~~September~~ 16, 2018

~~September~~
October

1. CALL TO ORDER

Commissioner Ed Scholfield called the meeting to order at 5:03 PM in the Port office. Also present were Commissioner Henry Aus; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Carla Larson of Whaling Days; Randy and Marvel Hunt; Caleb Reese; John West; Dan Sullivan; Tim Knapp of TIKAR Services arrived at 5:20PM; and Steve Rice and Ron Easterday of Rice Fergus Miller (RFM) arrived at 6:50PM.

The date and time of this meeting had been changed from the regular meeting date and time. Notice, which is attached, was placed on the Port office door on September 27, 2018 informing the public of the date and time change and also informing that a quorum of Port Commissioners would be attending the Washington Public Ports Association (WPPA) Small Ports Seminar in Leavenworth on October 18, 2018 through October 19, 2018.

It was agreed to excuse Commissioner Greaves' absence (motion by Aus, second by Scholfield, unanimous).

1.1. Agenda – *the agenda was approved as amended* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The September 20, 2018 Regular meeting minutes were approved as submitted* (motion by Aus, second by Scholfield, unanimous).

3. PUBLIC COMMENT –

3.1. Eagle Scout Service Project - John West is planning to construct a life jacket loaner station on the pier, which will entitle him to receive his Eagle Scout badge. Mr. West asked where the life jackets would be purchased and if it would

be up to him to initially stock it. It was explained that the life jacket loaner station near the boat ramp was installed by Stand Up for Kids Kitsap; when it was running low on jackets Tim offered to restock it. Phil said that if life jackets are needed people will donate them. Mr. West anticipates the project costing no more than \$300, which included the cost of seven life jackets. He outlined his plan for the station. It will be constructed of pressure treated lumber and will be built similar to a cupboard so that it doesn't obstruct the view or detract from the atmosphere. He will bring the final drawing to the November Port meeting along with a material list and budget. Tim will be asked to be involved as well.

3.2. Complaint – John Karche was scheduled on the agenda, but was not in attendance. He called the Port office to make a complaint. He is a squid fisherman and has been using the Silverdale Port facilities for several years, but the other night he was told by the on-duty Safe Security guard that he had to leave because the Port was closed after hours. Mr. Karche hoped that the Commissioners would consider changing the rule from only allowing boaters on the docks after hours to also allowing fishermen. Commissioner Scholfield said that in the past squid fishermen have contacted the Port and received permission to use the Port facilities after hours. Phil suggested squid fishermen be provided a pass giving them permission to be there after hours that they can show to the on-duty guard. Mr. Karche will be contacted to discuss.

Dan Sullivan is the owner of Sullivan Heating and Cooling as well as J. D. Sullivan Investments, a local commercial developer. Mr. Sullivan explained that he is interested in the future of the Old Town Pub (OTP) building. It was explained that the Port has hired Rice

Port of Silverdale – Minutes of Regular Meeting on October 16, 2018

Fergus Miller (RFM) to prepare an updated Waterfront Master Plan (WMP). Reid Middleton recently provided a structural evaluation of the building. The next step is to pay for a cost estimate report, but the Commissioners have put that on hold until the WMP is received. At this point the Commissioners are uncertain of the OTP's future. Mr. Sullivan said that he would like to see Old Town revitalized and is interested in developing that area. Phil suggested Mr. Sullivan be added to the Port's Citizen's Advisory Group. He thanked the Commissioners for their time.

3.3. Port Waterfront Plan Update – RFM personnel were not yet in attendance. Phil said that he was talking with the Clam Island Rowing (CIR) coach, Ellen Strong, about the idea of the Port adding an additional gangway and floating dock from the pier to accommodate non-motorized boats with storage for sailboats and rowing shells on the dock. Ms. Strong said that CIR would prefer getting to the water from the beach so doesn't believe they would be interested in storing rowing shells out on a dock. Commissioner Scholfield said that even if they didn't store the shells there, they would still benefit from having a non-motorized dock and storage could be used for other non-motorized watercraft.

At 6:50PM Steve Rice and Ron Easterday of RFM came into the Port office. They were surprised the meeting had already started and apologized as they thought it was scheduled to start at 7:00PM. Mr. Rice recapped the meeting and further discussion he has had with the County regarding pump station 3. Once the County officially agrees to the joint-use facility an interlocal agreement will be

generated. It is believed the County plans to bid the Pump Station 3 upgrade project the first part of 2019. Phil reported to Mr. Rice that he was approached by one of the rowing coaches who told him that CIR isn't that interested in actually storing the rowing shells on a non-motorized dock. Mr. Rice said that was contradictory of what Bridget Burke had talked to RFM about. It was agreed that all of the user groups should come together to compile a combined list of hopes and wants for the project. RFM will be working on the deliverable of the Port's Master Plan.

3.4. Pump Station 3 – an e-mail from Steve Rice dated September 26, 2018. It recapped the meeting RFM held with Kitsap County Public Works personnel regarding Pump Station 3. Phil read an e-mail dated October 3, 2018 from Kitsap County Public Works Director, Andy Nelson, forwarded to Phil by Mr. Rice. The meeting with the County was productive and Kitsap County Public Works is interested in working with the Port on a joint facility north of where Pump Station 3 currently sits. They also agreed that the roundabout at the end of Washington could be moved slightly north of the current design, this would allow more room from the road to the waterfront. Phil said that the Port will need to be involved in the decision of where the facility will be built since the upper level meeting room space will be owned and run by the Port. Since the project may include a portion of land that the Port used Recreation Conservation Office (RCO) grants to improve, Phil suggested the RCO be included in early discussions to avoid a possible snag later. He also suggested that Kitsap County Parks Department personnel be invited to the next Port meeting to further

discuss the endeavor. Tim cautioned the Commissioners to be sure to read all the fine print so that the Port is not overcharged for its portion of the project. Phil explained it will need to all be outlined in an interlocal agreement. *It was agreed to move forward with the concept of a joint-use facility with the County, of which the design, scope and other details are to be determined* (motion by Aus, second by Scholfield).

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – the description of the damaged motor was tabled. An e-mail from Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF) dated September 28, 2018 was reviewed. Mr. Trunkey asks that the Sailing and Rowing Programs' relationship with the Port be refined. They are waiting until this is finalized to gift several boats to the Port. Phil suggested Port Commissioners take a trip with KSRF and CIR personnel to the Mount Baker Rowing and Sailing Center and the Sail Sand Point Center. Apparently, both programs partner with the City of Seattle, which is somewhat comparable with the Port as it is a governmental agency. This is something Ms. Burke has been requesting for some time now because it is thought that the way these programs are set up could be used as a model for Silverdale. Both KSRF and CIR want the Port more involved in the programs. The County originally ran the Sailing Program and owned the sailboats. CIR didn't form until after it was under the Port's sponsorship. The County eventually decided to do away with the program even though it was making money. Governmental agencies can only gift items to other governmental agencies so the County

and the volunteers that were running the program began thinking about what other governmental entity would be a good fit at taking over ownership of the program. They approached the Port. There was much discussion and eventually the Port Commissioners agreed. The Port was gifted all the boats and equipment from the County and authorized to run the program. The same volunteers who were running the program under the County began running the program under the Port. Since the equipment was now technically Port owned and needed to be insured it was decided to lease the items/equipment to KSRF, which allowed them exclusive use. They were required to maintain all the equipment. KSRF was also authorized to arrange a sale of equipment at the fair market value on behalf of the Port as they saw necessary. Proceeds were used to purchase new boats, which were titled to the Port. Once CIR was formed they too entered into a similar lease with the Port. In 2017, the Department of Revenue (DOR) conducted a leasehold excise tax audit on the Port. The audit concluded that the Port did not charge the fair market value for the leased items to KSRF and CIR. Since the State is due 12.84% leasehold excise tax on what is charged, this was a problem. The Port paid approximately \$2,000 in back leasehold excise tax due to the State. These funds were taken from the non-motorized boat storage account. It is now thought that maybe a lease is unnecessary. That is why taking a trip to the Seattle programs may prove beneficial to see how they conduct business. Commissioner Scholfield said that if CIR is not charging enough to be able to maintain the equipment, that's a problem. They also have had the Port chase boat moored at the facility for most of

Port of Silverdale – Minutes of Regular Meeting on October 16, 2018

the summer, which creates excessive wear and tear and seems a bit negligent on their part. Phil suggested the Commissioners sit down with KSRF and CIR reps to determine exactly what everyone is wanting. Marvel Hunt said that she and her husband rent to one of the CIR coaches and from what she has heard they will be asking more from the Port in the future. Phil said that both the sailing and rowing programs really shine and they are serving the community by offering classes and getting individuals out on the water. For the Port to subsidize such popular programs isn't necessarily a bad thing. It also brings people down to the Old Town area which is good for economic development. RFM's plans for the Port heavily involve the sailing and rowing programs. Commissioner Scholfield said that water activities seem to come and go in popularity. Years ago, waterskiing was the craze and windsurfing after that. Whatever facility is built, especially those used with RCO funding, it should be built as a waterfront activity center.

b. Rowing – CIR is holding a raffle and asked that at the end of this meeting at 7:00PM a Commissioner draw the winning ticket from a hat.

c. Non-motorized boat storage – it is unknown if a new volunteer is in place. The space numbers have not yet been made. The space is currently at capacity.

4.2. Port Facilities

a. Dredging – Phil reported that diving will begin once the sampling plan has been approved. The outcome of the sampling will determine where the sediment can be dumped. Phil read an e-mail from Steve Rice of RFM

recapping the meeting that Phil and Commissioner Greaves had with RFM personnel, Suquamish tribal biologist Allison O'Sullivan, Washington State Department of Fish Wildlife biologist Nam Siu and Steve Sego of Waterman Investment Partners.

Commissioner Scholfield reported that the apartment tenants at the end of Pacific Avenue have been storing items such as bar-b-ques, outdoor furniture and such on the Port's land and partially on the beachfront. He had heard that Mr. Siu requested the items be removed. Commissioner Scholfield posted notice at the site that if the items were not removed by a certain date they would be disposed. The majority of items were removed by the tenants.

b. Port Rules/No Wake/Windsock – Tim will provide an estimate for buoys that state "No Wake" at a future meeting. Commissioner Scholfield will add "No Rafting" to the Port rules sign. He explained that according to the RCW in order to impound a vessel, the Port has to have the fees associated with impoundment be posted and they currently are not.

c. Department of Natural Resources (DNR) expansion of the Port's outer water boundaries - nothing to report.

d. Boat ramp handling pier & sailboat float – Tim reported that the sailboat float has been removed for the season and the handling pier is scheduled for removal on October 26th.

The Central Kitsap Farmers Market is done for the season.

e. Olympic Outdoor center (OOC) – owner John Kuntz has been invited to attend an

upcoming Port meeting to further discuss OOC's concession stand being on the pier all year. It is believed Mr. Kuntz is currently away. Everyone seemed to agree that the concession stand was supposed to be a temporary building erected seasonally and removed at the end of the season. Tim is concerned of how it will fare during storms. Ms. Hunt asked that the Port require the building be removed as originally discussed.

f. Website – footage from the second security camera has not yet been added to the website. There are some software issues that Commissioner Scholfield is trying to resolve.

4.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – Commissioner Scholfield said that maybe a call for locate should be conducted in hopes that the County will respond and determine where the connection of the sewer line is located. Prior to entering into a new lease, the tenant is requesting the Port to agree on replacing the carpeted areas in the building and also replace the floor covering in one treatment room. Mike explained that the tenant is going to close her business for one week in February and that is when she is hoping the flooring can be installed. He was tasked with working with the tenant on the details and providing an estimate at a future meeting.

b. 3330 NW Lowell Street/Sugar Studios – damage to the back door of the building appears to be an attempted break-in. Mike has since repaired it. He will be adding a dusk-to-dawn timer to the outdoor lighting.

c. 3473 NW Byron Street/Vacant –

Commissioner Scholfield reported that there is still a warrant for the arrest of Trever Mercer, the individual who broke into the building and skipped bail. Ordering a cost estimate report for the building was tabled.

4.4. Paving the alley between 9004

Washington and Silverdale Autoworks was discussed. Commissioner Scholfield was told by County personnel that if the square footage for the paving was under 2,000 nothing needs to be done except obtaining a right-of-way permit. Anything over 2,000 square feet requires an engineering evaluation. The area between the two buildings is roughly 1500 square feet. It was agreed that the Port should continue the paving to include behind 3255 NW Lowell Street even though it may require the evaluation. Silverdale Autoworks will be required to reimburse the Port the costs per square foot of their portion of the alley. Phil will apply for the right-of-way permit.

4.5. 2019 Preliminary Budget was reviewed. *It was agreed to adopt the 2019 Preliminary Budget as submitted* (motion by Aus, second by Scholfield, unanimous).

NEW BUSINESS

5.1. Silverdale Water District bill dated September 30, 2018 for the Waterfront Boat Launch property (account # 008662-000) is high compared to last year. Commissioner Scholfield said there isn't a leak. It is thought the timer was incorrectly set for the summer.

6. **SAFETY** – nothing to report.

The gangway was discussed. Tim was tasked with redesigning the area on the dock where

the gangway rests. Commissioner Aus said that he has seen other Port facilities that do not actually have slots for the rollers to sit in but rather just a large piece of metal.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$47,853.50, checks numbering 11893 through 11922 and Electronic Transfer 2018-10 to the U.S. Treasury in the amount of \$1,665.22, were approved (motion by Aus, second by Scholfield, unanimous).

Plans must have changed for CIR members as no one was present for their raffle drawing.

8. EXECUTIVE SESSION – None

9. ADJOURN

At 7:30PM the meeting adjourned (motion by Aus, second by Scholfield, unanimous).

Approved:



Henry Aus, Commissioner

Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Port of Silverdale Over-Water Discussion

Date: December 4, 2018

Project Name: Old Town Silverdale Future

ATTENDEES: Ben Anderson – AAA
Andrew Thorsen - AAA
Steve Rice – RFM
Ron Easterday – RFM
Abigail Overton - RFM
Phil Best – Port of Silverdale

Bridget Burke – Clam Island Rowing
Forrest Wells – Olympic Outdoor Center
Julie Jablonski – Kitsap Sailing and Rowing
Greg Jacobs – Kitsap Sailing and Rowing
Matt Hebard – Kitsap Sailing and Rowing
Sheila Trunkey – Kitsap Sailing and Rowing
Steve Trunkey – Kitsap Sailing and Rowing
John Bouck – Kitsap Sailing and Rowing

PURPOSE: Discuss over-water options and needs with Clam Island Rowing, Olympic Outdoor Center, and Kitsap Sailing and Rowing Foundation. Facilitated by RFM, Art Anderson Associates, and the Port of Silverdale.

Introductions

Ron Easterday and Steve Rice reviewed:

- Reasons for Comprehensive Master Plan update
- RFM/POS relationship
- Previous meetings and purpose of this meeting
- Comprehensive Plans are part of future grant applications

Steve Trunkey –Kitsap Sailing and Rowing Association

- Recently at regatta in Portland and while they have a beautiful new facility, the sailing currents and boat traffic render it a less than ideal location for sailing.
- Believes that all western Washington sailing organizations would prefer to have most regattas at the Port of Silverdale.
- Port of Silverdale offers many key features:
 - o All of Dyes Inlet is 30-50 ft deep which allows boats to capsize without damage to boat.
 - o Easily changed course markers
 - o No commercial traffic
 - o Current dock is laid out perfectly for viewing and sailing activities
 - o Park is a great asset
- Regatta seasons are spring and autumn, with summers being much lighter.
- Regatta events are almost always two days long, requiring overnight accommodations for attendees.

- Regattas involve anywhere from 150-300 sailors, not including spectators/support.
- Youth racing circuit could host 4-5 events per summer, or more.

RFM

- Can analytics and hard numbers be acquired? There needs to be an equitable account from each group of what needs are. Funding applications require this data.
 - o KSRF states they can provide this information

All attendees agree that 10 million dollar minimum for Public Facilities District grant is an easy number to reach for a project like this.

Julie Wurden Jablonski reads KSRF needs list – see attached.

KSRF sail boat data:

- 37 sail boats
- 2 safety boats
- Largest boat 19'
- All masts can be removed, although that is not preferable.
- Davits would be an asset on the dock for races and removal of boats from water

RFM

- If project is marketed as 'boat storage', funding will be hard to find.
- However, if project is marketed as 'events and people-use' funding will be more attainable.

Phil Best

- Are local schools in contract with any sailing organizations?
 - o KSRF has no agreement with district.
 - o Central Kitsap School District seems to view involvement with a sailing program as a hazard.

Forrest Wells – Olympic Outdoor Center

- OOC has been at Port of Silverdale for 6 seasons - kayaks and stand-up paddleboards.
- OOC has 5 rental locations and serves 7,500 people per season at Poulsbo location alone.
- Water Trails Festival started by OOC
- Triathlon draw
- In 2018, OOC facility moved onto dock at POS.
- All gear currently fits in 20'X20' building + a staging area.
- OOC mission is to get kids on the water and get programs at Silverdale waterfront up to the Poulsbo level – youth and adult programs.
- May-September are busiest months

Needs include:

- Power and water
- On-dock location. This could be flexible, but water access is key due to weight and length of equipment.

RFM

- Why is Silverdale great for kayaking?

Forrest - OOC

- Great location, depth and geography
- Dyes Inlet provides quintessential Northwest vibe.
- No commercial traffic, limited power boat traffic.

Phil Best – POS

- Private kayak storage could be a good feature to include in plan

Bridget Burke – Clam Island Rowing

- CIR offers the following:
 - Masters and Juniors program
 - Sculling and sweep
 - Junior high and middle school programs
 - Free 'learn to row' program for general public
 - P.E. classes for Kings West
- Previously partnered with YMCA for rowing classes
- CIR has 11 boats and is running out of storage. Currently renting a storage space.
 - 58-60' = 8-person boat
 - 43-45' = 4-person boat
 - 28-30' = 2-person boat
 - 28' = single-person boat
 - 11' = ergonomic machine (erg) – this includes clearance for user to lean back
- What they need is a covered indoor space – does not need to be heated. This would give people a 'home' for community. This space could double as an exercise room.
- CIR could go year-round with an indoor area
- Erg competitions could be a real draw
- Space for 10 ergs would be great
- There are four rowing organizations on the peninsula and there needs to be a facility.
- A float would be great and a rack with a low dock, ideal.
- Another thing to think about is trailer storage/parking, which could be remote.

Phil Best

- Build our design based on what we like and need, then see if we can lift any pre-existing requirements/issues.

Parking in general is an issue. What about Jenne Wright building?

Steve Rice described CKSD/POS relationship as it pertains to Old Town and the Jenne Wright building.

Further discussion included:

- Dock options:
 - o Creating a facility on a barge connected to dock and moving it seasonally
 - o Extending existing dock and/or changing layout
- Parks and Recreation Department involvement/help?
 - o P&R can only provide advertising. No funding available.
- Phil Best would like to see design, then have a citizen review board meeting.
 - o Dollar amounts and increased traffic data important
 - o Budget that equalizes

This is a summary of the items discussed. Please advise this office within 48 hours regarding any omissions or differences in understanding.

Prepared By: Abigail Overton, RFM

KSRF Needs/Input List

Example:

Classroom space for 30 kids/adults

Dedicated Youth Changing room/showers accessible by programs (security)

Event gathering space for regattas, preferably with deck/direct view of water, capacity 120+, kitchen area

Covered picnic/shelter area preferably with garage doors that open (think Orcas shelter or Bellingham or SSP) with access to kitchen/serving lunch area for events

Combined fitness training area with Rowing?

Sail/gear locker/shed?

One or two bays dedicated for boat work/repair/education?

Boat shelter, secured area?

Questions: public spaces can easily have adult programming/events take over and push out youth programs. Since our youth programs are heading to Feb- Nov during the year, how is use prioritized? I really think that the examples of youth spaces at yacht clubs and other places shows how important it is for youth programs to have a "home".

PT Maritime is a prime example of adults having priority. Yes they run programs for youth but they are really 2nd class around there in terms of design and expectations. If we start doing summer camps with STEM and longer days this is important.

Are we ok with an argument that puts Rowing in the waterfront and regulates sailing to the far reaches?

Will for profit organizations have priority over non profit, and end up also competing for the classroom/event spaces? Think adult learning programs, kayaking programs etc.

What do we want in terms of dock space? Room for two - three fleets for school teams? Think expansion.

ultimately can we part buy Conrads corner building, maybe for youth?

SIGN IN SHEET

DATE: December 4, 2018

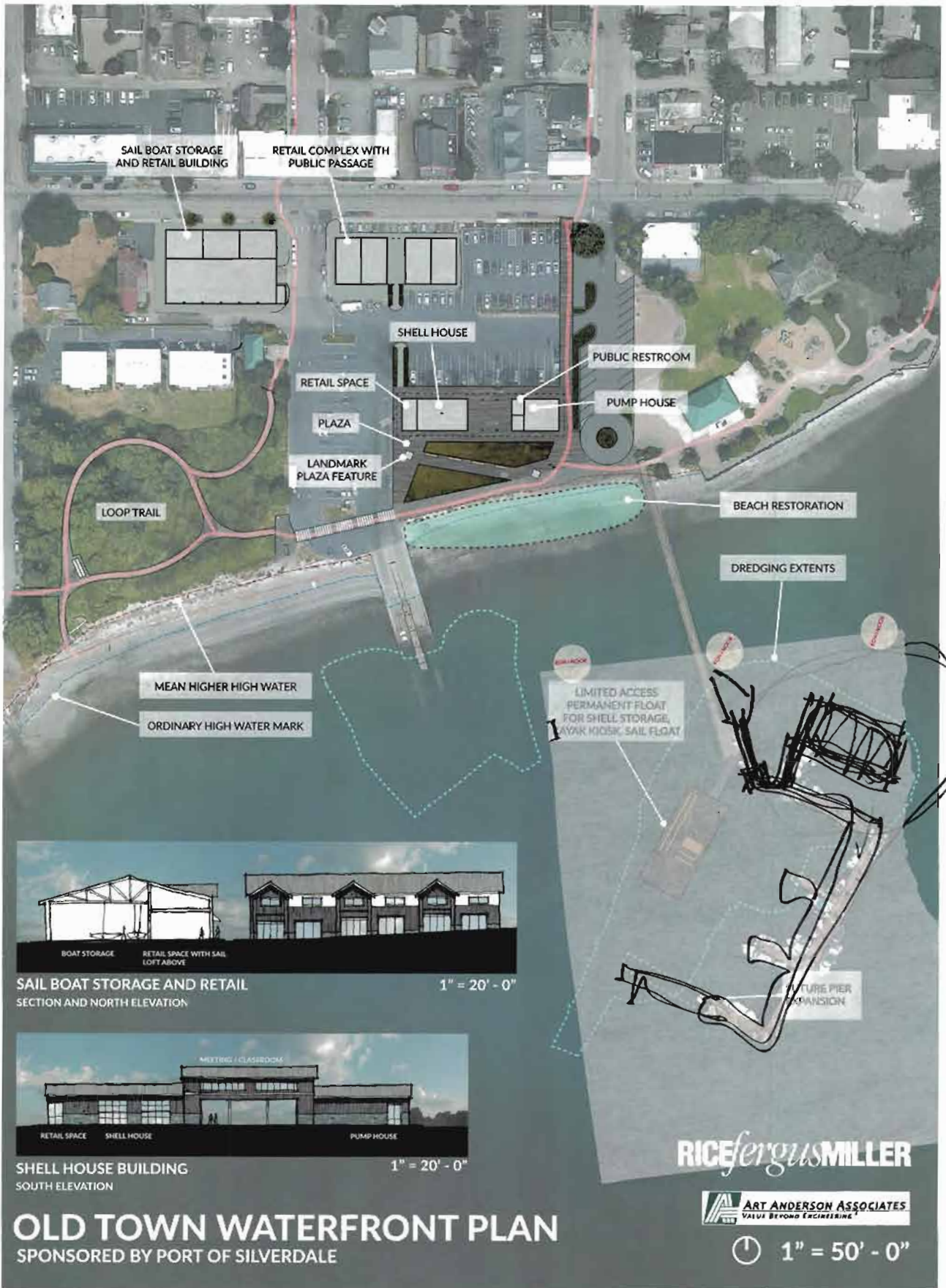
PROJECT NAME: Port of Silverdale Master Plan - 2017098

MEETING TOPIC: Over-Water Solutions

MEETING TIME AND LOCATION: 3:30pm Rice Fergus Miller - Forum

ATTENDEES

Name	Agency	Email	Present
Phil Best	Port of Silverdale	pblaw@wavecable.com	PB
Steve Rice	Rice Fergus Miller	srice@rfmarch.com	✓
Ron Easterday	Rice Fergus Miller	reasterday@rfmarch.com	RUE
Abigail Overton	Rice Fergus Miller	aoverton@rfmarch.com	✓ [Signature]
Mike Wright	Rice Fergus Miller	mwright@rfmarch.com	X
Bridget Burke	Clam Island Rowing	bridgetburke@@clamislandrowing.com	BB
John Kuntz	Olympic Outdoor Center	jkuntz@kayakproshop.com	
Forrest Wells	Olympic Outdoor Center	forrest@kayakproshop.com	[Signature]
Ben Anderson	Art Anderson Associates	nanderson@artanderson.com	[Signature]
Andrew Thorsen	Art Anderson Associates	athorsen@artanderson.com	✓
Julie Jablonski	Kitsap Sailing and Rowing	jw.jablonski@gmail.com	[Signature]
Greg Jacobs	Kitsap Sailing and Rowing	gregjacobs@wavecable.com	✓
Matt Hebard	Kitsap Sailing and Rowing	hebardmf@uw.edu	MB
Mike Fowler	Kitsap Sailing and Rowing	mikenying@yahoo.com	
Jenn Downes	Kitsap Sailing and Rowing	jenn@thedomestudio.com	
Sheila Trunkey	Kitsap Sailing and Rowing	strunkey@comcast.com	SET
Steve Trunkey	Kitsap Sailing and Rowing	strunkey.sail@comcast.net	SET
John Bouck	Kitsap Sailing and Rowing	john@creeksideplc.com	MB



BOAT STORAGE RETAIL SPACE WITH SAIL LOFT ABOVE

SAIL BOAT STORAGE AND RETAIL
SECTION AND NORTH ELEVATION 1" = 20' - 0"



MEETING / CLASSROOM RETAIL SPACE SHELL HOUSE PUMP HOUSE

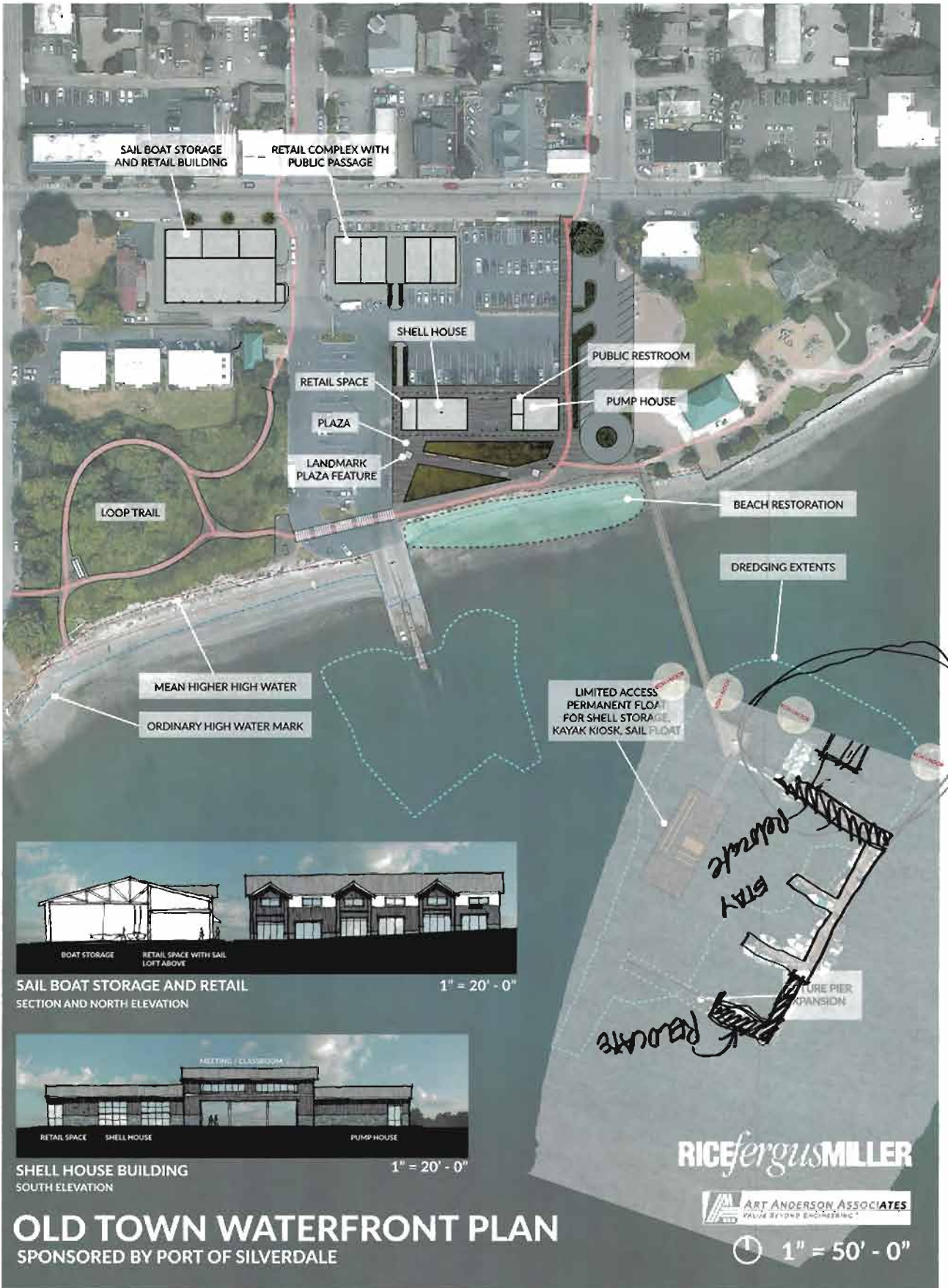
SHELL HOUSE BUILDING
SOUTH ELEVATION 1" = 20' - 0"

OLD TOWN WATERFRONT PLAN
SPONSORED BY PORT OF SILVERDALE

RICE *fergus* **MILLER**

ART ANDERSON ASSOCIATES
VALUE BEYOND ENGINEERING

🕒 1" = 50' - 0"



SAIL BOAT STORAGE AND RETAIL
SECTION AND NORTH ELEVATION
1" = 20' - 0"



SHELL HOUSE BUILDING
SOUTH ELEVATION
1" = 20' - 0"

OLD TOWN WATERFRONT PLAN

SPONSORED BY PORT OF SILVERDALE

RICEfergusMILLER

ART ANDERSON ASSOCIATES
PLANNING DESIGN ENGINEERING

1" = 50' - 0"





CLAM ISLAND ROWING



We are really excited to put in writing our needs for a shell house. While we are small club now, we hope to grow and hope the new shell house will be able to have room for growth.

Boats Racks

Our current fleet is 2 eights, 3 fours, 4 singles, and 3 doubles

8 person shell	60'
4 person shell	44'
Double	34'
Single	28'



Storage and workout room for 10 Ergs
We current have four ergs.

- Length 9' overall length needed 11'
- Can be stored upright



Other Needs

Lockers for rowers

Bathroom

Showers

15' x 20' workout room

Oar stand - must be 14' tall at least

Cox box storage with electrical access

Ron Easterday

From: jkuntz <jkuntz@kayakproshop.com>
Sent: Wednesday, December 19, 2018 10:09 AM
To: 'Forrest'; Abigail Overton
Subject: re Port of Silverdale - Over-Water Discussion Minutes

In addition to forrest comments

OOO is interested in expanding its concession to either on the dock or on land at the head of the dock, this is an important part of our operation.

We run an efficient operation in Poulsbo and wish our Silverdale facility to be configured similar.

Also leveling the dock walkway is an important issue to allow strollers and wheel chairs to access our facilities.

Thank you

John

John Kuntz

32379 Rainier Ave
PO Box 236
Port Gamble, WA 98364
(360) 297-4659 (work)
(360) 471-0836 (cell)
www.OlympicOutdoorCenter.com

President, Olympic Outdoor Center
Board, North Kitsap Trails Association
Board, Visit Kitsap Peninsula
Member, Kitsap Peninsula Water Trail Alliance

From: Forrest [mailto:forrest@kayakproshop.com]
Sent: Friday, December 14, 2018 10:36 AM
To: jkuntz@kayakproshop.com
Subject: *** SPAM ***FW: Port of Silverdale - Over-Water Discussion Minutes

From: Abigail Overton <aoverton@rfmarch.com>
Sent: Friday, December 14, 2018 9:49 AM
To: gregjacobs@wavecable.com; Forrest Wells <forrest@kayakproshop.com>; Steve Trunkey <strunkey.sail@comcast.net>; Sheila Trunkey <strunkey@comcast.net>
Subject: Port of Silverdale - Over-Water Discussion Minutes

Good Morning,

We want to thank you for coming to this meeting and for each of your contributions to the discussion. See attached PDF for the minutes, and please let me know of any errors, omissions, or differences in understanding.

Best,

ABIGAIL OVERTON

Project Coordinator

P.S. This email has been sent to all other attendees, but I had to adjust the file size for you.

Building Community for 30 Years

RICE FERGUS MILLER

275 Fifth Street, Suite 100, Bremerton WA 98337

rfmarch.com

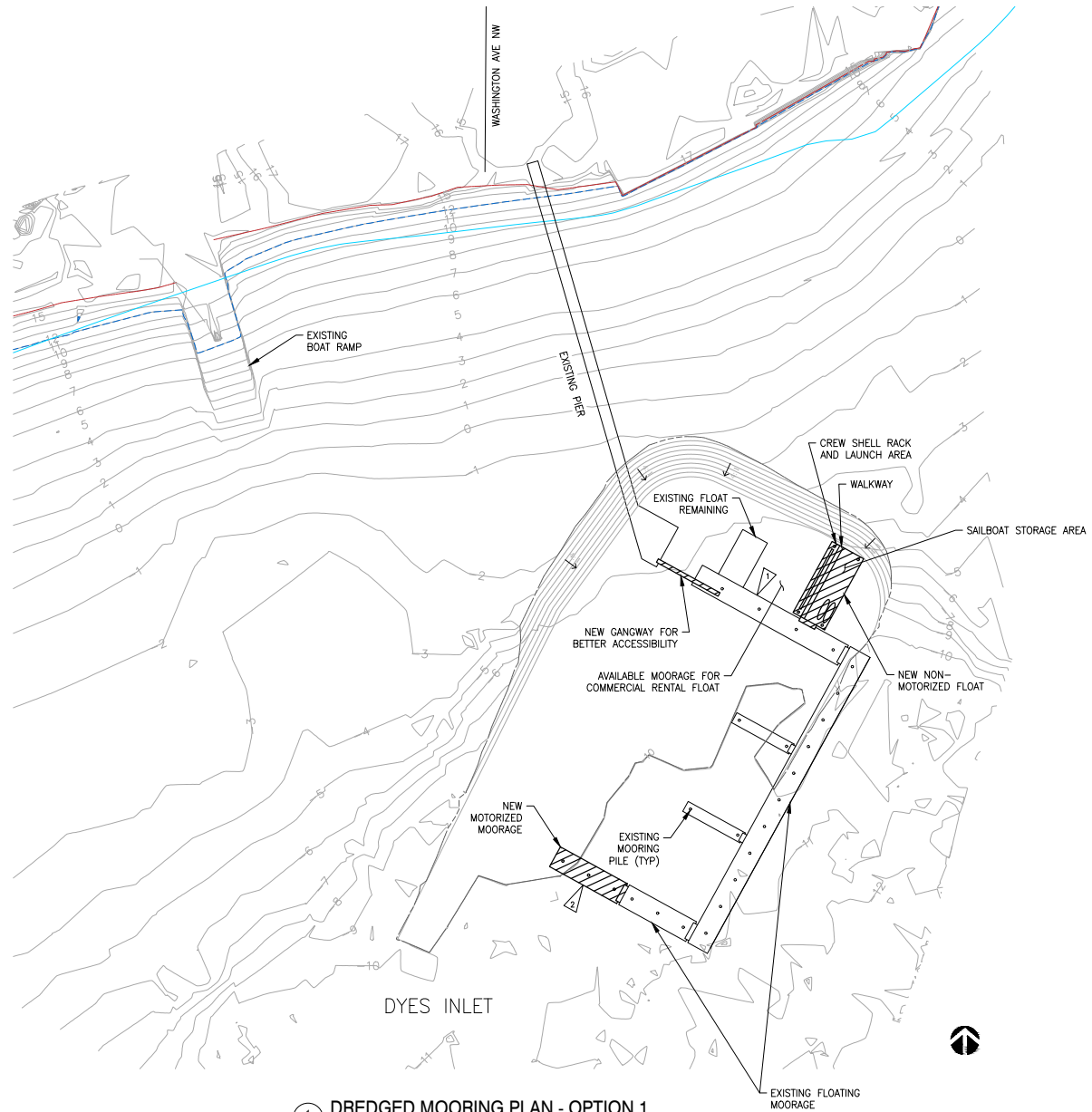
O 360-377-8773

D 360-362-1421

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USER PRIORITIES AND SOLUTIONS

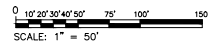
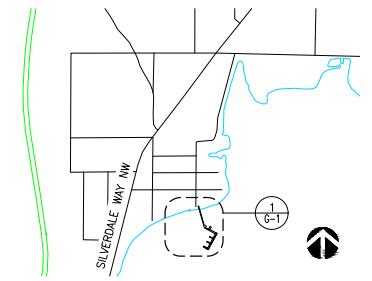
User Group	Top Priority Needs	How it is met in design concepts
Kitsap Sailing and Rowing Foundation	Overwater storage for quick boat use and access. They proposed 24 boats as ideal.	Option 1 and Option 2: 72'x20' portion of the float designated for sailboat storage.
	Amenable for races and events.	Option 1 and Option 2: The sailboat float will be at water level, which is good in itself for viewing. Also, viewing can happen along Waterfront Park, along the existing dock, and from the new upland facility.
Olympic Outdoors Center	Water level access on a float for storing, renting, and launching kayaks/paddle boards. Minimizing handling of boats while on dock	Option 1 and Option 2: Space available for small 20x20 float mooring along the new "non-motorized" section of dock.
	Segregated space from other uses	Option 1 and Option 2: Their float could have a restricted access gate
Clam Island Rowing	Overwater storage racks, so boats don't have to be moved from land at each use. Covering for UV/elements protection if possible.	Option 1 and Option 2: 72'x8' section of new float for shell rack. The 8' width allows for addition of some type of protective covering.
	Specific low freeboard, long edge for side-in launch from dock	Option 1 and Option 2: Low freeboard dock feature could be built in to the new float, along the side adjacent to the rack.
Other, determined by design team and Port	Ability to lock access to new, specific use floats, while maintaining access to public float.	Option 1: Restricted access gate between existing dock and new float. Option 2: New separate access gangway.
	Environmental mitigation options to make project approval more practical	Option 1 and Option 2: Shore restoration, removing creosote piles, grating to reduce overwater shading.
	Modify gangways accessing new and existing floats for make more accessible	Option 1 and Option 2: The existing ramp gangway could be replaced with a longer, wider gangway that is more accessible. However, due to the maximum tidal variation, a full ADA 1:12 slope at all tide ranges is not possible.
	Permanent floats, or temporary seasonal removal?	Option 1 and Option 2: Kayak 20x20 float could likely be temporary. However, if they leave their shed out during non-operating season, float would need to be permanent, and would need piling/s. Due to size, the larger rowing/sailing float should be permanent, with multiple pilings.
	Maintaining space and functionality for motorized use	Option 1 and Option 2: In both options, there would be no disruption to motorized waterway areas. We could add an extension to the existing dock last finger pier, to replace the lost motorized transient moorage slips.



1 DREDGED MOORING PLAN - OPTION 1
1"=50'

FLAG NOTES

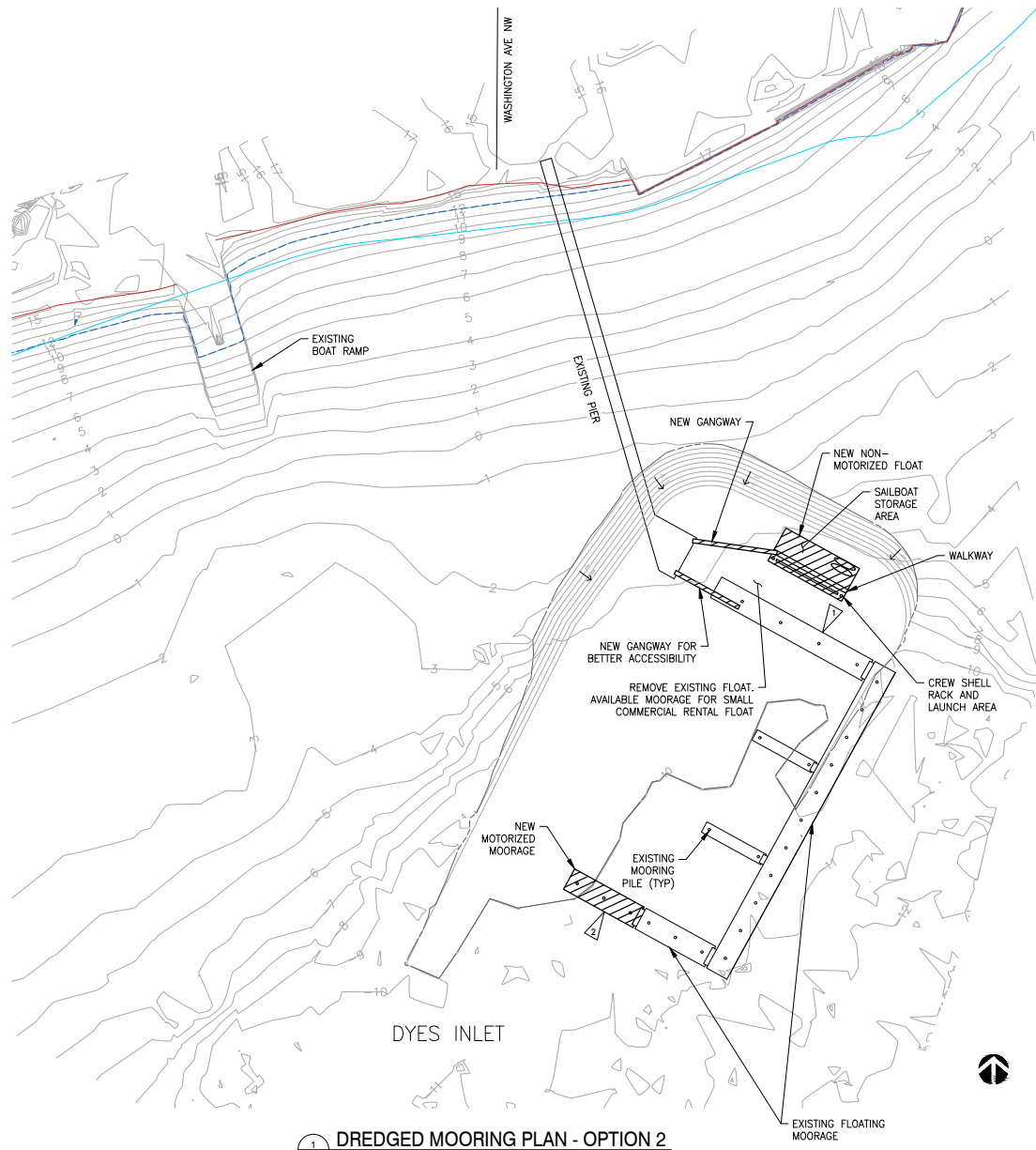
- 1 ~170 LF ON NORTH SIDE OF EXISTING FLOATS.
- 2 ~136 LF TOTAL BETWEEN TWO SIDE OF NEW MOTORIZED MOORAGE. THIS MOORAGE CAN BE LENGTHENED IF NEEDED.



SHEET IS 22x34 ANSI D
IF PRINTING 11x17 USE
50% SCALE FACTOR

**PORT OF SILVERDALE MARINA
MOORING PLAN**

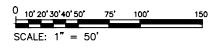
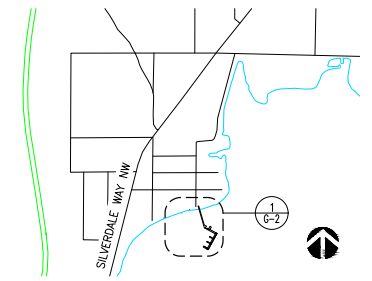
DRAWN:	
DESIGNED:	
CHECKED:	
ISSUE DATE	14 JAN 2019
REVISIONS	
JOB NO	FWRM085
SHT TITLE	SITE PLAN, DREDGED CONDITION



1 DREDGED MOORING PLAN - OPTION 2
1"=50'

FLAG NOTES

- 1 ~170 LF ON NORTH SIDE OF EXISTING FLOATS.
- 2 ~136 LF TOTAL BETWEEN TWO SIDE OF NEW MOTORIZED MOORAGE. THIS MOORAGE CAN BE LENGTHENED IF NEEDED.



SHEET IS 22x34 ANSI D
IF PRINTING 11x17 USE
50% SCALE FACTOR

**PORT OF SILVERDALE MARINA
MOORING PLAN**

DRAWN:	
DESIGNED:	
CHECKED:	
ISSUE DATE	14 JAN 2019
REVISIONS	
JOB NO	FWRM085
SHT TITLE	SITE PLAN, DREDGED CONDITION

Port of Silverdale – Minutes of Regular Meeting on November 15, 2018

Port of Silverdale

Minutes of Regular Meeting

November 15, 2018

1. CALL TO ORDER

Commissioner Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Ashley McNeil of Clam Island Rowing (CIR); Michelle Schmittler of Kitsap Community Agriculture Alliance (KCAA); Steven Starlund of Kitsap County Parks Department; Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); John Kuntz and Forrest Wells of Olympic Outdoor Center (OOC); Ron Easterday of Rice Fergus Miller (RFM); David Brumsickle of Silverdale Cyclery; Randy and Marvel Hunt; Caleb Reese; Commissioner Greaves arrived at 7:14PM; and Bridget Burke of CIR arrived at 8:00PM.

1.1. Agenda – *the agenda was approved as submitted* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The October 16, 2018 Regular meeting minutes were approved as corrected* (motion by Aus, second by Scholfield, unanimous).

Commissioner Aus moved to approve the October 22, 2018 Kitsap All Ports meeting minutes as submitted; second by Commissioner Scholfield.

Discussion: Phil explained that he did not attend the October Kitsap All Ports meeting and was surprised when he read in the minutes that there was a unanimously approved motion requesting him to create a standard Small Works Roster procedure for the group. *The motion carried unanimously.*

3. PUBLIC COMMENT –

3.1. Pump Station 3 & Port Waterfront Plan Update – e-mails between Phil and Kitsap County Public Works Director Andrew Nelson

were reviewed. Mr. Nelson said that the County was pleased the Port supports working together to plan for the relocation of the County pump station and potential combination with a Port facility. Mr. Nelson plans to contact Dave Tucker the Assistant Director for County Utilities to lead the effort for Public Works along with Sewer Utility Senior manager Stella Vakarc and Project Manager Barbara Zaroff. Commissioner Aus has extensive experience with hydraulic pumps, so he and Phil plan to meet with Mr. Tucker next week to discuss the endeavor and requirements. Phil plans to contact the Port's Recreation Conservation Office (RCO) grant manager, Kim Sellars, to discuss the project and determine any requirements the RCO may impose. Ron Easterday explained that RFM plans to set up a meeting with representatives of the non-motorized boating groups to determine the consensus of the desired over- the-water features. Phil explained that Kitsap County Public Works has been the main contact for discussions with the Port, but it was agreed that the Kitsap County Parks Department should also be included. The Port and the Parks Department have worked together in the past on projects such as the County's restroom/Port sower/laundry facility. Steven Starlund of Kitsap County Parks Department introduced himself. He explained that the majority of the public see the Port facilities and the Waterfront Park as one large park. The County's eroding bulkhead at the park was discussed. It was agreed that making the area a contiguous beachfront would be beneficial and that is a project that could possibly be worked on together. Mr. Easterday suggested RFM contact the Parks Department to determine their short-term plans. Phil said that Steve Seago of Waterman Mitigation Partners should also be included in discussions. Phil will send a contact list to RFM of all the players.

Port of Silverdale – Minutes of Regular Meeting on November 15, 2018

Phil received an e-mail from Angie Silva of the Kitsap County Commissioners office. It was regarding an Old Town Sports Complex, but apparently the e-mail was a bit premature as a proposal for the complex is an item on the Public Facilities District's December 3rd agenda.

3.2. Olympic Outdoor Center's (OOC) end of year report dated November 1, 2018 was reviewed. John Kuntz explained that although moving the business from the Port's parking lot to the pier was easier for his employees and on the equipment, it was a tough season believed to be partially due to the move and the poor air quality from the Canadian fires. He plans to have more signage directing the public to the concession out on the pier. Although the security was better for OOC this year, Mr. Kuntz asked that the Port begin storing video surveillance footage longer than the current ten days. He asked for footage to be held at least thirty days. The 2019 Ride the Tide will again be held in June and will commence in Tracyton and end in Port Orchard. This year's event was a great experience with fifty to sixty paddlers. Michelle Schmittler added that when the paddle ended in Port Orchard the farmers market was in full swing and paddlers were provided a \$5.00 off market goods coupon through Visit Kitsap. Mr. Kuntz said that most of those coupons were put towards lunch at the market. He said that recreational kayaking is growing and encouraged the Commissioners to work with Visit Kitsap on marketing the Port facilities. Phil reported that one problem that was brought to his attention was that the kayakers were at the dock near the pump out station, which made it difficult for boaters to safely access it. Forrest Wells explained that they installed an arm to the dock in that area to help stabilize the kayakers to allow customers to get in and out of them safely from the dock. They are considering using a different kayak configuration, which may eliminate the need for the arm. Mr. Kuntz was aware of the issue and

explained it will be further discussed at OOC's upcoming 2019 planning session. Phil said that it was the Port's understanding that the structure would be erected seasonally, yet a rather permanent structure, which remains on the pier, was constructed. Mr. Kuntz agreed that the communication failed along the way. He realizes the Port will charge him a monthly fee to keep the facility on the pier during the off season, but asked the Commissioners to consider more of a storage fee charge. Commissioner Scholfield explained that another issue is the height of the structure. It's much taller than expected and somewhat of an eyesore, although since the kayakers were removed for the season it is not quite as bad. He requested the structure be lowered. Commissioner Aus explained that there have been complaints about it obstructing the view from the pier. Mr. Wells said that they built the roof on a slant with the high end at twelve feet slanting down to eight feet. He said it was built this way to provide much needed additional storage. It also allows for signage. It was agreed that some sort of folding sign could be added to a lower roof. Commissioner Scholfield said that by next season the roof should be no more than ten feet high. There is also concern of how the structure will fare during the rougher weather. Mr. Kuntz plans to add storm straps to it. Randy Hunt asked if it now requires a permit since it's a permanent structure. It was thought that since the building is less than two hundred square feet a permit would not be required. There was discussion about the Port using it during the off season, but there doesn't seem to be a need for it. Caleb Reese offered to rent it in the off-season for \$100 a month; although his intention would be to haul it off the pier. Phil suggested the Commissioners determine the monthly amount to be charged and table this for discussion at next month's meeting.

Commissioner Greaves began chairing the meeting.

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – Steve Trunkey asked to reserve the Port office the first week in April 2019 to offer a Level One sailing instructor course. *It was agreed to allow KSRF's use of the Port office from April 1st through 4th, 2019* (motion by Aus, second by Scholfield, unanimous). Mr. Trunkey reported that a one-day regatta is scheduled for the first Saturday in March. Commissioner Scholfield requested the sails that are being stored in the upstairs area of the Port office shop be moved as he is unable to access that area. Mr. Trunkey assured that ten of the sails would be moved tomorrow, November 16th, and the additional ten would be moved in the near future.

Phil explained that he has been talking with KSRF and CIR representatives about the Port taking over the programs. He provided an outline of the Port of Silverdale Non-motorized boating programs. Currently, the programs are considered Port programs but are run by the two organizations via a lease of the Port equipment. The programs have had much growth over the years and organization volunteers believe it is time for the Port to officially operate the programs. Phil explained that it would be structured so that all costs, including hiring personnel to complete all the necessary administrative duties, would be covered through the class fees. It would be a self-supporting program. KSRF contracts the coach positions and CIR actually has a coach on payroll, these costs would also be covered through the class fees. Phil explained that he talked to an assistant Attorney General about this and was told that as long as all the programs are available and open to the public,

it shouldn't be a problem. Primarily KSRF has been made available to the Central Kitsap School District, but they also offer Monday night sailing that is open to the public during the season. They have done their best at accommodating individuals who are interested in learning how to sail and/or row. Ms. Burke explained that all applications and payments are completed online. At this point the Port doesn't accept electronic payments as it seems to be frowned upon by the County (that acts as the Port's Treasurer) as well as the State Auditor. It's not that it cannot be done; just one of the many details that would need attention prior to a transition. Another possible issue is that the volunteers wouldn't have immediate access to funds as the Port makes payments just once a month during the regular meetings. It was suggested that one volunteer per program be issued a credit card with a limited balance of no more than \$1,000 on it and require receipts to be submitted monthly prior to the bill being paid. Ms. Burke explained that there is already a system in place where purchases in excess of \$200 are not allowed without preapproval from the Board. Commissioner Scholfield suggested the volunteers continue as is so that they have control over the programs. The County charged fifteen percent when they ran the program. If the goal is to avoid having to pay the leasehold excise tax, which is approximated to be an annual \$500 for both programs, it doesn't seem logical because the costs associated if the Port takes over will far exceed the annual amount of leasehold excise taxes. Phil intends to generate a formal agreement for further review and possible adoption at next month's meeting. Mr. Trunkey said that he plans to get the

Port of Silverdale – Minutes of Regular Meeting on November 15, 2018

registration up on the website prior to the school's winter break. Adding it to the Port's website was discussed. Mr. Trunkey wasn't in favor of that. Commissioner Scholfield explained that he is working on a new Port website. While attending the WPPA Small Ports Seminar, he learned about software that other Ports recommended, which he has recently purchased.

Commissioner Greaves was tasked with drawing the winning ticket for CIR's raffle. Congratulations to Lorraine Jones who won ten pounds of Alaskan King Crab.

c. Non-motorized boat storage – Ms. Burke has ordered a new sign. She will submit the receipt for approximately \$80 for reimbursement. Marking the storage spaces was tabled.

4.2. Port Facilities

a. Dredging – Phil reported that Research Support Services (RSS) is the company that completed the core sampling. The cost for the sampling totaled \$5,619.00. Phil sent an e-mail dated October 24, 2018, which was prior to the sampling, to the Commissioners explaining the additional costs and that if no one objected he would give the okay for RSS to proceed. *It was agreed to authorize up to \$6,000 for the core sampling related to the dredging project* (motion by Scholfield, second Aus, unanimous).

b. Port Rules/No Wake/Windsock – nothing to report.

c. Department of Natural Resources (DNR) expansion of the Port's outer water boundaries - nothing to report.

d. Handling Pier – has been removed for

the season.

e. Gangway redesign – nothing to report.

f. Website – Commissioner Scholfield is working on an updated website as previously discussed.

4.3. Eagle Scout Project – an e-mail dated November 8, 2018 from John West was reviewed. He provided a material list for the pier life jacket loaner station. Commissioner Scholfield said that he would discuss it further with Tim as Chemonite lumber must be used and Tim may have extra available.

4.4. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the toilets backed up again and caused issues at the adjacent building (9004 Washington Avenue/DRC). Mike has since fixed the problem. Installing new floors at the building was discussed. Mike provided a sample of the flooring to be used. He estimated the material costs for replacement of the floors to be \$2100, but was unsure of the labor costs because of unknown factors. One issue is that the floors in that building are very uneven. It was agreed that if the tenant is willing to enter into a five-year lease at the current rate the floor throughout the building would be replaced. The tenant, Elizabeth Barnes, had also requested that the deck be repainted, but Mike said it just needs a good power washing, which he will complete. She has also been asking that a wheelchair ramp be installed. This has been on hold until the sewer connection issue is resolved, but it was agreed that a temporary/removable type ramp could be constructed. Commissioner Scholfield said that the exterior of the building

needs to be repainted, possibly next summer.

b. 3215 NW Lowell Street, Suite 231 – the tenant gave notice that she will not be renewing the lease at the end of the year

c. 3473 NW Byron Street/Vacant – Ordering a cost estimate report for the building was tabled.

4.5. Paving the alley between 9004 Washington and Silverdale Autoworks was discussed. Since the Small Works rules have changed the Port is no longer required to go out for competitive bidding using the Small Works Roster for projects that are under \$40,000; although, necessary permits would need to be obtained and prevailing wages still need to be paid. Commissioner Scholfield said that it is his understanding that Silverdale Autoworks' contractor already has a right-of-way permit in place. It was suggested that the Port reimburse Silverdale Autoworks for the Port's portion of the paving although there would have to be proof that prevailing wages were paid.

4.6. 2019 Final Budget was reviewed. There were no changes from the Preliminary Budget. *It was agreed to adopt Resolution 2018-01 Adoption of the 2019 Final Budget as submitted* (motion by Aus, second by Scholfield, unanimous).

It was agreed to adopt Resolution 2018-02 2019 Levy Increase as submitted (motion by Scholfield, second by Aus, unanimous).

NEW BUSINESS

5.1. Pet waste on Port property – an e-mail dated November 9, 2018 from Jeff

Schumacher was reviewed. He asked that the Port consider increasing the penalty and fine for violations of the Dog Leash and Pet Waste law. The Port doesn't charge a fine for such a violation. Commissioner Scholfield suggested Mr. Schumacher be asked to contact the Kitsap Public Health District.

5.2. Silverdale Christmas Tree Event Sponsorship flyer was reviewed. *It was agreed to donate \$50 to the Silverdale Dandy Lions in support of the annual Christmas Tree Festival* (motion by Aus, second by Scholfield, unanimous).

6. **SAFETY** – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$25,365.33, checks numbering 11923 through 11946 and Electronic Funds Transfer 2018-11 to the U.S. Treasury in the amount of \$1,743.46, were approved (motion by Aus, second by Scholfield, unanimous).

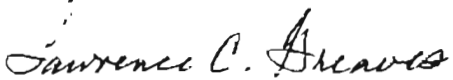
8. **EXECUTIVE SESSION** – None


9. ADJOURN

At 9:01PM the meeting adjourned (motion by Scholfield, second by Aus, unanimous).

Approved:


Henry Aus, Commissioner


Lawrence Greaves, Commissioner


Ed Scholfield, Commissioner

Port of Silverdale
Minutes of Regular Meeting
December 20, 2018

1. CALL TO ORDER

Commissioner Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Bridget Burke and Vanessa Allison of Clam Island Rowing (CIR); John Bouck, Julie Jablonski, Greg Jacobs and Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Carla Larson of Whaling Days; Caleb Reese; Taylor Sims; Susan and Andrew Zetty; and Tim Knapp of TIKAR Services arrived at 7:25PM.

1.1. Agenda – *the agenda was approved as submitted* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The November 15, 2018 Regular meeting minutes were approved as submitted* (motion by Scholfield, second by Aus, unanimous).

3. PUBLIC COMMENT –

KSRF representatives explained that a new FJ sailboat valued at approximately \$8,500 was recently donated to the foundation. Title will eventually be transferred over to the Port.

Greg Jacobs reported that a piece of metal roofing material on top of the Old Town Pub is loose and is concerned that it may fall off and hurt someone. He wanted to make sure the Port was aware of it as it should be fixed. He also said that the sailboat that is tied to the dock may be causing damage to the cleat it's tied to.

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – Steve Trunkey plans to submit an end-of-year report next month so

that it can include the December data and be a complete 2018 end-of-year report. Mr. Trunkey said that KSRF had a good year. Financially the ending balance will be about the same as what they began with at the start of the year. They have added new equipment including new sails. KSRF ranked #2 for overall regatta performance and the Sportsmanship Award. KSRF hosted the biggest team races ever and Silverdale is talked about as being the best venue in the Northwest. KSRF normally holds three weeks of summer camps, but plan to increase that to seven weeks in 2019.

c. Agreement - Mr. Turnkey had provided anticipated 2019 income and expense sheets for the Commissioners' review as they decide if the Port will run the programs directly. Phil generated an outline of the Port of Silverdale Facilities and Programs for Water Access. It was reviewed. Julie Jablonski said that speaking from a parent's view, she is so thankful for the program. She feels the program has changed her daughter's life for the better as it seems to have boosted her self-confidence and self-esteem. She is now more willing to help out and takes it upon herself to do so. John Bouck agreed that the program has been very beneficial for his son and one of the reasons he and his family decided to move to the area was the opportunities to get out on the water. Ms. Burke added that sailing is one of the few sports that smaller-framed kids can really excel in and she is very thankful for it. Mr. Trunkey added that the sailors have formed quite a community. It's much bigger than getting kids out sailing; it has become a huge part of their lives. Commissioner Scholfield wanted it noted for the record that his son, Jeffrey Scholfield, is a member of the sailing program. Commissioner Scholfield is skeptical that the Port can legally run the program as he has reviewed the RCW and doesn't see where it states that a Port can actually run a program. He read that the Port

Port of Silverdale – Minutes of Regular Meeting on December 20, 2018

can provide the facilities for a program, but nothing about the Port actually running such a program. He said that similar programs are run by City or County Parks Departments, not Ports. He also read that the Port can't even have a park facility without having the County's approval. Mr. Trunkey said that he has also read the RCW's and doesn't see anything prohibiting the Port from running the programs. Phil said that as the Port's attorney he also has reviewed the RCW's and does not agree with Commissioner Scholfield's conclusion. Phil reminded everyone that he talked to an assistant Attorney General about this and was told that as long as all the programs are available and open to the public, it shouldn't be a problem. CIR and KSRF personnel agreed that no other Ports, to their knowledge, are running such programs, but that doesn't mean it can't be done and maybe Silverdale can blaze the trail. Commissioner Scholfield said that if KSRF and CIR are just trying to save the annual \$500 in Leasehold Excise tax, it is not worth it because the Port will have to charge a percentage to administer the program and it will add up to a lot more than the \$500 they would be saving in the tax. Mr. Trunkey and Ms. Burke both agreed that saving the money paid to the State for the Leasehold Excise tax is not the reasoning behind this push. Both Programs are very successful and continue to grow, which is wonderful, but it puts a lot of pressure and added work on the volunteers. It would be a relief if the Port would take over the registrations and fees portion of the programs. This would free up the volunteers' time enabling them to focus on teaching individuals to row and sail. Mr. Trunkey said that he has researched for an on-line registration program and suggests the Port look into Sports Sites software. From what he has learned it will make it very easy for registration. Caleb Reese questioned why KSRF/CIR haven't used the software since it will make the process so much easier. Commissioner Scholfield said that there is a great group of

individuals running the programs now and it's successful. He agreed with Mr. Reese why don't they just begin to use this new software to help alleviate the registration workload. Vanessa Allison said that it basically comes down to the fact that the two programs primarily fall on the backs of two people; Mr. Trunkey and Ms. Burke. If these two individuals stop volunteering the programs stop. If the Port were to take it over it would have a more permanent foundation and since it's a self-sustaining program it wouldn't negatively affect the Port. Commissioner Scholfield said that it's self-sustaining only because of all the volunteer hours. If the volunteers go away the programs would also have to go away; otherwise, it would no longer be self-sustaining. Ms. Burke said that they would still manage the programs and would only ask that the Port manage the registration, collect the fees and officially take ownership of the programs. CIR and KSRF members said that they have come to the conclusion that since permanent moorage is not offered in Silverdale it makes the area even more attractive to the non-motorized boating community. Commissioner Scholfield said that although permanent moorage is not offered, the grant received to help build the Port's moorage facility was from the Recreation Conservation Office (RCO) non-refunded boat gas tax, so it was aimed at benefiting motorized boating enthusiasts. The Port already ran into an issue with the RCO when they realized that the sailboat float was attached to the moorage facility. They held the Port's retainer and Phil had to be involved with helping the RCO agree to the placement of the sailboat float. Commissioner Scholfield suggested KSRF and CIR ask the school district to run the programs. Apparently, there is no interest from the school district to run the programs. Ms. Burke said that the programs are already tied to the Port; it just seems logical for the Port to take over. Ms. Jablonski agreed and said that the Port was gifted

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hundreds of thousands of dollars of equipment. They are already a partnership. Commissioner Scholfield questioned the insurance. Currently the equipment is covered under the Port's Enduris policy. According to Enduris the Port can expect a \$5,500 to \$6,000 increase to cover paid personnel and volunteers. Mr. Turnkey said that they currently hold their own insurance, but the Port would need to cover the program and those costs would be covered through the registration fees to ensure the programs are self-sustaining. Commissioner Scholfield said that there is a great group of individuals running the programs now; if it ain't broke don't fix it. He doesn't want the Port to be in the hiring/firing business. Ms. Burke said that they have hired instructors for the season so there is an end date to each "employee" whether it's a contracted paid position or an actual employee. She also said that they would still want to be in charge of hiring coaches. Commissioner Scholfield requested the Port not be responsible for any employees of the programs. KSRF and CIR members said that Silverdale is a unique Port community and they see it as an opportunity. They also see it as a good fit with the Port as it supports the Port's economic development aspect.

Commissioner Scholfield questioned that as he sees most of the families bringing their own food and drinks down to the area and rarely has seen attendees/participants purchasing items from the local businesses. KSRF and CIR members disagreed as it brings foot traffic to the area and more times than not parents wonder around the area while their kids are in class not to mention the hundreds of individuals the regattas attract. Mr. Jacobs said that they could focus more on economic development and sponsor more regattas. He thanked Commissioner Scholfield for his feedback and would like the group to keep identifying possible setbacks, pros and cons so that they can plan. Commissioner Aus said that he was in favor of the Port running the programs. Commissioner Scholfield said that

he would need more definition before committing to anything. Tim offered his opinion. He said that he really loves the programs and what they offer especially for kids, but such programs don't exist without individuals with a passion and if it is left to the Port to find such individuals, chances are it won't happen. They should recognize that the Port supports them, but they are the core the driving force not the Port. The Port can assist the core, but it's up to the core to keep the program running. Ms. Burke said that if the Port would just begin taking over registrations she would be happy. Mr. Trunkey said that the season starts in February and the insurance needs to be paid in January unless the Port agrees to run the programs. It was agreed to hold a Special Meeting on January 8th, 2019 at 3:00PM at the Port office to determine the details of transitioning the programs to the Port and formally decide.

c. Non-motorized boat storage – Commissioner Scholfield will print out numbers 1 to 30 to use to mark off the spaces in the non-motorized storage area.

4.2. Pump Station 3 & Port Waterfront Plan Update – Commissioner Aus and Phil met with Dave Tucker the assistant Director of Kitsap County Public Works and discussed moving the County's sewage pump station 3 away from the waterfront onto Port property. Mr. Tucker was quite supportive and suggested Commissioner Aus and Phil take a field trip to Gig Harbor's new lift station, restrooms and viewing platform at Skansie Park, which they did. They found it to be a very nice multi-purpose facility and thought something similar could be built on Port property. Phil attended the meeting that RFM hosted with several non-motorized boating enthusiasts. He was impressed with the turnout of KSRF and CIR representatives. There was discussion about moving a portion of the moorage facility further south and designating the shallow area for the

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non-motorized boating traffic. Phil said that RCO might be more amenable to that and it would be beneficial for safety. Ben Anderson of Art Anderson and Associates may be talking with Tim to further discuss. A letter dated October 16, 2018 from Rice Fergus Miller (RFM) re: Amendment 2 to the original agreement with the Port was reviewed. Phil explained that the original scope had increased beyond what they were originally asked to complete. This amendment totaled \$45,574. It was unknown if that amount included the \$15,000 that had already been paid to RFM. Phil suggested he and Commissioner Greaves talk to RFM personnel to determine the specifics of the amendment and negotiate.

4.3. Olympic Outdoor Center (OOC) – Commissioners discussed how much to charge OOC since the structure is staying on the pier off season. It was agreed that no matter what the full Leasehold Excise tax of \$64.20 will need to be paid to the State Department of Revenue. *It was agreed to charge OOC \$217.90 per month, which is half of the seasonal amount charged (\$564.20 / 2 = \$282.10; \$217.90 storage fee \$64.20 1/h excise tax) from October thru May* (motion by Aus, second by Scholfield, unanimous).

4.4. Eagle Scout Service Project – Tim's information will be e-mailed to Eagle Scout, Jack West.

4.5. Paving of alley – Phil reported that he contacted Molly Foster of Kitsap County Right-of-Way Division. Ms. Foster was going to have another County employee contact him regarding paving of the alley, but he hasn't heard from anyone yet.

4.6. Port Facilities

a. Dredging – a letter dated December 13, 2018 from Marine Surveys and Assessments (MSA) was reviewed. It explained that the

Port's dredge project has gone over budget by \$11,880.60 due to circumstances beyond MSA's control. Phil agreed that the Army Corps of Engineers' rules had changed midway through the project and so that impacted the requirements. *It was agreed to pay Marine Surveys and Assessments the additional \$11,880.60 as outlined in correspondence dated December 13, 2018 due to unforeseen circumstances* (motion by Scholfield, second Aus, unanimous).

b. Port Rules/No Wake Buoys – Tim plans to submit an estimate for the buoys next month.

c. Department of Natural Resources (DNR) expansion of the Port's outer water boundaries - nothing to report.

d. Gangway redesign – Tim reported that transitioning to a non-roller system is not possible due to the weight of the gangway. He is working on a cleaner design.

e. Sailboat (WN6823JF) had been anchored out, but someone tied it to the Port's facility. It has since been chained to the Port's moorage facility because there is over \$400 in outstanding moorage fees. The owner, Charles Lewis, Jr., was trespassed from Port property several month's ago and is believed to be currently residing in the Kitsap County jail. Tim said that the boat has taken on a lot of water. Commissioner Scholfield plans to pump it out within the next few days.

f. Website – Commissioner Scholfield is working on transferring the security camera footage to the new website he is creating.

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4.7. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the tenant signed a five-year lease. Mike will be working with the tenant to complete the necessary conditions of the new lease. The toilets backed up again and caused issues at the adjacent building (9004 Washington Avenue/DRC). Mike has again fixed the problem.

b. 3473 NW Byron Street/Vacant – Ordering a cost estimate report for the building was tabled.

NEW BUSINESS

5.1. 2018 Budget vs. Actual Report was reviewed.

5.2. Paid Family Medical Leave – in 2019 the State will require employers to provide paid family and medical leave benefits to employees who meet certain criteria. Since the Port has paid staff that meet the criteria, action is required. *It was agreed to pay the Paid Family and Medical Leave Program premium on behalf of Port employees beginning January 2019* (motion by Scholfield, second by Aus, unanimous).

5.3. Silverdale Destination Guide Magazine – the Silverdale Chamber of Commerce is changing from a walking map to an actual magazine. They are hoping the Port will continue to advertise. *It was agreed to place a ¼ page add in the 2019 Silverdale Destination Guide for \$395* (motion by Scholfield, second by Aus, unanimous).

5.4. Commissioner Scholfield's mileage reimbursement includes \$68.53 for Port items

he accidentally charged to his personal account.

5.5. Death Cab for Cutie held a video shoot on Port property. Patty Graf-Hoke of Visit Kitsap coordinated it and proof of insurance was provided.

6. **SAFETY** – nothing to report. Commissioner Scholfield said that he will be sure to wear his life vest when pumping out the water from the sailboat that is chained to the moorage facility.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$29,495.70, checks numbering 11947 through 11974 and Electronic Funds Transfer 2018-12 to the U.S. Treasury in the amount of \$1,704.34, were approved (motion by Aus, second by Scholfield, unanimous).

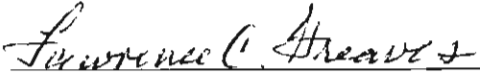
8. EXECUTIVE SESSION – None

9. ADJOURN

At 9:34PM the meeting adjourned (motion by Aus, second by Scholfield, unanimous).

Approved:


Henry Aus, Commissioner


Lawrence Greaves, Commissioner


Ed Scholfield, Commissioner

Port of Silverdale – Minutes of Regular Meeting on January 17, 2019

Port of Silverdale

Minutes of Regular Meeting

January 17, 2019

1. CALL TO ORDER

Commissioner Lawrence Greaves called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Ed Scholfield; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Bridget Burke of Clam Island Rowing (CIR); John Bouck, Greg Jacobs and Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Carla Larson of Whaling Days; John Kuntz and Forrest Wells of Olympic Outdoor Center (OOC); Ron Easterday of Rice Fergus Miller (RFM); Andrew Thorsen of Art Anderson Associates (AAA); Mickey Hall; Randy and Marvel Hunt; Caleb Reese; and Tim Knapp of TIKAR Services arrived at 7:14PM.

1.1. Agenda – *the agenda was approved as submitted* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The December 20, 2018 Regular meeting minutes and the January 8, 2019 Special meeting minutes were approved as submitted* (motion by Aus; second by Scholfield, unanimous).

3. ELECTION OF OFFICERS

3.1. *It was agreed that Commissioner Scholfield will serve as Chairman* (motion by Aus; second by Scholfield, unanimous).

It was agreed that Commissioner Aus will serve as Secretary (motion by Scholfield; second by Aus, unanimous).

Commissioner Scholfield began to chair the meeting.

4. PUBLIC COMMENT –

4.1. Complaint – Kymberly Smith was scheduled on the agenda, but was not in

attendance. Ms. Smith called the Port office to make a complaint against the Safe Security guard on duty on the night of December 29th, 2018. She claimed the guard was very abrupt and intimidating. Also, a local Old Town resident, Christine Reed, came into the Port office to make a similar complaint against the guard who was on duty on December 31st, 2018. It was believed to be the same guard. A call was placed to both Christine Scott the owner of Safe Security as well as Jason the supervisor. They have since discussed the complaints with the officer.

4.2. Shelter on pier – John Kuntz of OOC was notified that the Commissioners decided last month that OOC would be charged half of the seasonal rent for the shelter to remain on the pier during the off-season. OOC pays \$500 plus \$64.20 for Leasehold/Excise Tax from June through September. Mr. Kuntz explained that OOC cannot afford to pay \$282.10 during the offseason. He provided a handout showing the rates charged by the Port of Poulsbo, which is \$.49 a square foot. With the rack removed the footprint of the structure out on the pier is 120 square feet. He asked that the Commissioners consider charging a per square foot fee similar to the Port of Poulsbo, which would come to \$58.80 per month. Randy Hunt asked if the building is legal since it is actually a permanent structure and no permit was obtained to build it. When OOC first proposed the venture to the Port it was said that the structure would be temporary and since permits are not required for temporary structures OOC did not obtain a permit, but since it is staying on the pier it would most likely be considered a permanent structure. Greg Jacobs said that since it is just 120 square feet and doesn't include electricity it is more like a shed, which wouldn't require a permit. Mr. Hunt suggested the Port call the County and ask. Commissioner Scholfield explained that it's not the Port's place as it was outlined to OOC that they could build the

Port of Silverdale – Minutes of Regular Meeting on January 17, 2019

structure on the pier contingent upon receiving all the required permits. Caleb Reese voiced his dissatisfaction with the building still being on the pier. He said that it was originally agreed by Commissioners to allow OOC to build a temporary structure on the pier, which would be there for approximately four months out of the year. As a member of the public that has to look at the structure year-round; he is not happy. Commissioner Scholfield said that either the agreement needs to be modified or OOC should pay half of the seasonal rent according to last month's decision. Mr. Kuntz said that it's marginal for his business to be at the Silverdale location, especially with the random sewage spills that cause his business to close. Phil suggested the building be made into a portable structure so that it can be removable. Mr. Jacobs said that he worries anything more temporary than the current structure would cause damage to the pier. Commissioner Aus suggested the Port charge one-fourth of the normal amount. Mr. Reese disagreed and said that for that amount you can't even rent a storage space in the area and he feels that OOC is taking advantage of the Port and the public because they got permission from the Port to use an area of the pier for a temporary structure and it turns out to be a permanent structure. Commissioner Greaves suggested OOC pay the Port \$100 per month during the off-season. Phil said that he didn't think the State Auditor would have a problem with that but the Commissioners need to find the balance between the Port's obligation to the public and working with OOC. ***Commissioner Greaves moved to charge Olympic Outdoor Center \$100 per month, which includes Leasehold/Excise tax, to keep the concession stand on the pier from October through May, second by Commissioner Aus.*** Discussion: Bridget Burke suggested the stand become a Port facility and the Port rent it out to OOC or whoever wants to rent it. Phil said that generally within the Port's leases when an item becomes a fixture it belongs to the Port. The stand is kind of a

fixture now, so that could be considered. ***The motion carried unanimously.***

4.3. 3295 NW Lowell Street – tenants Tina Abeel and David Holman were scheduled to attend tonight's meeting, but were not in attendance. They wanted to negotiate entering into a longer lease.

4.4. Whaling Days – Carla Larson provided the signed Use of Port Property Application for the 2019 festival. Prior to signing the application Commissioner Scholfield crossed out "excluding the dock". Phil agreed that it should be crossed out because festival goers also go on the pier and docks. Ms. Larson agreed.

4.5. Pump Station 3 & Port Waterfront Plan Update – Ron Easterday provided overwater concepts and a list of user priorities and solutions. The two concepts, Option 1 and Option 2, were reviewed. Andrew Thorsen of AAA explained that it was determined that the idea to move the existing docks to create space for a non-motorized boating area was not practical. Mr. Easterday explained that RFM is just hoping to get a general concept so that they can make a big stride to finish up the Port's comp plan. It was agreed that Option 2 was the more desirable of the two concepts. Mr. Easterday made note of several changes to the design that were discussed. Phil has not yet talked to Kim Sellars the Port's Grant Manager with the Recreation Conservation Office (RCO). Once the design is updated he plans to contact Ms. Sellars to discuss. Phil will draw up an interlocal agreement between the Port and the County regarding the new placement of the pump station. Mr. Easterday said that it is his understanding that the County will be working on the project design in May 2019 and the Port needs to be involved and suggested that contact be made with the County as soon as possible. Amendment 2 to RFM's contract with the Port has not been signed. Commissioner Greaves and Phil will

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schedule a time to meet with RFM personnel to further discuss the amendment. Ms. Burke asked how the Port manages funds for capital improvement projects. It was explained that the Port has some funds set aside for projects, but for the most part the Port would be relying on RCO grants.

5. UNFINISHED BUSINESS –

5.1. Port Programs

a. Sailing – the end-of-year report was reviewed.

b. Rowing – Ms. Burke explained that CIR was able to use the Port's government rate with AquaTech to purchase a new motor for the Boston Whaler, which is intended to be donated to the Port. So now CIR has two safety boats available which should grow the program even more allowing experienced rowers out on the water at the same time as less experienced rowers. CIR will also be installing a rolling gate on the street side of Rowing Alley.

c. Agreement – Phil provided two separate agreements between the Port and Kitsap Sailing and Rowing Foundation as well as Clam Island Rowing. The agreements were reviewed. *It was agreed to enter into the Agreement between the Port of Silverdale and Kitsap Sailing and Rowing Foundation and Clam Island Rowing for Administration and Management of the Port's Sailing and Rowing Programs* (motion by Aus; second by Greaves; unanimous). The agreements were signed by the Commissioners and John Bouck signed for KSRF. Ms. Burke explained that she would present the agreement to the CIR Board for approval and signature at their February 1st, 2019 meeting. Several details of this transition have yet to be determined including insurance coverage, payment processing, etc. Since registration for sailing classes is basically in process, it was agreed that participants will make payments directly to the Port in the form

of a check and delivered to the Port office. Mr. Trunkey agreed that would work for now, with the goal of having on-line registration in place prior to the summer registration.

d. Non-motorized boat storage – nothing to report.

5.2. Port Facilities

a. Dredging – payment in the amount of \$14,990.83 to Marine Surveys and Assessments (MSA) is being authorized at tonight's meeting.

b. Port Rules/No Wake Buoys – Tim provided an estimate for installing "no wake" buoys at the Port's outer water boundary. *It was agreed to authorize TIKAR Services to install four buoys as outlined in Estimate #2019-1051 totaling \$7,848.00* (motion by Greaves; second by Aus, unanimous).

c. Department of Natural Resources (DNR) expansion of the Port's outer water boundaries - nothing to report.

d. Gangway redesign – Tim plans to bring options of a new design to next month's meeting.

e. Sailboat (WN6823JF) continues to be chained to the Port's facility.

f. Website – Commissioner Scholfield continues his efforts on the new website.

5.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – Mike reported that the new flooring is being installed. The sewage grinder in the basement of the adjacent building (9004 Washington Avenue/DRC) had a slight leak. Mike fixed it.

Port of Silverdale – Minutes of Regular Meeting on January 17, 2019

b. 3473 NW Byron Street/Vacant –

Ordering a cost estimate report for the building continues to be tabled. The Port received several notices about a piece of loose metal on the roof which was a safety concern because it looked as if it might fall off at any time. The pitch of the roof is very steep and it was determined that several areas in the roof are rotten, so it was a safety hazard for anyone without the proper equipment to access the roof. Emel Tree Service was called and with the use of their bucket truck they removed the hazardous metal. They contacted the Port to inform there wouldn't be a charge and wished the Port a happy 2019. It was agreed to send Emel Tree Service a thank you letter from the Port.

c. 3215 Lowell, Suite 231 – a check in the amount of \$530.00 to Lee Ann Rhoden is being authorized at tonight's meeting. This is a refund of the \$230 last month's rent as Ms. Rhoden prepaid eight month's rent, and the \$300 damage deposit as the condition of the unit was left satisfactory.

5.4. Eagle Scout Service Project – Tim hasn't heard from the Eagle Scout, Jack West. The e-mail that was sent to Mr. West providing him with Tim's information will be resent. Mr. Trunkey explained that he knows another Eagle Scout that is looking for a project to earn his badge. Mr. Trunkey has a few ideas that would benefit the Port, which he plans to further discuss with Tim.

5.5. Paving of alley – Phil hasn't received a response from the County regarding the need for a right-of-way permit to pave the alley. Commissioner Scholfield said that the portion of the alley on the west side of Washington

was destroyed. A call was placed to the County to request them to repair it. The County said that they don't deal with alleyways; it is the responsibility of the local property owners. So apparently a right-of-way permit would not be necessary. Commissioner Scholfield requested the Port just go ahead and get the alley paved.

6. NEW BUSINESS

6.1. Rule addition – Commissioner Scholfield reported that there are a number of individuals smoking marijuana on Port property. It was agreed that "No Smoking Marijuana" would not need to be added to the Port's rules since it is illegal to smoke it in public. It was agreed that individuals known to be smoking marijuana on Port property should be given a warning and if it continues or they refuse to stop they should be trespassed.

6.2. Silverdale Chamber had requested Port staff to write up an article about the Port to be added to the 2019 Destination Guide. The article was attached to the agenda packet for the Commissioners to read at their leisure.

6. SAFETY – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$53,151.33, checks numbering 11975 through 12004 and Electronic Funds Transfer 2019-01 to the U.S. Treasury in the amount of \$1,717.34, were approved (motion by Aus, second by Scholfield, unanimous).

8. EXECUTIVE SESSION – At 8:52PM it was announced that the meeting would be going into Executive Session for approximately ten minutes to discuss possible litigation.

Port of Silverdale – Minutes of Regular Meeting on January 17, 2019

At 9:05PM the meeting returned to Regular Session.

Commissioner Scholfield said that the gutter on the Old Town Tavern is about ready to fall off and someone once again broke into the building. It has since been secured.

Commissioner Scholfield said that he might purchase a camera and aim it at the building. Phil explained that he received a call from an individual who is interested in salvaging as much as possible from the building. It was agreed that determining the salvage value would be beneficial.

9. ADJOURN

At 9:10PM the meeting adjourned (motion by Aus, second by Scholfield, unanimous).

Approved:



Henry Aus, Commissioner

Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Port of Silverdale – Minutes of Regular Meeting on February 21, 2019

Port of Silverdale

Minutes of Regular Meeting
February 21, 2019

1. CALL TO ORDER

Commissioner Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Bridget Burke of Clam Island Rowing (CIR); John Bouck, Julie Jablonski and Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Carla Larson of Whaling Days; Steve Slaton of the Silverdale Rotary Club; Roy Sahali of Sahali Farms; Ron Easterday and Steve Rice of Rice Fergus Miller (RFM); Laurie Aardal and Skip Denny of the Hawaiian Canoe Club – Hui Heihei Wa'a; Ken Brooks; Kevin Cavanaugh; Mickey Hall; Randy and Marvel Hunt; and Caleb Reese.

1.1. Agenda – *the agenda was approved as submitted* (motion by Aus, second by Scholfield, unanimous).

2. CONSENT AGENDA

2.1. *The January 17, 2019 Regular meeting minutes and the January 28, 2019 Kitsap All Ports meeting minutes were approved as submitted* (motion by Aus; second by Scholfield, unanimous).

It was agreed to excuse Commissioner Greaves' absence (motion by Aus; second by Scholfield, unanimous).

3. PUBLIC COMMENT –

3.1. Use of Port Property application – Steve Slaton of the Silverdale Rotary Club submitted the application to use a portion of Port property to hold the annual Duck Race on the Sunday during Whaling Days. Mr. Slaton explained that all of the net proceeds of the Duck Race are used to support Rotary charitable projects with the majority of the funds going directly back into the community. *It was agreed to allow the Silverdale Rotary Club to hold the Duck Race*

on July 28, 2019 as outlined in the Use of Port Property Application (motion by Aus, second by Scholfield, unanimous). Commissioner Scholfield requested that the Silverdale Rotary take the lead in securing space at the dock for the duck barge. Mr. Slaton agreed and thanked the Commissioners.

3.2 Use of Port Property application – Carla Larson of Whaling Days explained that she brought the information from the last Port meeting back to the Whaling Days Board. She was tasked with relaying to the Port that the Whaling Days Board doesn't agree that the festival insurance should cover the pier/floating moorage facility because Whaling Days doesn't receive any of the moorage revenue. Commissioner Scholfield explained that the Port has repeatedly asked Whaling Days to monitor the pier/floating facility after hours and inform the public that the area is closed unless they have a boat moored at the facility. Ms. Larson explained that a lot of the non-boating public like to stand on the pier to watch the fireworks on the Friday night of the event, which is after hours. Commissioner Scholfield said at that time access on the pier might be okay as long as it's monitored but non-boaters shouldn't have access to the floating moorage facility. He suggested wristbands be provided to boaters and their guests. Commissioner Scholfield said that Whaling Days either has to insure the pier/floating moorage facility or have security down there to keep the general public off of it after hours. Ms. Larson said that unless Whaling Days is receiving the moorage fees, the Board doesn't feel it is Whaling Days responsibility. Commissioner Scholfield said that the Port rented out the pier and moorage facility for the hydroplane races years ago, maybe Whaling Days should rent it. The rental amount could be equivalent to the estimated moorage revenue the Port has received in the past during the week of the festival. Phil added that the issue isn't control of the pier/floating moorage facility but keeping the festival

Port of Silverdale – Minutes of Regular Meeting on February 21, 2019

attendees from accessing it after hours. Steve Trunkey explained that sailing classes are scheduled through the summer so the sailboat float will need to remain accessible to KSRF. Ms. Larson agreed.

3.3 Night time use of Port parking area –

Edward Gossett was listed on the agenda but was not in attendance. Mr. Gossett called the Port office and requested a copy of the Port's rules. The rules were sent to him via e-mail. In a reply e-mail dated February 15, 2019 Mr. Gossett explained that he likes to park at the boat launch parking area after hours to take pictures, but he has been "hassled" by the Port's security guard informing him that he isn't allowed to be on the property after hours. It was reported that a white male in a vehicle registered to Mr. Gossett had been harassing Port security on multiple occasions by refusing to leave when requested, using profane language and gestures. It was agreed that the Port's rules clearly state that the vehicle parking areas are closed dusk to dawn except for boaters.

3.4. Memorial bench – Ken Brooks explained that he was hoping to sponsor a memorial bench on Port property in honor of his mother who recently passed away. His mother really enjoyed the Port/Waterfront Park area. Ms. Larson explained that the Silverdale Dandy Lions have received permission to install a bench on Port property, maybe they can partner together on a bench. Commissioner Scholfield said that a bench west of the boat ramp would be a good location. Lee Fabricators should be contacted as they construct the frames of Port benches. Mr. Brooks and Ms. Larson agreed to work together on this.

3.5 Non-motorized boat storage – Laurie Aardal explained she was representing the Hawaiian Canoe Club – Hui Heihei Wa'a. The club has been operating along the shores adjacent to the Templeton building at the end

of Byron Street for years. The outrigger canoes are generally left on the beach there from March through October. To stay competitive the club is upgrading from sprint canoes to distance canoes, which are much lighter and more delicate. They have recently purchased a light weight carbon racer, which is valued at approximately \$20,000 and they are looking for a more secure space to store it. The area across the street from the Port office was discussed, but it is not fenced and very visible. Ms. Aardal explained that the club would be willing to clean up an area to make space if necessary. She said that although the club's current location is kind of out-of-sight out-of-mind the club still considers themselves as part of the non-motorized boat community in this area and they hope to be included in the future development of the Old Town area in regards to non-motorized boating. Commissioner Scholfield suggested they use the area on the south side of the 3215 NW Lowell Street building. It isn't fenced, but it also isn't that visible from the road. Ms. Aardal plans to view the area and get back in touch with the Port. She thanked the Commissioners for their time and consideration.

3.6 Pump Station 3 & Port Waterfront Plan Update – Ron Easterday said that it's been about a year since RFM was tasked with updating the Port's comp plan. It's been a long journey with land use concepts, preliminary planning, etc. as well as discussions with the County and the Central Kitsap School District (CKSD). Mr. Easterday provided the Commissioners and staff with draft copies of the comprehensive plan update for review. He said the RFM treated the update as an amendment to the Port's current comp plan as items in the current plan are still pertinent. He asked the Commissioners to review the plan for further discussion at the March meeting. Steve Rice provided the concept use plan, which he explained is basically a map that recaps the comp plan update (CPU) in an easy

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to see and understand format. Mr. Rice explained the three priorities listed in the CPU: Priority #1 - Overwater Projects, Priority #2 - Joint Project with the County, and Priority #3 - Storage/Retail. He detailed the evolution of the three priorities. The future of the Old Town Pub (OTP) and the neighboring properties along Byron Street are listed under Long Term Projects on the CPU. Mr. Rice thanked the Commissioners for taking RFM along this journey; stating that they have learned some things of value along the way. He also thanked the "faithful attendees" that provided input and insight. Marvel Hunt voiced her discontent that the future of the Old Town Pub (OTP) building is still unknown as it is not part of the three priorities, but rather it is listed under long term projects. Mr. Rice reminded everyone that the Port hired Reid Middleton via RFM to provide a structural analysis of the building, which determined it was in very poor condition with a substantial price tag to restore it. At that point it was decided to table the OTP's future until the comp plan update was completed. Ms. Hunt said that the future of the OTP needs to be determined and should be part of Priority #2 either by restoring it or rebuilding it. She urged the Commissioners to make a decision. If it is decided to demo and rebuild she hopes the Commissioners will keep the Old Town feel/façade and maybe even use material from the OTP. Ms. Hunt said that Old Town is the historical part of Silverdale and it needs to be protected somehow. She has talked with many community members that aren't aware of the Port meetings let alone the comp plan update, but these people are passionate about saving Old Town. Randy Hunt said that there really hasn't been much discussion about what the possibilities could be for the OTP. He believes that if it were restored it would be a great draw for the Old Town area. He agreed the decision needs to be made to either save it or not. Phil explained that the CPU is the first step after that is the design phase, which will determine the OTP's future. Ms. Hunt said that she

understands it's not the design phase, but this whole update seemed to originate because the Port purchased the key property component – the OTP and now it's not even addressed in the plan. Mr. Hunt added that he gets the feeling that since the Port isn't making the OTP a priority on the CPU, the Commissioners are just waiting for the community to forget about it so that it can be torn down with little to no reaction from the public. Commissioner Scholfield said that he sees the Pub as a placeholder. He thinks it will need to come down pretty soon as each day it's becoming more of a liability to the Port. He had high hopes for the Pub when the Port first purchased it, but soon realized that the forty plus years of deferred maintenance on the building makes it impractical to save. The Port has already spent over \$30,000 on it in an effort to bring it up to the County's standards only to have it tagged as a dangerous building by the County. Bridget Burke agreed with the Hunts that the CPU is great and was thankful RFM took the time to incorporate the ideas formulated by the Citizens Advisory Committee, but the fact remains that there is an abandoned building sitting along the main thoroughfare of Old Town. She said that another concern is that the Port's Comprehensive Plan from 2007 had lots of good plans, but most didn't come to fruition. Commissioner Scholfield suggested the Port consult with a grant writer so that the process of applying for Recreation Conservation Office (RCO) grants can begin. Phil explained that it will take several grants to complete all of the priorities listed on the concept use plan. John Bouck reiterated that with no breakwater the Port is a unique area and ideal for the nonmotorized boating community. That is why the sailing and rowing programs were drawn to this area as well as the private canoe club and the kayak rental business. The area is already a draw for the nonmotorized boating enthusiasts but with added facilities it will be even more of a draw, which is great for

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economic development. Caleb Reese said that when the design phase begins, he hopes that the shell house and other amenities can be designed similar to the OTP; keeping the Old Town feel.

Phil explained that he has been working with Dave Tucker, Assistant Director of the Kitsap County Public Works Department, who has been working directly with Kitsap County Commissioner Ed Wolfe regarding the sewage pump station 3 project. Commissioner Wolfe requested an interlocal agreement between the County and the Port be generated. Phil provided copies of the interlocal agreement he prepared. It was reviewed. Phil explained that the interlocal agreement is nonbinding but allows both entities to move forward on the project. It does not need to be signed by the Commissioners tonight; it is for their review and if they agree Phil will provide it to Mr. Tucker to present to the County Commissioners. Phil explained that the project will have an interesting legal structure since the Port will actually own the building with the County leasing a portion of it. ***It was agreed to approve the concept of the presented Interlocal Cooperation Agreement between Kitsap County and Port of Silverdale for Silverdale Waterfront Improvement and Pump Station 3 Upgrade*** (motion by Aus, second by Scholfield, unanimous).

Phil and Commissioner Greaves haven't met with RFM personnel to further discuss the Amendment 2 to the original agreement, but since RFM has now provided the actual product, payment seems warranted. The amendment still needs approval and will be added to next month's meeting agenda and a check will be processed for approval at that time.

4. UNFINISHED BUSINESS –

4.1. Port Programs

- a. Sailing – an e-mail dated January 17,

2019 from Julie Jablonski was reviewed. She requested permission for KSRF to use a portion of the floating moorage facility to once again hold a dinner fundraiser. ***It was agreed to allow KSRF to use a portion of the floating moorage facility on July 13, 2019 from 2:00PM to 7:00PM to hold a “Dinner on the Dock and Rendezvous” fundraiser*** (motion by Aus, second by Scholfield, unanimous).

Mr. Trunkey explained that the Bainbridge Island High School Sailing has asked to partner with KSRF to hold a regatta in April in Silverdale. This was done once before and it should be taken as a compliment that other programs request to use the facilities. It also confirms the value of this area to the non-motorized boating community. ***It was agreed to allow KSRF and Bainbridge Island High School Sailing Team to hold the Northwest Interscholastic Sailing Regatta at the Port of Silverdale floating moorage facility on April 27 – 28, 2019*** (motion by Aus, second by Scholfield, unanimous). The far ends of the floating moorage facility will be reserved and moorage for the visiting support boats will be waived. Mr. Trunkey will provide the Port with a list of the support boats' WN#'s to insure the registered owners don't receive delinquent moorage letters from the Port.

Tim plans to install the sailboat float this coming Saturday, weather permitting. Mr. Trunkey asked that the boat launch handling pier be installed prior to the April regatta. Tim will be notified.

- b. Rowing – Ms. Burke asked to hold a regatta in August at the Port facility. ***It was agreed to allow Clam Island Rowing to hold the Bill Richards Rowing Regatta on August 17, 2019 at the Port of Silverdale*** (motion by Aus, second by Scholfield, unanimous).

Ms. Aardal explained that the Hawaiian Canoe Club will be hosting a regatta on June 1, 2019. Although it will not actually be held from Port property, they will need to use space at the

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Port's vehicle/boat trailer lot for the trailers. Commissioner Scholfield requested Ms. Aardal keep the Port updated on what they might need.

A drone photographer is scheduled to be taking pictures of CIR in action on March 2nd. Ms. Burke questioned if that requires Port permission. Commissioner Scholfield said there was no need to get permission because once it's off the dock the Port has no control.

Ms. Burke reported that CIR has acquired space at the Kitsap Mall to house the ergs, which are used when participants are unable to safely be out on the water due to the weather. CIR now has two juniors' coaches and two adult coaches. They have also acquired a new motor, which will eventually be donated to the Port. Ms. Burke would like to first have the new relationship between the Port and CIR in place. She reinstated CIR's insurance as it was coming due. The first day of varsity rowing is this coming Monday with the novice juniors' team beginning on Tuesday. Adult rowing will begin in March.

Commissioner Scholfield was informed by the Port's landlord, Carrie Simons, that the fence on the Byron Street side of rowing alley is going to have to be pushed back as the County claims it is encroaching on County property. It is apparently not an emergent request and the details have yet to be determined, but Commissioner Scholfield wanted Ms. Burke to be aware. The County is planning to work on several sidewalks in Old Town this summer so that may have prompted it. Ms. Burke asked that if the streets are torn up this summer, to the point where CIR participants are having trouble getting the shells to the water, would the Port allow them to temporarily fence off a couple of the vehicle/boat trailer stalls to keep shells there. It was agreed that would be possible although CIR would need to provide the fencing. Commissioner Scholfield said that the contractor that is awarded the County's

sidewalk job might provide CIR with fencing. Ms. Burke took note.

c. Agreement – Phil reported that a meeting with Joe Davis of Enduris was held. Mr. Davis suggested the agreement KSRF and CIR entered into with the Port be cancelled and the Port begin running the program with the help of volunteers. Commissioner Scholfield said that it would be much simpler and cost effective for the programs to continue to be run by KSRF and CIR. He added that one example of additional costs related to the programs will be background checks because a non-profit can obtain free background checks whereas a governmental entity cannot. The Central Kitsap Fire Department pays \$200 for each background check conducted. Mr. Trunkey suggested that instead of trying to rush the transition for this season maybe the goal should be for the transition to be complete prior to the 2020 season. KSRF and CIR will continue to run the programs as they have up until this point. One reason for his suggestion is because the majority of the on-line registration/payment methods conduct a weekly sweep of the funds, which would be deposited into the Port's account just once a week. The RCW requires that funds be deposited into accounts within 24-hours unless the district has written approval from the County Treasurer to do otherwise. A request was sent to Kitsap County Treasurer Meredith Green asking for a waiver of the 24-hour deposit requirement and explaining why. Ms. Green responded via an e-mail dated February 19, 2019 with several recommendations and questions. She did state that if the Commissioners decide to proceed, a resolution from the Port with the answers to her questions and a request to waive the requirement would be required. This doesn't insure that the 24-hour deposit would be waived, but it seems possible. In any case it's going to take time and since the season has already begun with the transition not complete it was agreed that

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holding off on it at this time was necessary. The Port has already received and deposited several sailing registration payments. Commissioner Scholfield said that refunds will need to be made to each participant that paid. Phil said that he will draw up an agreement for payment to KSRF next month. It will state that the refund insures all of those who have paid are still registered. Mr. Trunkey agreed. *It was agreed to rescind the agreements that were entered into on February 21, 2019 between the Port of Silverdale and Kitsap Sailing and Rowing Foundation and Clam Island Rowing for Administration and Management of the Port's Sailing and Rowing Programs* (motion by Aus, second by Scholfield, unanimous). Mr. Trunkey thanked the Commissioners for the willingness to take over the programs and he hopes it will happen in 2020.

Commissioner Scholfield reported that there is a big round buoy at the end of the floating moorage facility. It is unknown from where it came. There are also big chunks of fiberglass down by Pacific Avenue.

d. Non-motorized boat storage – nothing to report.

4.2. Port Facilities

a. Dredging – Phil reported that the analysis results should be received soon.

b. Port Rules/No Wake Buoys – Commissioner Scholfield reported that before Tim can proceed with the installation of the “no wake” buoys he needs to know the Port’s outer water boundaries. Phil said that he has that information and will provide it to Tim.

c. Department of Natural Resources (DNR) expansion of the Port’s outer water boundaries - nothing to report.

d. Gangway redesign – prior to the meeting Tim provided an estimate for the

redesign and met with Commissioners Aus and Scholfield individually at the gangway to discuss the project. *It was agreed to accept TIKAR Services Estimate #2019-1052 in the amount of \$4,850.50 for the Gangway Redesign Project* (motion by Aus, second by Scholfield, unanimous).

e. Sailboat (WN6823JF) continues to be chained to the Port's facility. Phil said he will have to research the law regarding derelicts. Commissioner Scholfield said that he plans to call the State to inform them that the Port will begin processing the boat as a derelict. He was approached by the Sea Scouts, whose boat was moored at the Port, as they were interested in the vessel. Commissioner Scholfield told them that it has to go through a process and since the vessel seems to be in decent shape it may end up being put up for bid, where the highest bidder would then own it. Ms. Hunt thought she had seen the boat anchored out a couple times within this last month, but questioned that since it is chained to the dock. It seems she was mistaken.

The whereabouts of an RV that apparently had been parked at the Port vehicle/boat trailer lot for over a month was questioned. Mr. Reese said that it has moved to the old QFC parking lot along Kitsap Way in Bremerton and has several flat tires.

f. Website – Commissioner Scholfield has been working on the new website: <http://portofsilverdale.squarespace.com>. He explained that he hasn't been able to get the video to work on the new site yet, but will continue to work on that. He was thankful to find out about the software he used to create the new site at the Washington Public Ports

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Association (WPPA) seminar last October. It is much easier than what was previously used, no coding required.

4.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – Mike reported that the new flooring is complete. There was nothing to report regard the sewage connection.

b. 9004 Washington Avenue/Dispute Resolution Center (DRC) – the tenants complain of an ongoing ant problem. Mike has determined that the ants are sugar ants and more than likely attracted to food either left out or in the trash. The DRC's lease was recently reviewed and determined that this issue is actually the tenant's responsibility. DRC was informed.

c. 3215 Lowell, Suite 231 – the unit remains vacant. Commissioner Scholfield explained that the two upstairs offices in the building are hard to rent partially believed to be due to their small size and because they are on the upper level, which is only accessible via a flight of stairs. In the past, Suite 231 has been vacant for months at a time. The upstairs closet rents for \$15 a month and is used for storage. KSRF members have asked Commissioner Scholfield about the possibility of storing boxes at the Port. He suggested KSRF be offered to rent Suite 231 for \$30 a month until it can be rented as an office.

d. 3332 NW Lowell Street/Studs & Nails – Mike reported that the basement flooded because the sump pump failed. He purchased and installed a new sump pump, which fixed the problem. He plans to purchase an extra sump pump to have on hand.

e. 3473 NW Byron Street/Vacant – Ordering a cost estimate report for the building continues to be tabled. Commissioner Aus said that the Commissioners do need to decide what is going to be done with the building. The Commissioners agreed that if it is decided to tear it down several pictures should be taken of it and anything of nostalgic value, such as the OTP sign that hangs on the outside of the building, should be kept/stored so that such items can be used if/when a new structure is built. Even some of the structural lumber may be able to be stored for future use. Ms. Hunt explained that she saw several Central Kitsap Fire Department trucks and personnel down at the vehicle/boat trailer lot today and her first thought was that the pub must have caught fire. Apparently CKFD was in the lot for training and the OTP is still standing. Commissioner Scholfield suggested a salvage company be contacted to provide an estimate.

4.4. Eagle Scout Service Project – nothing to report.

4.5. Paving of alley – Commissioner Scholfield suggested Luethe's Grader Service be contacted and asked to scrape the alley from Washington down past/around the parking area at 3255 Lowell. Then an asphalt company will be contacted to pave it. Phil said that with the new \$40,000 small works threshold it wouldn't require bids, although a small works roster application will need to be on file for whatever company completes is hired to complete the job as there is necessary information on the application to insure the company is issued a 1099 at the end of the year. Mike will contact Luethe's once the weather improves. In the meantime, he will provide Phil with the measurements.

5. NEW BUSINESS

5.1. County – Commissioner Scholfield reported that he has reviewed the County's 90% completion plan of the Bayshore Drive Improvement Project. He noticed that two of the Port's parcels at the south end of Washington Avenue were listed as County property. The County had originally intended to issue an easement for those parcels but after discussions with Port personnel it was agreed the easement was unnecessary and the County would remove that area from the plan. Commissioner Scholfield plans to contact the Project Manager.

5.2. Keys – a set of keys to the Port office, restrooms and showers/laundry was lost. Mike has been working on getting new keys for all the impacted buildings.

5.3. Commissioner District 1 – Commissioner Greaves has indicated that he may not complete his term. Phil explained that in the past when the Port had a vacancy, a questionnaire was given to those interested in filling the position. The questionnaires were then discussed at a Port meeting and a decision was made. Ms. Hunt asked if it is required to publish an announcement in the local newspaper. Phil explained that it would be an appointment and there is no requirement to publish an announcement of the vacancy. Commissioner Aus added that if the Commissioners cannot find an individual to fill the position then it would be advertised. Phil said that the Commissioners will have ninety days from the date of resignation to fill the position. If it is not filled within that time then the County has the authority to fill it. Ms. Hunt said that in order for the public to be made

aware of the vacancy an ad should be placed in the paper. She suggested an article be published thanking Commissioner Greaves for his years of service and ending the article informing the public about the vacancy and how anyone interested can apply. At this point Commissioner Greaves plans are unknown. This will carry over to next month's agenda.

5.4. Tragedy at the pier – the dock security footage showed Hevi Muhaz Tahir, 17, jumping from the far end of the Port's floating moorage facility the afternoon of January 28, 2019 in an apparent suicide. Ms. Muhaz's body was recovered off the shores of South Kitsap day later. Thoughts go out to her friends and family.

6. **SAFETY** – Kitsap County recently had over fifteen inches of snow within a few days. Commissioner Scholfield said that he and others have been working on keeping the snow off of the sidewalks in hopes to prevent any possible injury.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$29,793.20, checks numbering 12005 through 12028 and Electronic Funds Transfer 2019-02 to the U.S. Treasury in the amount of \$1,698.94, were approved (motion by Aus, second by Scholfield, unanimous).

8. **EXECUTIVE SESSION** – None.

9. ADJOURN

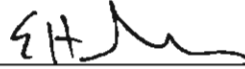
At 9:10PM the meeting adjourned (motion by Aus, second by Scholfield, unanimous).

Approved:

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Henry Aus, Commissioner



Ed Scholfield, Commissioner

Lawrence Greaves, Commissioner

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Port of Silverdale

Minutes of Regular Meeting

March 21, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Lawrence Greaves; Commissioner Henry Aus; Attorney Phil Best; Administrator Theresa Haaland; Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Gus Housen of Whaling Days; Roy Sahali of Sahali Farms; Ron Easterday of Rice Fergus Miller (RFM); Laurie Aardal of the Hawaiian Canoe Club – Hui Heihei Wa'a; Kevin Cavanaugh; Mickey Hall; Marvel Hunt; Caleb Reese; and Tim Knapp arrived at 7:40PM.

1.1. Agenda – *the agenda was approved with the addition of “Transferring Boats” for discussion under 4.1.a* (motion by Greaves, second by Aus, unanimous).

2. CONSENT AGENDA

2.1. *The February 21, 2019 Regular meeting minutes were approved as submitted* (motion by Aus; second by Scholfield, passed. Commissioner Greaves abstained as he was not present at the February meeting).

3. PUBLIC COMMENT –

3.1. Non-motorized boat storage – Laurie Aardal of Hui Heihei Wa'a canoe club explained that the club took possession of the new light weight carbon racer canoe earlier than anticipated and decided to store it on the south side yard at the Port's 3215 Lowell Street property. She thanked the Commissioners for offering the space. It works out well as the canoe is somewhat out of sight yet close to the water. Ms. Aardal explained that it is Hawaiian culture to bless a canoe prior to taking it out on the water. She said she would let the Port know when the blessing is to take place.

3.2. Whaling Days – Carla Larson was not in attendance as anticipated. Gus Housen of

Whaling Days explained that he will probably be the new Whaling Days representative. He has issues he plans to address at a future meeting.

3.3. Port Waterfront Plan Update – Ron Easterday asked if there were any comments about the draft comprehensive plan amendment RFM provided last month. Commissioner Aus explained that he tabbed minor items he would like to see changed/corrected. Commissioner Scholfield said that the economic analysis needs to be updated. Phil said that he also has a couple of comments. He provided a portion of a draft executive summary of the amendment that he is creating. It was reviewed. Phil suggested that instead of calling the actions “priorities” they be called focus areas and action items as funding availability will determine the priority. Phil is creating the executive summary to provide to interested individuals without having to provide them with the entire comprehensive plan since RFM's update is incorporated within the last comprehensive plan. Mr. Easterday said that RFM will provide the plan electronically. *It was agreed to approve the concept of the February 2019 Port of Silverdale Comprehensive Plan Update with minor changes* (motion by Aus, second by Greaves, unanimous).

Phil reported that he and Commissioner Greaves met with Steve Rice on March 6th and discussed the future implementation of the plan. Mr. Rice suggested that the Port hire a project manager or at least hire representation for the design of the new pump station. This will require a new selection process. RFM's Amendment 2 to the contract was signed and a check in the amount of \$42,863.42 is scheduled for authorization at tonight's meeting.

Pump Station 3 – Phil reported that the County is on board with moving forward with the

project. After receiving approval from the board last month to accept the Interlocal Cooperation Agreement between the Port and the County, Phil forwarded it on to the County for approval. The County sent it to their legal team who requested it be changed to a Memorandum of Agreement along with a few other minor changes. Phil said that the details of the project are not necessary at this point as they will be outlined within a future Interlocal Agreement. The Memorandum of Agreement allows both parties to move forward with the project. *It was agreed to enter into the Memorandum of Agreement with the County for Silverdale Waterfront Improvement and Pump Station 3 Upgrade* (motion by Aus; second by Greaves, unanimous). The agreement was signed and will be forwarded to the County for approval and signature.

Strawberry Creek Restoration Planning – an e-mail was received from Paul Schlenger of Environmental Science Associates who is working with the Central Kitsap School District on a project to restore the lower reach of Strawberry Creek. They are having a site walk of Strawberry Creek on March 27th from 1:30PM to 3:00PM and hoped someone from the Port would be available to attend. Commissioner Scholfield said he would plan to attend the site walk.

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – Steve Trunkey questioned when the boat launch handling pier will be installed as the regatta KSRF is hosting is scheduled for the end of April and it's essential to have the handling pier in prior to the event. Commissioner Scholfield said that Tim has it on his calendar, but it all depends on the weather.

Phil provided paperwork transferring boats and equipment to the Port from KSRF. *It was agreed to accept the boats and equipment listed on*

the “Transfer of Boats and Equipment to Port of Silverdale by Kitsap Sailing and Rowing Foundation” dated March 21, 2019 (motion by Aus; second by Greaves; unanimous). The paperwork was signed.

Mr. Trunkey said that KSRF is planning an informal regatta tomorrow because the main event, which is held in Oak Harbor, had a one-boat restriction. It was thought holding the informal regatta would be encouraging to the sailors that couldn't participate in the Oak Harbor event.

b. Rowing – nothing to report.

c. Rent of equipment and Leasehold Excise tax – the 2018 tax for KSRF and CIR's use of Port equipment needs to be paid. It equates to approximately \$500. When it was last paid in 2017, it was agreed to use funds from the non-motorized boat storage account. Both KSRF and CIR did pay the fees for 2018 although a new agreement was not signed because it was thought the Port would be taking over the programs. Commissioner Scholfield agreed that money from the non-motorized account be used again for the 2018 taxes that are due. It was questioned if KSRF and Clam Island Rowing (CIR) should be paying the equipment rental fees for 2019. Phil provided paperwork to rescind the agreement that was entered into on January 17, 2019. He explained that it not only rescinds that agreement but also restores the pre-existing operation agreement. Mr. Trunkey said that they aren't renting the equipment from the Port this year. KSRF and CIR are just running the programs for the Port and so they don't plan to enter into a new lease agreement and therefore do not have to pay the leasehold excise tax for 2019.

d. Agreement - the paperwork to rescind the January 17, 2019 agreement and restore the pre-existing operating agreement was

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signed. The Port had obtained funds from sailing participants for registration fees in the amount of \$2,275.00. A check payable to KSRF in the amount of \$2,275.00 is scheduled to be authorized at tonight's.

e. Non-motorized boat storage – nothing to report.

4.2. Port Facilities

a. Dredging – Phil reported that Marine Surveys and Assessments (MSA) has incurred much more costs than originally anticipated mainly because the disposal management program changed significantly from the time the project first began. MSA was forced to contract out to a third party to perform a second analysis of the material. The company hired was Pyron and the additional costs totaled \$2700. MSA plans to pay Pyron directly and bill the Port. A check in the amount of \$3,526.00 to MSA is scheduled to be authorized at tonight's meeting. *It was agreed to pay MSA \$3,526.00 and reimburse them for the costs of the newly required third party analysis* (motion by Greaves; second by Aus; unanimous).

b. Port Rules/No Wake Buoys – Phil plans to provide Tim with the current water boundary map. The buoys will not be permanent so that if/when the Port's outer water boundaries are expanded, they can be moved.

c. Department of Natural Resources (DNR) expansion of the Port's outer water boundaries – Phil reported that he has a call into a DNR representative.

d. Gangway redesign – last month TIKAR Services estimate #2019-1052 was accepted. The status of the redesign is unknown at this

point and Tim was not yet present for further discussion.

e. Sailboat (WN6823JF) continues to be chained to the Port's facility. Commissioner Scholfield thought the next step was for the Port to take possession of the vessel. Phil said that it's an unfortunate situation because the vessel was the owner's home prior to going to jail. Commissioner Scholfield said that there was a time when the owner was released from jail and he didn't come to retrieve it at that time. Marvel Hunt said she saw the white sailboat that anchors out in Dyes Inlet tied up to the chained boat at the Port facility. Commissioner Scholfield said that there is a group of individuals that live on their sailboats in and around Kitsap waters. He has talked to one of the individuals and informed him that if a vessel is moored at the Port for more than thirty consecutive days the Port must also collect the State Leasehold Excise Tax of 12.84%. One of the concerns about this group is that it is unknown where they dump sewage. The Department of Ecology has been notified of the concern. According to Commissioner Scholfield the DNR has asked the Port to post a notice on the vessel that anchors out in Dyes Inlet, if it ties up to the Port's facility. It was agreed that the Port has no control over vessels anchored out beyond the Port's outer water boundaries. Commissioner Scholfield said that unless the Port wants to keep the chained sailboat all summer, now is the time to start processing it as a derelict. Phil explained that there is a difference between a derelict vessel and one with delinquent moorage fees. Commissioner Scholfield disagreed as it was an abandoned boat that was tied to the Port's facility. Commissioner Scholfield said that

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notice needs to be mailed to the last known address and published in the newspaper. Phil plans to review the law.

f. Website – Commissioner Scholfield has been working on the new website: <http://portofsilverdale.squarespace.com>. He asked that everyone take a look at it and provide input prior to going live with it.

4.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – There was nothing to report regarding the sewage connection.

b. 3332 Lowell Street/Studs-n-Nails - Mike has repaired the deck.

c. 3215 Lowell, Suite 231 – Remains vacant. Terms of the lease and other details of the unit will be provided to Commissioner Scholfield.

Commissioner Scholfield explained that Silverdale Water reported a leak at the property. He determined that there is a leak in the service line. Silverdale Water is aware of this.

d. 3473 NW Byron Street/Vacant – Ordering a cost estimate report for the building continues to be tabled. Nothing to report regarding salvaging material from the building.

4.4. Eagle Scout Service Project – An e-mail from John West was received this afternoon. He explained that he is working on the project's paperwork that he will submit to the Boy Scouts of America for approval. Tim requested a copy of the e-mail so contact can be made with Mr. West.

4.5. Paving of alley – Mike plans to contact Luethe's Grader Service once the weather improves.

4.6. Commissioner District 1 – Commissioner Greaves explained that he would like to resign from his Port position. He has been recruiting his replacement, Mickey Hall. Mr. Hall was asked his thoughts about serving as a Port Commissioner. He said that he has talked with Phil and Commissioner Scholfield to determine the responsibilities of the position. He explained that as a favor to Commissioner Greaves he would fulfill his term. District 1 is scheduled to be on the November ballot. Phil explained that at last month's meeting Ms. Hunt had suggested notice be placed in the paper thanking Commissioner Greaves for his time served announcing the vacancy and inviting the public to apply. Ms. Hunt asked Commissioner Greaves when he plans to make it official. He replied that he and his wife, Lavonne, plan to travel a lot this summer; so, the sooner the better. Phil explained that the Port has ninety days from receipt of an official resignation letter to fill the vacancy. If not filled within that time then it becomes the County's responsibility. It was explained that the Port of Tracyton had success in filling a vacancy by submitting a letter to the editor in the Kitsap Sun. Tim suggested it be advertised with a deadline of maybe a week and all applications be reviewed and discussed at a regular meeting. Commissioner Scholfield agreed and suggested the deadline of April 11th. One requirement is that the applicant lives within District 1, which can be verified via the County website. Phil suggested Commissioner Greaves submit his resignation on April 17th. The regularly scheduled Port meeting is set for

Port of Silverdale – Minutes of Regular Meeting on March 21, 2019

April 18th. Serving since 2000, Commissioner Greaves will be greatly missed.

5. NEW BUSINESS

5.1. Kitsap County Department of Public Works Correspondence dated February 25, 2019 was reviewed. The County has hired Zipper Geo Associates, LLC to conduct structural assessments of buildings within 500 feet of the work area prior to construction of the Bay Shore Drive, Washington Avenue and Byron Street Improvement Projects. Phil explained this is the County's attempt at protecting itself from possible future claims of damage that may have already existed prior to project construction. They had requested a point-of-contact to gain access to the buildings, but since they are rented to several different businesses, Phil suggested a point-of-contact for each business be provided to the County. Port tenant contact information will be sent to the County and e-mail notification will be provided to the tenants prior to contact by the County. The Commissioners agreed.

5.2. Silverdale Chamber of Commerce – Advertising in the walking map is up for renewal. *It was agreed to place an 1/8 page ad in the 2019 Silverdale of Commerce walking map* (motion by Aus; second by Greaves; unanimous).

5.3. Discover Kitsap Peninsula magazine is created by Sound Publishing. A Sound Publishing representative came into the Port office asking if the Port was interested in advertising in the magazine. It is distributed on the Washington State ferries. Although the Port hasn't ever advertised in the publication the picture on the Summer 2018 edition cover

page is of the Port's waterfront and a portion of the pier.

Ms. Aardal said that the distribution on the ferries isn't very targeted. She felt it was money better spent to advertise in the local walking map. The Commissioners agreed and decided against advertising in the Discover Kitsap magazine.

Phil reported that an individual came into the Port office recently asking for the map of the Clear Creek Trail (CCT). Unfortunately, there are none in the office. It was suggested a request for maps be placed to Mary Earl who is heavily involved with the CCT.

5.4. Gift Certificate – An e-mail dated February 26, 2019 from the Pacific NW Nordic Tug Owners Association was reviewed. The Association was asking for a two-night moorage gift certificate in exchange for adding Port brochures to their goodie bag, which they plan to hand out to tug owners who are participating in a rendezvous in Anacortes, WA the end of May. *It was agreed to send the NW Nordic Tug Owners Association one two-night free moorage gift certificate* (motion by Aus; second by Greaves; unanimous).

5.5. Complaint – On March 18, 2019 Lester Burke, who is a local boater, called the Port office and made a complaint. He asked why the boat launch handling pier wasn't yet installed and insisted that since the weather has been so nice it is boating season. Mr. Burk was told that boating season doesn't start officially until May and the floats are normally placed back in the water sometime in April. He also didn't think it was fair that the full moorage

Port of Silverdale – Minutes of Regular Meeting on March 21, 2019

rate is charged when the water isn't available at the floating moorage facility.

5.6. Colonial Life – Katey Rudisill is an agency sales manager for Colonial Life, who recently secured office space at the neighboring building to the Port office. She dropped off information about Colonial Life's services and said she would be more than happy to talk to anyone who may be interested.

5.7. Public Disclosure Commission – Commissioners were reminded to file their Personal Financial Affairs (F1) online at <https://www.pdc.wa.gov/learn/file-online> by April 15th to avoid penalties.

5.8. Owed to the Port – Oz Thoresen paid his debt to the Port in full. He apologized for how long it took. Interest was accrued on the debt up until the day Mr. Thoresen paid - kudos to him for being diligent.

6. SAFETY – Nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$75,038.05, checks numbering 12029 through 12055 and Electronic Funds Transfer 2019-03 to the U.S. Treasury in the amount of \$1,959.82, were approved (motion by Greaves, second by Aus, unanimous).

8. EXECUTIVE SESSION – At 8:09PM it was announced the meeting would be going into Executive Session for approximately ten minutes to discuss potential litigation.

At 8:17PM the meeting returned to Regular Session. Tim was the only person waiting outside the door and returned to the meeting.

Phil asked Tim about the possibility of adding a washdown station as new construction within the new waterfront plan, as there could be grant money available for it. Commissioner Scholfield said that stormwater isn't allowed to go into the sewer system so that would be an issue. Tim agreed. Commissioner Aus said that the Port of Kingston offers a washdown station and questioned why Silverdale couldn't. It was thought that the Port of Kingston's washdown station may have been grandfathered in from years ago. Commissioner Aus said that this should be further investigated to be sure of the regulations before ruling out the possibility. Phil agreed and said that the Port may be allowed with some restrictions. Tim added that with the emphasis on improving the Puget Sound waters there may be money available for such a project. Commissioner Scholfield reminded everyone that there is a sewer connection at the Old Town Pub. Tim suggested that if the Port were to build a washdown station the costs to operate it be significantly higher than a standard car wash; otherwise, that's what it will become. Phil said that at that point the Port may want to begin charging for boat launching and parking too and install a Ventek machine.

Commissioner Aus said that water hook ups for the planned new construction need to be determined now as the County will be paving the roads. He suggested RFM be asked to determine the water needs for the future development. Phil cautioned the Commissioners on continuing the relationship with RFM beyond the comprehensive plan update because the next phase will be a separate contract with no guarantee that RFM

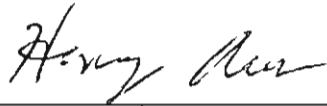
Port of Silverdale – Minutes of Regular Meeting on March 21, 2019

will be the company designing it. Tim suggested Morgan Johnson the General Manager of the Silverdale Water District be contacted to further discuss this as he has the knowledge and the ability to move ahead with implementation and it won't cost the Port to consult with him. The Commissioners agreed that was a good idea and Commissioner Aus will contact Mr. Johnson.

9. ADJOURN


At 8:40PM the meeting adjourned (motion by Greaves, second by Aus, unanimous).

Approved:



Henry Aus, Commissioner

Lawrence Greaves, Commissioner



Ed Scholfield, Commissioner

Port of Silverdale – Minutes of Regular Meeting on August 15, 2019

Port of Silverdale
Minutes of Regular Meeting
August 15, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:01 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Marvel Hunt; Carla Larson; Jack West; and Tim Knapp of TIKAR Services arrived at 7:10PM.

Agenda – *the agenda was approved as submitted* (motion by Aus, second by Reese, unanimous).

2. CONSENT AGENDA

2.1. *The July 18, 2019 Regular meeting minutes were approved as submitted* (motion by Reese; second by Aus, unanimous).

3. PUBLIC COMMENT –

3.1. Eagle Scout Project – Jack West submitted his Life Jacket Loaner Station Eagle Scout Project Proposal. He explained that he has received all of the required signatures except for the Port. *Commissioner Aus moved to accept the proposal as presented; second by Commissioner Reese.* Discussion: the financial plan within the proposal was discussed. Mr. West explained that he failed to update that portion of the plan as it states \$55 for lumber, but Tim Knapp has generously offered to donate the Chemonite lumber for the project. *The motion carried unanimously.*

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – nothing to report.

b. Rowing – an e-mail dated August 5, 2019 from Bridget Burke of Clam Island Rowing (CIR) was reviewed. She was hoping the Old Town Pub's (OTP) outdoor kitchen would be removed by the end of August so that

they can smoothly transition from the alley on the side of the Port office building to that area behind the OTP before the weather turns. Tim explained that August is the fish window, so he is busy with in-water work. He plans to remove the outdoor kitchen sometime between September 1st and 15th. Once that is complete Mike will work on installing a couple of electrical outlets.

4.2. Port Facilities

a. No Wake Buoy Project – Tim reported that he is waiting for a shipment of foam, which is taking longer than expected. Reflective decals and orange tape will be on the buoys.

b. Department of Natural Resources (DNR) – Phil reported that he is working with the surveyor to get the legal description of the area.

c. Sailboat (WN6823JF) – Phil reported that the owner of the boat has until August 28th to appeal. After that the Port can go through the process of selling it. Commissioner Scholfield added that the parameters of the sale need to be set; such as, upon purchase it is required to be removed from the water or purchaser must have readily available moorage for the boat.

d. Benches – Phil provided a Request for Quotations (RFQ) for the benches. Tim provided the specs which were attached to the RFQ. Phil said that he would use the Small Works Roster (SWR) to reach out to companies that might be interested in bidding the job. If there are not many companies that specialize in such work, Phil will perform some research and invite other companies to complete a SWR application.

e. Port Rules and Regulations Policy – Phil has reviewed the Port of Brownsville's

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rules and found them to be interesting. Commissioner Scholfield said that the policy should include some sort of fee schedule listing other Port charges, such as the \$75 Impound Fee and \$25 Collection Fee for unpaid moorage. Phil will continue to work on it.

4.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – Mike reported that all of the insulation has been removed, but the ant problem continues. The tenant requested that the construction of the ramp wait until the buildings are painted.

b. 9004 & 9020 Washington Avenue/DRC & Elizabeth's House of Wax – Phil reported that he has an RFQ ready, he is just waiting for the paint details which will be attached to the RFQ. Mike said he has a sample for Elizabeth's House of Wax, but the DRC is still deciding on the color.

c. 3330 Lowell/Sugar Studios – at last month's meeting there was much discussion about the gravel that was placed along the right of way leading into the 3330 Lowell Street property, which created easy access to the back of that property. The Port was contacted by the County and informed that they didn't see an issue with the gravel creating an accessway along the right-of-way, but they did request the Port apply for a permit. Phil agreed that having the permit in place was a good idea. He has been working on applying for it. It is now all an on-line application. Phil will continue with the application process.

d. 3473 NW Byron Street/Vacant – the OTP sign was removed by Mike of Action Maintenance Services last month but he didn't

remove the bracket. When Mike of AMS is back in the area he will be asked to remove the bracket since he has the proper equipment to do so.

4.4. Release of Easement – Phil provided two documents: An Order of Vacation, that is already in place, and a draft resolution requesting vacation. *It was agreed to adopt Resolution 2019-01 Requesting Alley Vacation in Block 9 of Silverdale Plat* (motion by Aus; second by Reese; unanimous). Phil will prepare a letter to the County which will include a copy of the Port's resolution.

4.5. Paving of alley – Phil reported that he e-mailed the RFQ to six companies listed on the SWR (Agate Asphalt; Apply-A-line, Inc.; Caseco Associates, Inc.; Granite Construction Company; Lakeside Industries, LLC; and Northern Asphalt, LLC). It was determined Northern Asphalt and Agate Asphalt, the companies that supplied the original bids to Mike, were not on the SWR. Applications were e-mailed to both companies and they have since completed and returned them. Lakeside Industries is the only company that submitted a quote. It came in at \$50,334.15. After the deadline, all of the other companies that did not submit a quote were contacted to ask why they didn't bid. For the most part they just missed the deadline, but said they would be interested if there was an extension. Phil suggested to rescind the RFQ at this time. Mike suggested this be held off until mid-October as all of these companies are extremely busy this time of year. Tim suggested a pre-bid meeting be scheduled and reminders be sent to all of the companies beforehand.

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4.6. Olympic Outdoor Center – Phil plans to write to Mr. Kuntz asking him to remove the 2x4, to be sure that the flags are not a hinderance to the public and to use proper fasteners for the flag poles. The structure on the pier during the off-season will also be discussed.

4.7. Comp Plan – Phil reported that Rice Fergus Miller (RFM) personnel dropped off the draft plan just hours ago. Steve Rice is on the mend from a bicycling accident he had within the month; otherwise he'd be here tonight and Ron Easterday is on leave. Phil pointed out the Executive Summary on page three of the draft plan and said that it was a good idea to have that added. He suggested the Commissioners take their copy of the draft plan home and review it and individually contact him with their thoughts. After that it will need to go to the Port's grant writer, Kathleen Byrne-Barrantes, to make sure it's adequate for possible Recreation Conservation Office (RCO) funding and Phil suggested one more meeting be held with the original Citizen's Advisory Committee before final adoption. RFM is charging the Port \$6,500 for this final draft plan. Commissioner Scholfield said that he thought this was the product the Port would receive from the original amount paid. He said that paying \$6,000 after all of the edits from the grant writer and the committee might be somewhat reasonable, but shouldn't be paid until it is complete and the final product is in hand.

4.8. Grant funding – Phil reported that he hasn't yet contacted the Port's grant writer, Kathleen Byrne-Barrantes, but plans to do so soon. He received a call from Dave Tucker of the Kitsap County Public Works Department.

Mr. Tucker asked where the Port is thinking the pump station should sit. Phil thought as far north as possible and the Commissioners seemed to agree. Mr. Tucker told Phil that he had spoken to the RCO and they didn't seem to have any issues with it being moved. Commissioner Scholfield said that the RCO might have an issue if any of the trees or lights are disturbed.

4.9. Whaling Days – An e-mail dated August 2, 2019 from Holly Nuckols was reviewed. Ms. Nuckols and her husband had moored their vessel at the Port during Whaling Days. She complained that the Safe Security guards harassed a fellow boater (Lester Burk) by asking him to stern tie his boat, although Mr. Burk was trying to save room for friends that were coming in via boat. There apparently were other boats side tied to the finger piers that were not asked to reposition their boats. According to Ms. Nuckols, the security guard threatened to impound Mr. Burk's boat if he didn't stern tie it. She also complained that boaters were not notified that the docks were going to be closed to foot traffic for the fireworks. Mr. Burk came into the Port office a couple weeks ago to pay his bill. He also had a complaint about being singled out as he was asked to reposition his boat while others seemed to be overlooked. Another complaint Mr. Burk had was that the foot traffic to the boats was closed off to non-boaters prior to 10PM each night. Mr. Burk also said that it didn't seem fair that OOC could have paddle board renters in the water but boaters were not allowed to go in the water. Tim said that according to the State, kayaks and paddle boards are considered vessels which warrants them being in the moorage area water;

Port of Silverdale – Minutes of Regular Meeting on August 15, 2019

whereas, the floaties and innertubes the boaters like to use are not considered vessels and it's just much too dangerous. Carla Larson reported that a large boat on the last finger pier refused to stern tie, which would have freed up a lot of dock space for boaters. The scouts did another good job on cleaning up Port property during the festival. *It was agreed to pay Boy Scout Troop 1540 \$800 for their cleanup efforts on Port property during the 2019 Whaling Days Festival* (motion by Reese; second by Aus; unanimous).

Commissioner Scholfield reported that oil from the Paradise Entertainment rides was left in the Port's parking lot. They did come back and take care of it when asked. Marvel Hunt said that she noticed there weren't nearly as many cars as there had been in years past. Ms. Larson said that the income was up but she agreed there were less attendees. On Friday night she noticed some road closure barricades were not in place, so she took care of that.

5. NEW BUSINESS

5.1. Sea Plane crash into Dyes Inlet – on August 6th a sea plane owned and operated by John F. Gotschall crashed into Dyes Inlet. Thankfully, no one was hurt. Apparently, the tires on the plane were locked in the down position when he tried to make a water landing. Commissioner Scholfield said that the Central Kitsap Fire Department handed the cleanup off to the Port. Several State agencies contacted the Port to ensure it was being taken care of and pollution was at a minimum. The Port incurred \$2,359.34 in costs to clean up and dispose of the debris. A letter has been generated dated August 14, 2019 addressed to Mr. Gotschall informing him of the amount due

to the Port. Commissioner Reese said that the amount owed is minimal. He heard of a boat that recently sunk at the Port of Bremerton and the cleanup costs totaled \$7700.

5.2. Upcoming State Audit – an e-mail dated July 31, 2019 from Melinda Seibert of the Washington State Auditor's Office was reviewed. It was sent to inform the Port that they are putting together the audit schedule for this Fall. They will be contacting the Port in the near future with details of the date and time.

5.3. Westsound Networking Group has requested that Commissioner Reese attend their regular meeting on August 28th and provide a brief presentation about the Port. Commissioner Reese plans to use the draft concept use plan that RFM had generated.

5.4. Enduris – an e-mail dated August 14, 2019 from Joe Davis of Enduris was reviewed. They are considering dropping the Marina Operators Liability insurance for the Port since the Port doesn't provide any services where possession of boats is necessary. Enduris will continue to cover the general liability exposures of the marina operations.

5.5. Benefits – Commissioner Scholfield explained that Airlift Northwest offers an Aircare membership that cost approximately \$80 a year per member and covers members and their family members that are under the same medical coverage. Commissioner Scholfield said that a broken leg in Kitsap County can result in an airlift to Harborview Medical Center in Seattle and cost upward of \$20,000 out of pocket. He suggested the Port offer this as a benefit to both Port staff and Commissioners. It was agreed that this may be

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something the Port can offer. A call will be placed to determine if there is a government rate. Retirement benefits for Port staff were also discussed. Since the Port is a municipal corporation, employees would qualify for the State Public Employees Retirement System (PERS). Tim said that employees must work at least eighty hours within a month. It was agreed this should be further looked into and both items will continue onto next month's agenda.

5.6. Safe Security – an e-mail dated August 1, 2019 from Christine Scott of Safe Security was reviewed. Ms. Scott explained that she is considering offering a day-time patrol, which would include three site checks each day and a response as quickly as possible to any request for support with a specific issue.

Commissioner Scholfield suggested looking into the costs of having an on-site security guard that patrols just the Old Town area. He thinks several of Old Town businesses may be interested in supporting this and if everyone pitched in it would cut down on the costs. Ms. Scott will be asked to provide a cost estimate for the on-call guard as outlined in her e-mail and also a cost estimate for a guard to patrol Old Town similar to the Port Host position, but during the week.

6. SAFETY – nothing to report.

Commissioner Scholfield explained that the power at the far finger pier needs to be repaired. At the last All Ports meeting the following electricians were highly recommended; Bronco Electric; Bainbridge Island Electric; and George's Electric. Tim said that he had a recommendation and would provide that information to the Port. Small

Works Roster applications will be sent to those businesses. Commissioner Scholfield said that it's a maintenance repair and should cost no more than \$10,000 so an RFQ would not be necessary.

Tim provided the special keys for the water faucet near the boat launch restrooms. A key will be provided to Steve Turnkey of Kitsap Sailing and Rowing Foundation (KSRF) and also to Bridget Burke of Clam Island Rowing (CIR).

Ms. Hunt asked why the Port is no longer working with the Central Kitsap School District (CKSD) in regards to the future of Old Town. She said that the Port representatives did attend meetings with CKSD when RFM was first hired to complete the comp plan, but since that time there is no longer Port representation at the CKSD meetings and she feels it could possibly be a missed opportunity. Phil explained that when RFM was first hired the request was for them to create a Waterfront Comprehensive Plan. RFM was already working with the CKSD and they had ideas to combine the efforts, which was beyond the scope of what the Port had hired them to do. Phil said that he has sat in on a few of the meetings regarding the future of Strawberry Creek and that is a project the Port can be part of and support. Ms. Hunt understood the Port's stance, but said that she feels it's so important for the Port and CKSD to keep the lines of communication open, keep in touch and work together wherever possible. Commissioner Scholfield said that the Port was basically trying to stop the bleeding with RFM as there were costs for every meeting. Ms. Hunt understood, but urged the

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Commissioners to stay in contact with Doug Newell, CKSD's Executive Director.

Ms. Hunt asked what the Port's plans are in replacing Phil as legal counsel as she has heard that he plans to retire at the end of the year. Phil said that he has contacted another local attorney who is interested in taking over his position with the Port. Commissioner Reese said that someone would need to be on retainer. Tim said that there are a handful of local attorneys with understanding in municipal law. Ms. Hunt said that four months goes by fast and this is something the Commissioners need to be discussing.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$37,306.26, checks numbering 12158 through 12185 and Electronic Funds Transfer 2019-08 to the U.S. Treasury in the amount of \$2,018.52, were approved (motion by Aus; second by Reese; unanimous).

8. EXECUTIVE SESSION – None

Phil reported that attorneys from both sides of the Bayer claim are planning to come over after hours to view the site where the accident took place.

Tim reported that he is currently working on the dredge under the boat ramp during the August fish window.

Phil said that the Army Corps of Engineers contacted him to let him know they had received the Joint Aquatics Resource Permit Application (JARPA).

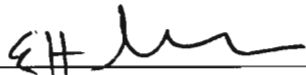
9. ADJOURN

At 8:42PM the meeting adjourned (motion by Reese; second by Aus; unanimous).


Approved:



Commissioner



Commissioner



Commissioner

Port of Silverdale

Minutes of Regular Meeting
September 19, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Greg Jacobs of Kitsap Sailing and Rowing Foundation (KSRF); Bridget Burke of Clam Island Rowing (CIR); Mike Vasquez of MTV Home Repair; Kathleen Byrne-Barrantes of Grant Solutions; Jason Mimms of Safe Security; Kitsap County Public Works personnel: David Tucker, Stella Vakarcz and Barbara Zaroff; Marvel Hunt; Carla Larson; Roy Sahali; and Tim Knapp of TIKAR Services arrived at 7:40PM.

Agenda – *the agenda was approved as submitted* (motion by Aus, second by Reese, unanimous).

2. CONSENT AGENDA

2.1. *The August 15, 2019 Regular meeting minutes were approved as submitted* (motion by Reese; second by Aus, unanimous).

3. PUBLIC COMMENT –

3.1. Pump Station 3 – Dave Tucker, Assistant Director for Kitsap County Public Works, explained that he and Phil have been discussing a better placement of the building. A County-prepared preliminary concept drawing was reviewed. The area must be crane accessible and the building which houses all of the electrical equipment, valves, backup generator, etc. must face the wet well for safety reasons allowing employees to easily communicate when working on the system. Odor control was discussed. Mr. Tucker said that today's technology is so much better than when the current system was constructed. He likened the current odor control system to a car that was made in the 1970's compared to a car

made today; really not much to compare. Stella Vakarcz added that they are now adding chemicals upstream, which seems to be helping with odor issues as they haven't received many complaints since they began this new method. Phil said that at Skansie Park in Gig Harbor they have a stack that has a winding vane that creates a vortex and pushes the exhaust up and away. Mr. Tucker explained that pump station 4 along Bucklin Hill and pump station 19 at Nels Nelson are being upgraded and they take priority for construction timing over pump station 3 just because of how everything flows. He said that the County needs to determine if they are going to join the Port with the construction or will they just be a tenant of the Port. Greg Jacobs said that the limiting factor of the building will be the roof height. Phil reminded everyone that within the new comp plan the concept was to have a viewing platform on the top of the building, similar to Skansie Park with pump station 3 and a shell house opposite of that on the ground floor. Mr. Tucker said that as long as it functionally works for the County there should be no issues. Phil asked what the County plans for the property that the current pump station 3 sits upon. Mr. Tucker said that they have been discussing possibly keeping the old wet well, which is underground, to be used as an overflow during emergencies, but that is not for certain. Barbara Zaroff said that there have been no definite decisions regarding the future of that property, although a landscape architect did draw up some concepts that include a walking trail. Mr. Jacobs said that the slope in that area is somewhat profound, which should be kept in mind when determining its future. Phil said that it is hoped that the Port and the County can work on that together with a joint effort to mitigate the beach from the boat launch all the way to the pier, which is along County-owned beachfront. Mr. Jacobs mentioned that the County could potentially need access to that underground tank if it remains. Mr. Tucker said that if it doesn't work

the County can always dig up the old wet wells. He added that Public Works is limited to sewer-related projects. As the Port's plan develops, they can come back and further discuss it at that time. The preliminary concept drawing was further reviewed. Commissioners agreed that repositioning the building from where it was located according to the design would create a more open feel to the area and seems it would meet all the County's requirements. Mr. Tucker said that this is just the first attempt to discuss it publicly. He can ask about having things moved around, but doesn't want to continually ask for changes as it cost money each time. Ms. Zaroff added that another piece of information that will be very telling and necessary is the geo tech reports. Mr. Tucker said that they will have to coordinate with the Port on that. He thanked the Commissioners for their time and said that they are available anytime to continue the discussion on this project. Phil explained that Mr. Tucker has discussed this project with the National Park Service Department of Interior and was given the green light, since the County mitigated with a park in South Kitsap after being found to not have complied with the requirements of the original funding agreement by not placing all above-ground items of pump station 3 below ground. He also talked with Kim Sellars of the Recreation Conservation Office (RCO) and received the okay from her as well.

3.2. Grant Funding & Comprehensive Plan

Phil had sent Kathleen Byrne-Barrantes an e-mail dated September 10, 2019 that outlined the Ports priority projects. Ms. Byrne-Barrantes thanked Phil for the information. She said that some of the projects can be combined for possible grant funding, which is the best way to go about applying for grants. She had some questions, but she said they were just minimal and she would contact Phil outside of the meeting to further discuss. Phil suggested that in regards to the beach mitigation project, besides working with the County Public Works

they should also include the County Parks about the possibility of joining together to include the Waterfront Park's failing bulkhead. Kathleen explained that in general when the County works on a project it cost \$1,000,000 per mile. Commissioner Reese suggested that when a design for the pump station 3/shell house building is created it include a design for either upgrading or replacing the Port-owned buildings along Byron Street including the Old Town Pub (OTP), so that it is all esthetically similar. Ms. Byrne-Barrantes asked if the Port intends to keep the OTP. Phil said that according to reports the Port has obtained about the building, it would be much cheaper to take it down and just keep the façade. The other buildings may be able to be moved. Ms. Byrne-Barrantes explained that whatever design is determined it needs to be the preliminary design, it cannot be just a conceptual design when applying for the planning grants. She had reviewed the Port's updated comp plan and said that it is a good place to start. Marvel Hunt asked about the Port working together with the Central Kitsap School District (CKSD). Phil explained that he has attended two meetings with CKSD that focused on the restoration of Strawberry Creek, which is a project that would be a good fit for the Port. There is discussion about replacing the current man-made culvert-style stream with a meandering natural stream. This may require property acquisition. This all needs to be included within the Port's comp plan in order to be eligible for possible grant funding. Commissioner Scholfield said that acquisition of the Monroe property should also be included. Ms. Hunt said that CKSD is a willing helper that wants to keep Old Town Silverdale a historical area and she hopes the Port continues to connect with them. Phil said that he plans to continue to attend meetings with CKSD on behalf of the Port. Ms. Byrne-Barrantes said that the first RCO deadline is March 1st, when comp plans are due. She plans to send the final draft of the comp plan to

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the RCO for review prior to adoption in case they have suggestions. Phil explained that he recently sent the original Advisory Committee members a link to the updated comp plan. He didn't receive any feedback, so believes they had no changes. He plans to send a link to the County as well. Ms. Byrne-Barrantes said that the Port will need a letter from the County that states they support the Port's comp plan. She went on to explain the importance of including the public as much as possible. Phil told her that during the creation of the comp plan update by Rice Fergus Miller (RFM) the County and the Port joined in the creation of a public survey, which was conducted and the results are within the draft comp plan. It was agreed that the Port should plan to make a presentation of the final draft comp plan at an upcoming Central Kitsap Community Council (CKCC) meeting and advertise it to the public. Commissioner Scholfield said that the economic analysis needs to be updated within the plan. It was suggested Kitsap Economic Development Alliance (KEDA) be asked to provide information. Ms. Byrne-Barrantes thanked the Commissioners for their time.

3.3. Safe Security – Jason Mimms, Safe Security supervisor, explained that the company currently contracts nightly security service to the Port as well as additional coverage for special events, the last day of school, 4th of July, etc. With the assistance of Kitsap County Sheriff (KCSO) deputies, a convicted felon was recently trespassed from Port property before being taken to jail. The individual was attempting to sleep on one of the benches near the boat launch restroom, when he was asked to leave but refused and become argumentative. That's when the Safe Security guard on duty called 9-1-1. Mr. Mimms reported that the individual is no longer in custody. He said that similar situations are becoming more of the norm around Kitsap County. Within the month the residents at the east end of Lowell Street, adjacent to Port-

owned properties, found a homeless individual sleeping in the outside stairwell that lead to their basement. Port tenants in the neighboring building recently sent an e-mail asking what measures the Port is taking to address what appears to be a growing roaming homeless population. Bridget Burke reported that it appears someone is living in a tent in the wetlands area between the boat launch and Pacific Avenue. Mr. Mimms made note of it. Apparently, the Kitsap Rescue Mission will be temporarily closing on October 13th, so the homeless situation is probably going to become even more evident. Mr. Mimms went on to explain that Safe Security is offering to expand its services to the Port to include a Monday through Friday daytime patrol with three random visits to Port properties and it includes an on-call response officer. The cost for the service is an additional \$950 per month. Commissioner Scholfield said that in talking with some of the local Old Town businesses it seems there is an interest in services that would provide a security guard that remains in the Old Town area during business hours, similar to the "Port Host" guard that the Port hires seasonally on the weekends. Commissioner Scholfield suggested that Mr. Mimms survey the local businesses to see if this is something they want to proceed with and, if so, the Port may also be interested in it. Roy Sahali said that since everyone is aware of the problems that exist and are also aware that the Mission in Bremerton will be closing, why isn't there a little more conversation to try and figure out a solution; instead of being reactive be proactive. He said that he is empathetic for people who have no home and no place to go and can understand why they would come down to the waterfront area as it seems like a good place to be and is open to the public. Ms. Byrne-Barrantes suggested a flyer with contact information for all of the services available be on hand and provided to the homeless. Commissioner Scholfield said that chances are those fliers would be found

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crumpled up on the ground. Mr. Sahali said that you can't expect the same response as people who have all their needs met. Commissioner Scholfield said that the majority know what is available to them. Those that aren't in shelters are usually not because of drugs and/or alcohol. Ms. Burke said that she thought Mr. Sahali was asking the Port, as a local government, to get the discussion going with other local agencies in an effort to address the problem and try to find avenues to help alleviate it. Mr. Sahali said that he's not asking for answers but is suggesting that Port representatives be more empathetic to the homeless situation. Phil asked if Mr. Sahali thought the Port should take a role in it. Mr. Sahali answered yes although he wasn't sure what that role should be, but since it is a known probability that there will be a wave of homeless in the area after October 13th when the Mission closes, why not start talking about what can be done to help. Greg Jacobs said it would be beneficial if there was a way to separate the more criminal individuals versus the victimized homeless individuals. Mr. Mimms said that although he's been doing his job for over a decade, he is sympathetic to the homeless. He's not calloused towards them as he treats them just like anyone else. He keeps his requests short and respectful. It is only difficult if the interaction goes south on their end. Most times these individuals are not felons, but you can't tell the difference. Commissioner Scholfield said that at one point the Port did allow a homeless individual to live in his vehicle on Port property and in exchange he picked up the area and acted as added security. It's not that the Port Commissioners are against the homeless it's just becoming more and more of an issue with some bad actors in the mix and now what we do for one we must do for all so it just seems that asking them to move along is the logical thing to do. Phil said that over the years the Port has received complaints from the public that they were asked to leave the boat launch area or

pier when they were just down there enjoying the view, causing no problems. Mr. Mimms said that it's okay to be down there during the day, but around dusk individuals are asked to leave to be in compliance with the Port rules and similar to the homeless what you ask of one you must ask of all – no favorites. He said that they do get pushback from some individuals who are under the impression it's public property, but it's not it's private property. Commissioner Scholfield said that the Port has the same rules as the County Parks; they close after sundown. Phil corrected Mr. Mimms about it being private property and advised him that he should not be telling people it's private property but rather that the Port has jurisdiction over this property and it is closed at this time. Mr. Mimms took note. He said that he works upstairs in the Port building and he thinks it would be beneficial to the Port to have the added security during the day. Commissioner Reese asked why wouldn't we just call 9-1-1 if there was a problem. Commissioner Scholfield said that having the security guard would be beneficial as they are familiar with working with KCSO and at times must identify individuals. Situations often escalate quickly; it would be better to have a security guard trained and/or experienced in such situations to be involved rather than Port staff and/or tenants. Tim cautioned Mr. Mimms on having a good guideline of how and when they engage people. A lot of times people just want to sit down and enjoy the view while they are eating lunch or dinner from their cars. People certainly shouldn't be asked to leave the area prior to dusk. Mr. Mimms said that the daytime service isn't intended to remove anyone from being on the property. It's more for uncommon incidents and adding a security presence to Port properties. Tim suggested the Commissioners try it for a few months and then review it. It was confirmed that there was no additional contract for the added service and it could be dropped at any time. *It was agreed to contract Safe Security to conduct daytime patrols*

Mondays through Fridays, 0800 to 1700, including an on-call response officer costing the Port \$950.00 per month (motion by Reese; second by Aus; unanimous). Mr. Mimms will contact other Old Town businesses to determine the need for a stand along guard in the old town area.

4. UNFINISHED BUSINESS –

4.1. Port Programs

a. Sailing – Mr. Jacobs asked that Ms. Burke go before him as she had a time constraint.

b. Rowing – Ms. Burke reported that the CIR's Juniors Program now has nineteen participants, which is the biggest amount so far. She said that CIR is finally hitting the tipping point financially with a positive cashflow. Ms. Burke said that she has talked to Tim about moving all of CIR's equipment from the side of the Port building to the waterside of the OTP on October 5th. One of CIR's members is a landscape architect and has it all mapped out where everything will go and Ms. Burke plans to provide Tim with the map. Tim explained that day one will consist of moving the boats across the street temporarily. The next day he will move the shed and the racks to the OTP area and the boats will be brought back into the space on the side of the Port office building until Tim is able to install the racks. Ms. Burke said that she nearly got ran over tonight from an erratic driver. She asked that if the County ever does the road work on Byron that the Port request that a speed bump or speed table also be installed. Commissioner Scholfield said that the County plans to install what they call "bulb outs" that have proven to reduce speeding in areas. Ms. Burke said that CIR would like to hang a banner on the OTP to advertise the program. Tim plans to run water from the meter to CIR's new area and install a frost-free yard hydrant. Ms. Burke said that the rowers are concerned that the Port will be taking the building down in

the near future, which will impact them. She asked if there was a plan to raze it and leave the area just a flat empty lot for the time being. Commissioner Scholfield said that keeping the building as is for now is important to preserve the footprint, which includes the height. If it is decided to rebuild after the building is already taken down the County can require the Port to follow the new standards and setbacks. Commissioner Reese said that also the Port will be applying for grants that may cover such a project, but if the building was taken down beforehand the Port would be responsible to pay for that with no reimbursement. Tim added that if it is taken down before a grant is awarded the Port pays for that, but if it is taken down after the grant is in place the Port can use the associated costs towards the required matching funds. An e-mail from Ms. Burke dated September 1, 2019 was reviewed. On August 31, 2019 a group of rowers were washing down a boat using the spigot near the boat launch. A group of visitors came up to them and were asking questions about the rowing program and just small talk. While they were talking the Port's landscaper started trimming with a weed eater close by, making it nearly impossible to continue the conversation but also kicking up dirt and rocks. One of the rowers was even hit in the leg by a rock and she confronted the landscaper, who was unapologetic and basically blamed the rowers for not moving as he continued on trimming the area. Commissioner Scholfield said that shortly after the incident the landscaper informed him of what happened. He admitted he blew it as he was behind schedule and was anxious about not being able to complete his work. He didn't mean to kick up the rocks. Ms. Burke just wanted the Commissioners to be aware of it.

Sailing – Mr. Jacobs reported that there will be a regatta on November 2nd. Tim said that the handling pier will be removed by that date. Tim asked when the sailboat float can be

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pulled from the water. Mr. Jacobs said that he needs to talk to Steve Trunkey about that; he knows they do plan to have the Fall sailing going up until November and having the handling pier as long as they can is beneficial. Mr. Jacobs said that KSRF provided a seven-week summer camp program this year. In year's past it was just a three-week program. The classes were less full this year but it did provide more flexibility, which was expected since it was the first year.

4.2. Port Facilities

a. No-Wake Buoy Project – Tim reported that the project is complete and provided pictures. Payment is being made at tonight's meeting.

b. Department of Natural Resources (DNR) – Phil had AES Consultants provide him a map of the proposed lease area. He has also asked them to draw up the legal description, which will be sent to the DNR.

c. Dredge – An e-mail from Nam Siu of Department of Fish and Wildlife (DFW) addressed to Meg of Marine Surveys and Assessments (MSA) dated September 18, 2019 was reviewed. There is a work window change for the dredge. For the deep water dredging around the floats, the new window is September 1st through February 15th and the new window for the intertidal dredge around the boat ramp is September 1st through October 15th. Commissioners agreed that this should be an adequate amount of time to get the dredging of both areas completed. Phil will ask MSA to proceed.

d. Sailboat (WN6823JF) – Phil suggested that the boat be regarded as an abandoned vessel and move forward with selling it. Scott Carlson is the individual who moved the boat during Whaling Days and has been asked to repay the Port \$75 as Port personnel had to go and retrieve the boat from

Dyes Inlet. Mr. Carlson has verbally agreed to pay the \$75 on several occasions, but it has yet to be received. Phil informed Mr. Carlson via e-mail dated August 29, 2019 that the Port will have to reinstate the criminal investigation for boat theft and issue a no-trespass order against him if the \$75 is not received. Mr. Carlson did not respond. Commissioner Scholfield suggested it be forwarded to Kitsap County Sheriff's Office with the case number and have him trespassed at the same time. Commissioner Reese suggested to hold off on the trespass, but have a sheriff deputy contact him. That may be enough to get him to pay. *It was agreed to have Scott Carlson trespassed from Port property if the \$75 fee is not paid by October 1st* (motion by Aus; second by Reese; unanimous).

e. Benches – Phil explained that he prepared a Request for Quotations (RFQ) for the benches. At the time, TIKAR was the only business that we knew of to complete the work. Phil researched and found another fabricator, who completed and returned a Small Works Roster (SMR) application, but failed to respond to the RFQ in time. He had called Phil today and said that he was going to be submitting the RFQ. Phil explained to him that the deadline had passed. Tim did submit the RFQ but questioned the wording of it. It was his understanding that the current seven benches be retrofitted at their current locations and the new benches just fabricated and not installed, but the RFQ requires installation of the new benches. It was agreed the RFQ should not include installation of the new benches. Phil made note of it and said that he will change the wording on the RFQ and send it back out with a new deadline date and time.

f. Port Rules and Regulations Policy – Nothing to report

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g. Electrical Repair – the pedestal that was damaged due to a fire several months ago needs to be repaired. SWR applications were sent to both Bronco Electric and Bainbridge Island Electric. Bronco Electric did not respond. Bainbridge Island Electric submitted their application and a contract/estimate for the repair. It was reviewed. It offered two options: one was to replace a 50AMP cable feeder from the existing junction box to the nearest pedestal which would cost \$1900, the other option was to replace a 50AMP cable feeder from the existing junction box to the farthest pedestal which would cost \$2100. It was agreed that since 30AMP service is currently available on the floating docks, Bainbridge Island Electric will be asked to resubmit an estimate for 30AMP service and they will be notified that the Port is required to assure contractors pay prevailing wage, so a breakdown of hours/rate is also necessary.

h. Sea Plane Crash Expenses – a letter dated August 14, 2019 was mailed to Mr. Gotschall the pilot that crashed his seaplane into Dyes Inlet last month. Central Kitsap Fire and Rescue (CKFR) requested the Port respond to the incident because the Port had the necessary crane equipment at the boat launch area, since TIKAR was working on the dredge under the boat ramp at the time. TIKAR and Commissioner Scholfield's time to respond and remove the sea plane and debris from Dyes Inlet totaled \$2,359.34. Mr. Gotschall has yet to respond to the letter that was mailed to him. The letter will be sent certified mail return receipt requested before being forwarded to CKFR.

i. Eagle Scout Life Jacket Loaner Station Project on Pier – Tim said that he

donated all of the lumber and delivered it to Jack West's house. He also requested they use stainless steel fasteners for the project.

j. Pier – E-mails to and from Phil and John Kuntz of Olympic Outdoor Center (OOC) were reviewed. Phil had asked Mr. Kuntz to remove the 2x4 board that was screwed into the pier just west of the kayak rack as it posed a possible tripping hazard to the public. On August 21, 2019 Mr. Kuntz responded saying that he would remove the board since it is close to the end of the season. The board is still there. It was suggested Tim remove the board and bill the Port and in turn the Port will bill OOC. Tim said he will charge the Port \$50 to remove the board. *It was agreed to pay TIKAR Services \$50 to remove the 2x4 board and forward the charges onto OOC* (motion by Reese; second by Aus; unanimous).

4.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – Mike has been in contact with the County regarding the septic outlet and they are going to add the outlet to the drawings.

b. 9004 & 9020 Washington Avenue/DRC & Elizabeth's House of Wax – a call was received from the Silverdale Water District informing the Port that there was a water leak somewhere between the two properties. Commissioner Scholfield said that it is unknown exactly where the leak is and it may be in the ground. The toilet at 9020 Washington at times runs, Mike was going to check on that. RFQ's to paint the buildings were e-mailed to several businesses listed on the SMR. Two responses were received by the September 18, 2019 deadline as follows: Action Maintenance and Painting \$22,706.00

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and Quality Coating Enterprises \$10,022.00. Both responses were reviewed. The bids reflected the use of different brands of paint. It was determined that the RFQ required the paint color to be used not a certain manufacturer. *It was agreed to accept the low bid from Quality Coating Enterprises in the amount of \$10,022.00* (motion by Reese; second by Aus; unanimous).

c. 3330 Lowell/Sugar Studios – Phil reported that the road approach permit is complete.

d. 3215 Lowell Street, Suite 161 – tenant was behind in rent for August and September, but came into the Port office and paid plus late fees and included \$50 towards October.

e. 3473 NW Byron Street/Vacant – the sign bracket on the OTP is still there.

4.4. Release of Easement – Phil explained that he hasn't heard back from the County yet.

4.5. Paving of alley – Commissioner Scholfield asked Mike if he had applied for the permit yet. Mike explained that he first needs to know the contractor that will be performing the work. Phil reported that he resent the RFQ's to the same six companies (Agate Asphalt; Apply-A-line, Inc.; Caseco Associates, Inc.; Granite Construction Company; Lakeside Industries, LLC; and Northern Asphalt, LLC) as last month explaining to them that the deadline was extended in hopes to receive more responses. Two responses were received by the September 17, 2019 deadline as follows: Lakeside Industries \$49,834.15 and Agate Asphalt \$78,272.43. *It was agreed to accept the low bid from Lakeside Industries, LLC in the*

amount of \$49,834.15 (motion by Reese; second by Aus; unanimous).

4.6. Comprehensive Plan – Commissioner Scholfield requested a link to the draft comp plan be added to the website. RFM billed the Port \$5,200.00 for their most recent work on the comp plan. This was discussed. *It was agreed to pay Rice Fergus Miller \$5,200.00 as outlined in Invoice #2017098.00-11* (motion by Aus; second by Reese; unanimous).

4.7. Grant Funding was discussed under Public Comment.

4.9. Westsound Networking Group – Commissioner Reese recently made a Port presentation to the group. He explained that he was given about twenty minutes to make the presentation. The group was very appreciative and several attendees were surprised to learn that the Port owned property other than the pier and moorage facility. Commissioner Reese said it was a good experience.

4.10. Benefits – nothing to report.

4.11. Legal counsel – Phil said that he has decided to renew his license to practice law for another three years. He is interested in being hired on as Port staff and be eligible for retirement benefits.

4.12. State Audit – will be conducted beginning at the end of this month.

5. NEW BUSINESS

5.1. 2019 Budget Review report was discussed.

5.2. 2020 Preliminary Budget will need to be adopted during the October meeting.

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5.3. Port credit cards – it was thought the way purchases via credit card are generally made should be discussed to insure all the Commissioners understand and answer any questions they may have. Commissioner Scholfield uses his wife's Amazon Prime account for Port items in order to receive next day shipping at no cost. The Port's credit card is used to make these purchases, but it is not set as the default payment account. Commissioner Scholfield has the items shipped to his house because the Port office isn't always open during regular business hours and Fed Ex will not leave packages at the door step of the Port office, understandably so, but they will leave packages at the front door of a residence. Commissioner Reese questioned why items aren't purchased at local businesses in an effort to support economic development. Commissioner Scholfield explained that the Port does purchase from Lowe's and Costco as well, but when items are not available from the local stores, he will shop for them via Amazon. The Commissioners understood the reasoning for using the personal Amazon Prime account and agreed that having items delivered directly to Commissioner Scholfield's residence was understandable.

5.4. Enduris – the premium is scheduled for authorization at tonight's meeting it increased \$2,577.00 from last year. The \$1,000.00 deductible regarding the Bayer claim is also scheduled for authorization at tonight's meeting.

5.5. Moorage payment dispute – a \$45.00 delinquent moorage fee payment from Greg Hunt was received and included a note stating that this is the second time that they have been

billed for moorage that they had paid for with cash they had placed in the moorage fee box at the landside of the pier. It was suggested a response be sent to Mr. Hunt explaining that he may want to begin saving the envelope receipt so that the envelope can be tracked and/or stopping by the Port office to pay directly.

5.6. WPPA Small Ports Seminar is scheduled for October 24th and 25th in Leavenworth. On Thursday morning an optional intensive workshop regarding public works projects is being offered. It will be beneficial to have the Port attorney and Port staff attending the seminar especially the portion related to public works. It was agreed that since all three Commissioners, Port attorney and Port staff will all be in attendance this will be considered a Special Meeting and notice will be placed on the door of the Port office prior to leaving for the seminar.

5.7. Wedding – Julee Younger contacted the Port office a couple weeks ago inquiring about the possibility of having her wedding on the far finger pier at the moorage facility. She was asked to complete a Port Use application, provide proof of insurance listing the Port as additionally insured and submitting \$75 payment. She completed everything and the wedding was held just prior to tonight's meeting.

Commissioner Reese has been attending the CKCC meetings in the past, but now is basically representing the Port at the meetings. The last meeting both Commissioners Reese and Aus were in attendance. The did not plan it and no Port business was discussed between them as they realized that would be in

violation of the Open Public Meetings Act. Commissioner Aus explained that he will periodically attend the meetings if he hears of something that interests him. He wasn't expecting the stipend for attending the meeting. It was agreed that Commissioner Reese should be paid for attending the meetings as a Port representative.

6. SAFETY – nothing to report.

7. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$69,758.73, checks numbering 12186 through 12213 and Electronic Funds Transfer 2019-09 to the U.S. Treasury in the amount of \$1,998.98, were approved (motion by Aus; second by Reese; unanimous).

8. EXECUTIVE SESSION – At 10:57PM it was announced that the meeting would be going into Executive Session for approximately ten minutes to discuss litigation

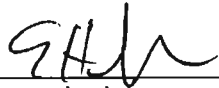
At 11:08PM the meeting returned to Regular Session.

Due to a conflict-of-interest Phil will be contacting local attorney Phil Havers to begin taking over as legal counsel for the Lanouette litigation.


9. ADJOURN

At 11:09PM the meeting adjourned (motion by Aus; second by Reese; unanimous).

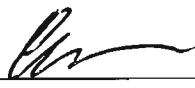
Approved:



Commissioner



Commissioner



Commissioner

Port of Silverdale – Minutes of Regular Meeting on October 17, 2019

Port of Silverdale
Minutes of Regular Meeting
October 17, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Attorney Phil Havers. Others were in attendance prior to the meeting being called, but were informed Executive Session was early on the agenda.

At 7:01PM it was announced the meeting would be going into Executive Session for approximately twenty minutes to discuss litigation.

At 7:27PM the meeting returned to Regular Session.

Attorney Havers thanked the Commissioners for their time.

Those now in attendance included Commissioners Scholfield, Reese and Aus; Attorney Best; Administrator Haaland; Tim Knapp and Lee Knapp of TIKAR Services; Bridget Burke of Clam Island Rowing (CIR); John Bouck and Steve Trunkey of Kitsap Sailing and Rowing Foundation (KSRF); Carla Larson of the Silverdale Dandy Lions; Roy Sahali of Sahali Farms; Laurie Aardal of Hui Heihei Wa'a Canoe Club; and Silverdale resident Marvel Hunt.

Agenda – *the agenda was approved as submitted* (motion by Aus, second by Reese, unanimous).

2. CONSENT AGENDA

2.1. *The September 19, 2019 Regular meeting minutes were approved as submitted* (motion by Reese; second by Aus, unanimous).

3. **EXECUTIVE SESSION** was covered prior to the consent agenda.

4. PUBLIC COMMENT –

4.1. Christmas tree lighting – Carla Larson provided a draft layout of the Silverdale 65th annual Christmas Tree Lighting celebration. She asked if the Commissioners would consider making a minimal donation to the festival as well as provide the fire pit as in years' past. She already has a donation for the wood for the fire pit. *It was agreed to donate \$30.00 to the Silverdale Dandy Lions to help cover the costs of the 2019 Christmas tree lighting celebration and provide the fire pit for a bon fire during the celebration* (motion by Aus; second by Reese; unanimous).

4.2. Hui Heihei Wa'a canoe club member Laurie Aardal said that the club really appreciates the Port allowing them to store their new canoe along the fence on the south side of 3215 Lowell Street. On November 2nd they will be moving two of the three canoes to Mitchell Lumber for winter storage. She said that the club would ultimately like to secure storage space in the Old Town area to house their forty-foot canoes with trailer accessibility, so they are very interested in being included in the Port's long-range plans.

5. UNFINISHED BUSINESS

5.1. Port Programs

a. Sailing – Tim reported that the sailboat float and the boat launch handling pier are scheduled to be removed on October 29th. Steve Trunkey said that they are planning a regatta on November 2nd and 3rd. John Bouck explained that it is much easier and better if the handling pier is available for the regatta. Tim said that he would like to get them out on October 29th.

b. Rowing – Bridget Burke explained that she met with Tim at the fenced area behind the OTP, which will now be referred to

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as the boatyard, and they roughly mapped out the area of where the shells and equipment will be placed. She said that one of the members is a landscape architect and provided a drawing of the layout. It was reviewed. The longest rowing shell is approximately sixty feet long by six feet wide so it will be a tight fit, but it will work thanks to some out of the box feedback from Tim. They do have an issue of where to place the program's storage shed, which is used to house all of the tools and the rowers' personal items when they are out on the water. Ms. Burke asked if it could be moved behind the Kitsap Art studio. Commissioner Scholfield said no because it would impede on the art studios parking, but they could use one of the parking stalls behind the pub. Ms. Burke said that using one of the stalls behind the fenced area would actually work great. She added that although the move is going to be a bit burdensome, overall it is really good because it gives CIR even more room to grow and they won't have to cross a road to get to the water. She said that she is not sure when they will be moving from rowing alley to the boatyard. She originally thought they would move the first week in November, but that is quickly approaching and she needs to make sure all the details are worked out and the drawing is updated so that everyone is on the same page and it all works out on paper. She will e-mail the Port office the updated drawing for the Commissioners and Tim to review.

The lease between the Port and KSRF and CIR was discussed. It was explained that when there was discussion of the Port taking over the programs, the Port had paid the Leasehold Excise tax on behalf of KSRF and CIR and the leases were not renewed. The total paid to Washington State Department of Revenue for the sailing equipment totaled \$1,497.50 and for the rowing equipment \$1,067.50. This covered the timeframe from 2014 through 2018. In order to proceed with taking over the programs the Port needed to

receive an exception from the Kitsap County Treasurer allowing funds to be deposited weekly rather than daily. The Treasurer was hesitant about granting an exception but said she would consider it once the Port had answered several of her questions and requests. The Port had a limited amount of time to be granted the exception and it was impossible to complete the items the Treasurer had required in time. There was also uncertainty if once all the questions were answered and requests fulfilled if the Treasurer would accept them and grant the exception, so it was decided to have KSRF and CIR run the "Port" programs as volunteers. Come to find out, if these are actually Port programs then the Port needs to be in charge of all the funds related to the programs, which is not the case. Commissioner Scholfield said that KSRF and CIR need to reimburse the Port for the leasehold excise tax that was paid to the State on their behalf and a new lease needs to be generated and entered into all by the end of the year. The programs non-profit status was discussed and it was questioned why one of the Port tenants, that is a non-profit, is exempt from paying leasehold excise tax. Phil said that the difference is real property verses personal property. Commissioner Scholfield suggested the Port be responsible to maintain the equipment. Mr. Trunkey said that they spend thousands of dollars a year on maintaining the boats and equipment not to mention all of the volunteer hours included with that. He provided a list of their maintenance costs throughout the past several years. Carla Larson asked if they could be retroactively reimbursed for those costs since the Port is responsible for maintenance. It was agreed that the Port would be responsible for the maintenance of the motorized boats including motors and trailers. In exchange KSRF and CIR would be responsible to continue to maintain the fleet of sailboats and rowing shells and equipment. It was thought that if KSRF and CIR could provide all of the 2019 receipts

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for maintenance on the safety boats, motors and trailers, the Port could reimburse them for those costs. Commissioner Scholfield said that they also need to follow a safety checklist prior to driving off with a boat and trailer. He will send a link for the equipment safety checklist. Phil will update the lease to include a provision about maintenance of the motorized safety boats, motors and trailers. He will also see if a maintenance reimbursement can be made retroactively.

Mr. Turnkey, Mr. Bouck and Ms. Burke thanked the Commissioners for their time and consideration. Ms. Burke mentioned that the Port's webcam is very popular.

4.2. Port Facilities

a. Department of Natural Resources (DNR) – Phil e-mailed Don Olmsted the proposed updated outer-water boundary map. Mr. Olmsted has yet to respond.

b. Dredge – An e-mail from Meg Amos of Marine Surveys and Assessments (MSA) dated October 9, 2019 was reviewed. It informed the Port that an informal appeal hearing for the dredge is scheduled for November 8, 2019 at 9:00AM via teleconference. A group called Sound Action filed the appeal and had several concerns which were outlined within the e-mail. Ms. Amos had drafted her responses to the concerns and seems confident that they will prevail.

c. Sailboat (WN6823JF) – Phil explained that he needed a little more time to process the boat as he wants to double check the legalities prior to putting it up for sale. Periodically individuals call the Port office asking about the boat as they are interested in possibly purchasing it. A list of their contact information has been created. Commissioner Scholfield said that the boat is currently dry.

d. Benches – Phil explained that he made the corrections to the Request for Quotations (RFQ) for the benches and re-sent the RFQ to the two businesses: Rory's Custom LLC and TIKAR Services LLC. Estimates from both were attached to the agenda packet and reviewed. Rory's estimate came in at \$22,041.65 and TIKAR's totaled \$21,255.00 both included tax. Phil explained that Rory's submitted their first estimate but it didn't include the wood for the benches, so Phil called them to inform them that they should include the wood so that the Commissioners can view similar estimates as outlined in the invitation. They did resubmit an updated estimate within the deadline. *It was agreed to award the bid to the low bidder, TIKAR Services LLC* (motion by Aus; second by Reese; unanimous).

e. Port Rules and Regulations Policy – Nothing to report

f. Electrical Repair – Bainbridge Island Electric was asked to resubmit the contract to reflect 30AMP service to the damaged pedestal at the floating moorage facility. The new contract was reviewed. Bainbridge Island Electric was informed that prevailing wage must be paid. They replied that an Affidavit of Intent to Pay Prevailing Wage will be filled with the State. *It was agreed to accept Bainbridge Island Electric's Contract #VPE09261901 preferably with the first option at \$1,750.00 plus tax but if needed the second option at \$1,900.00 plus tax* (motion by Reese; second by Aus; unanimous).

g. Sea Plane Crash Expenses – a second letter dated September 27, 2019 was mailed certified return receipt requested to Mr.

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Gotschall, the pilot that crashed his seaplane into Dyes Inlet last month. The form showing that he signed for the letter has not yet been received from the Post Office. Commissioner Scholfield requested a letter be sent to Central Kitsap Fire and Rescue (CKFR) requesting reimbursement to the Port for the costs it incurred in removing the plane debris, since the Port performed the work on their behalf and at their request.

h. Eagle Scout Life Jacket Loaner Station Project on Pier – Jack West had e-mailed the Port office asking about details for a kiosk that will inform the public about the life jacket loaner program. The Port has an extra informative sign from the station near the boat launch. A photograph of that sign was e-mailed to Mr. West and he was asked to contact Port staff to set up a time for him to come in and take measurements of it. Tim said that he has reached out to Mr. West, but hasn't received a response.

i. Pier – John Kuntz of Olympic Outdoor Center (OOC) was notified of the costs to remove the board. He has since reimbursed the Port the \$50. He was also notified of another issue that recently came up when a boater moored his boat in the area near the end of the gangway where OOC had attached a kayak assist bar to the Port's floating dock. It looks as though when the bar was removed the wood must have just been pulled off leaving the screws sticking out and when the boater tied his boat in that area the remaining screws damaged his boat. Commissioner Scholfield was on the docks when it happened and he told the boater he needs to take this up with OOC. Phil said that Enduris should be notified that the Port may be included in a claim as the

damage happened while the boat was moored at the Port. It was thought the screws may still be there. Tim plans to check on it. OOC submitted the annual report for the summer of 2019. It was reviewed. Commissioner Reese found it interesting that within the report it stated that they had received a complaint about the paddleboard support board and that they dealt with it in an expedient manner, yet the Port ended up having TIKAR remove the board after it still remained there over a month after requesting OOC to remove it.

j. Boat launch handling pier – scheduled for removal on October 29th as previously discussed.

5.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the sewer line connection to the existing outlet is tabled until the County's Bayshore/Washington Improvement Project is underway.

b. 9004 & 9020 Washington Avenue/DRC & Elizabeth's House of Wax – Quality Coating Enterprises was e-mailed the Notice of Award and Contract. They have since power washed the building. It is unknown when they plan to paint, but are more than likely waiting for a window of dry weather.

c. 3330 Lowell/Sugar Studios – when the tenant came into the Port office to drop off her rent check, she voiced her concern about the thick mud behind the neighboring building – 3332 Lowell. It was acknowledged that it is continually wet and mucky in that area.

d. 3423 Byron/Old Town Massage – when the tenant came into the Port office to drop off her rent check, she explained that she would

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like to have a wall installed in the larger room, making it two smaller work areas. The Commissioners requested that the tenant first provide the Port with the plans for approval. After that tenant would be required to hire MTV Home Repair to apply for the necessary permits and complete the project. Tenant would be responsible for all of the costs.

e. 3473 Byron/Old Town Pub – the sign bracket still remains on the building and is believed to be secure. Dan Sullivan of J. D. Sullivan Investments stopped by the Port office and asked about the future of the OTP. He is planning to attend the November meeting to further discuss.

5.4. Release of Easement – Phil explained that this is not actually a release of easement. It was suggested the title be changed to Easement. He has been e-mailing with County personnel the request to vacate the alley. The County is requiring a sixteen-foot utility easement. Phil noticed that the other County utility easements on Port property are fifteen feet. He has since asked them to reconsider with a fifteen-foot easement to make it all uniform with what is already in place and since the Port wants to have the most optimum area possible for future development. A map showing the proposed changes was reviewed. The County will not approve the vacation without the easement in place, so the Port may be required to accept the sixteen-foot easement, but it is hoped they will reconsider.

5.5. Paving of alley – the Notice of Award Public Works Contract was sent to Lakeside Industries. They returned the signed contract. ***It was agreed to sign and accept the contract with Lakeside Industries for the paving of the alley and***

parking area east of Washington Avenue between Byron and Lowell Street (motion by Aus; second by Reese; unanimous). Phil reported that Lakeside has already provided the Affidavit of Intent to Pay Prevailing Wage and proof of insurance. He explained that he may have to amend the contract as it currently states the work must be completed by November 30, 2019, but he is in the process of applying for the permit and the County may require an additional stormwater permit due to the amount of square footage being paved. Phil will keep the contractor up to date on the permit status.

5.6. Comprehensive Plan – Kathleen Byrne-Barrantes has been working on updating and making some changes to the draft comprehensive plan that Rice Fergus Miller has prepared. She included a Capital Improvement Program (CIP) outline, which was reviewed. Phil said that Ms. Byrne-Barrantes made some good additions to the plan including this CIP. He questioned if the 2020 Preliminary Budget, as prepared, would line up with the amounts listed in the CIP. Tim explained that there was an old concept about having an open aired market along the strip of land on Byron Street that the Port owns; the OTP and adjacent properties. Commissioner Scholfield said that he thought something of a similar concept would be more towards the grassy area with glass windows on the bottom floor to lessen any obstruction of the view. Commissioner Reese said that Mark Hughes is on the Board of the Central Kitsap Community Council (CKCC) and he asked if the Port could make a presentation of the comp plan during the council's December 4th meeting. It was agreed that by December 4th the draft should be pretty complete, so yes that would be

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doable. Commissioner Reese will plan to make the presentation.

Marvel Hunt had dropped off a letter to each of the Commissioners within the month. The Commissioners were given the letter prior to the meeting. Within it, Ms. Hunt outlines concerns she has with Phil's role with the Port. She explains that Phil had reached out to the original Citizen's Advisory Committee (CAC) for feedback on the draft comp plan. Phil didn't receive a response from anyone and according to Ms. Hunt he announced at the September 19th Port meeting that no one responded so he took that as no one had anything to say. The month prior it was announced that there would be one more meeting with the CAC, but Ms. Hunt thought that since Phil said no one had anything to say that another meeting would not be called, which disturbed her. She ended the letter explaining that she is concerned of the appearance that Phil is running the Port not the elected officials. She asked the Commissioners to step up and take full control. Start inviting the public in by following up with committees, follow through on projects and advertise the meetings in the local paper. Phil said that he is not and was not against having a final CAC meeting. He thought the committee members should be invited to the upcoming Port meeting and be brought up to speed on the comp plan. He realizes all the committee members spent many hours attending meetings to get to this point. Commissioners Scholfield and Reese suggested a separate meeting be held just for the committee members so that they can have the necessary time to ask questions and discuss everything. Phil said he fully agrees and suggested a Special Meeting be called so that all three

commissioners can attend. It was agreed to hold a Special Meeting on November 19, 2019 at 4:00PM at the Port office. The CAC will be notified and encouraged to attend.

5.7. Grant Funding – once the Comprehensive Plan is complete, but prior to adoption, it will be sent to the Recreation Conservation Office (RCO) for input.

5.8. Pump Station 3 – Commissioner Scholfield, Phil and Theresa met with Steve Rice of Rice Fergus Miller (RFM). He was asked to expand the comp plan map to include the Monroe property and the Shore Place apartments. Mr. Rice asked if the Port would consider a proposal for a preliminary design of the pump station from RFM. He explained that this is something that will be necessary to move forward with grant funding and he has already completed some of the work as he began it when he first started working with the Port on the comp plan. The preliminary design covers the rough estimates for site work, permits, soft costs, etc. It's not the actual design of the project. It is something the Port could use to go out for bid for the design of the building. The proposal will need to be approved before Mr. Rice would move forward so he was asked to go ahead and submit it for review at an upcoming Port meeting. It was thought that if the proposal is accepted this will be an extension of RFM's current contract with the Port. RFM did bill the Port the final \$1,300 for the most recent changes/updates to the comp plan. The check is set to be authorized at tonight's meeting. During the meeting with Mr. Rice the bill was brought up and discussed with Mr. Rice. He was asked if RFM personnel spent all that time on the updates or if they just bill for the remainder of the contract. Mr. Rice

Port of Silverdale – Minutes of Regular Meeting on October 17, 2019

said there was no question that they did work all of the hours and offered to send an updated billing that includes the hourly breakdown.

5.9. Benefits – information from Airlift Northwest of Washington was provided. The annual cost for members is \$79, but members must have air medical transport benefits already in place. Commissioners and staff will verify that they have these benefits and this will be further discussed next month.

5.10. 2020 Preliminary Budget was reviewed. It was agreed that additional funds should be added to 774.00 Waterfront:774.60 Programs. Changes were made. *The 2020 Preliminary Budget was approved as submitted* (motion by Aus; second by Reese; unanimous). Notice will be placed in the local newspaper accordingly.

5.11. State Audit – a letter dated October 8, 2019 from Tammy Lane the Assistant State Auditor who performed the Port's audit was reviewed. The audit for fiscal years 2017 and 2018 is complete and the preliminary results will be shared with the Commissioners during the exit conference scheduled for Wednesday October 30, 2019 at 11:00AM at the Port office. There is a chance that all three Commissioners will be attending so it will be considered a Special Meeting and notice will be placed on the Port office door.

5.12. Purchased Services contracts and professional service contracts need to go out for bid soon. Phil explained that this is one of the main reasons he wanted to go to the Washington Public Ports Association (WPPA) Small Ports Seminar in Leavenworth as purchased service contracts are on the agenda

early Thursday morning. He plans to have this ready by the end of the year.

6. NEW BUSINESS

6.1. Tide Books – an e-mail dated September 20, 2019 from Ned Krilich was reviewed. He asked if the Port would be interested in purchasing tide books advertising the Port on the cover. It was agreed that it is just so much easier and convenient to view the tides via a cell phone. The Commissioners decided against advertising with the tide books.

6.2. Commissioner District 1 will be on the November ballot. Commissioner Reese is running unopposed.

6.3. WPPA – Commissioner Reese is interested in attending the New Commissioner seminar that is scheduled for November 19th and 20th in Tacoma. The WPPA registration fee is \$150.00. *It was agreed to authorize Commissioner Reese to attend the upcoming WPPA sponsored New Commissioner Seminar in Tacoma, WA on November 19 – 20, 2019* (motion by Aus; second by Scholfield; unanimous).

6.4. Old Town – a local business woman came into the Port office today and explained that she is trying to get Old Town to get festive by adding Christmas lights and decoration to their buildings. Commissioner Scholfield said that the Port did have lights on the building they were just removed so that it could be painted. New lights may need to be purchased. The woman was hoping lights could be installed along the streets and thought that may be the responsibility of the Port. She was advised to contact the County. She asked if the pier could be decorated with lights. Tim said that he has talked about adding some lights to the marker

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pilings and now maybe the buoys. He will work on it. Tim explained that the caps on the pilings aren't the permanent ones yet. Years ago, a bolt of lightning hit one of the pilings and the cap went flying up in the air, it was incredible.

7. SAFETY/SECURITY

7.1. Trespass – Safe Security daytime patrol trespassed Lisa Sperry for one year as she was being erratic and showed aggressive behavior and refused to comply with the security guard's orders.

7.2. Incidents – Commissioner Scholfield reported that an individual was driving erratically and doing donuts in the Port's parking lot earlier today. Unfortunately, his license plate number could not be read.

Phil asked if the Commissioners needed to approve the Port's staff attendance at the upcoming seminar in Leavenworth. *It was agreed to authorize Port attorney, Phil Best, and Port administrator, Theresa Haaland, to attend the WPPA sponsored Small Ports Seminar in Leavenworth, WA on Wednesday, October 23rd, 2019 through Friday, October 25th, 2019* (motion by Aus; second by Reese; unanimous).

8. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$39,688.11, checks numbering 12214 through 12244 and Electronic Funds Transfer 2019-10 to the U.S. Treasury in the amount of \$1,997.78, were approved (motion by Reese; second by Aus; unanimous).

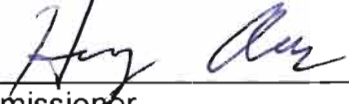
9. EXECUTIVE SESSION – None

Phil said that he is working on interrogatories on the Bayer claim.

10. ADJOURN

At 10:11PM the meeting adjourned (motion by Reese; second by Aus; unanimous).


Approved:



Commissioner



Commissioner



Commissioner

MINUTES OF SILVERDALE PORT MEETING ON November 18, 2019

Commissioner Scholfield called the meeting to order at 4:01PM at the Port office – 3550 NW Byron Street, Silverdale, WA. It was explained that the meeting was being held to discuss the draft updated Comprehensive Plan and other Port business. The Waiver of Notice of Special Meeting and a Notice of the Special Meeting, that was taped to the Port office door the Friday prior to the meeting, are both attached hereto. Also in attendance were Commissioner Aus; Commissioner Reese; Attorney Phil Best; Administrator Theresa Haaland; Dave Montoure of Kettle Fish; Community Advisory Committee (CAC) members: Ruth Harris, Phil Havers, Mark Hughes, Marvel Hunt, Carla Larson, Joyce Merkel, Vickie Sontag, Mike Vasquez, Kay Wilson, and Monica Downen attended via telephone.

Phil Havers had provided a Settlement Agreement between the Port and Fred Lanouette. It was reviewed. Commissioner Scholfield explained that the settlement was in favor of Mr. Lanouette receiving \$18,000 from the Port for an alleged breach of the Port's obligations under the Purchase and Sale Agreement and subsequent lease of the 3473 Byron Street property. ***It was agreed to accept the written Settlement Agreement (Superior Court, Cause No. 19-2-01997-18) and that Port commission chair Ed Scholfield sign the agreement on behalf of the Port*** (motion by Reese; second by Aus; unanimous).

The Executive Summary Plan and enlarged Focus Area Plans that were prepared by Rice Fergus Miller (RFM) were on display. The Capital Improvement Program (CIP) handout that was prepared by the Port's grant writer, Katheleen Byrne-Barrantes, was provided to the attendees. Commissioner Reese presented the Port's draft Comprehensive Plan.

Phil Havers said that he was concerned about the plan because nearly eighty percent of it seems to be aimed at boating facilities, with a new dock for non-motorized boats and upland storage for sailboats and/or rowing shells. He recalled when the CAC members met sometime ago, one of the main topics was how to draw the public to the Old Town area and he doesn't see the focus on boat storage doing that. It's good for the Port to put some focus on the non-motorized boating facility and the boating community, but he was surprised that there wasn't more focus on items to support economic development to the area. Others agreed. Marvel Hunt said that this plan isn't too different than the Port's 2006 Comp Plan. She provided a Port plan from 1974 which included a lot of improvements including tennis courts and a much larger area. She is concerned that this new plan will just lay dormant like past plans. Commissioner Scholfield reviewed the plan and said that actually quite a bit did come to fruition with 1974 plan, such as the brownfields were mitigated, the pier was constructed, forty additional parking stalls were added, the boat launch was installed, etc. Ms. Hunt said that there are several historical buildings along Byron Street alone and part of the Port's position should be to recognize and keep the history of Silverdale. Old Town Silverdale was Silverdale back in the day

and if the Port is not intentional about the history it is sure to be lost. She said that the Port should provide guidelines to property owners when remodeling and/or new construction in Old Town. The new building at the corner of Byron Street and Washington Avenue doesn't really fit with the Old Town feel. Commissioner Scholfield explained that the Port has no jurisdiction over what property owners do with their property. The County is responsible for that and they do have certain guidelines, such as new construction is not allowed to include parking visible from the streets. Ms. Hunt said that she just thinks that this comp plan should include a broader view of the historical area.

It was questioned why the Port purchased all the properties in the Old Town area. Joyce Merkel said that she worked as a County Planner for over thirty-two years and she remembers longtime Port Commissioner Harry Knapp purchasing several lots in Old Town. She wasn't even sure if the Port had a plan for all the properties. Commissioner Scholfield explained that the main purpose is to allow water access to the public. There is one privately-owned parcel along the east portion of Lowell Street adjacent to Port owned properties. If/when the Port purchases that property, plans can begin for that area, but until then those rentals provide a decent revenue stream.

Mark Hughes said that he likes the over-the-water work that is planned. He was fond of RFM's suggestion of making Old Town the front porch of Silverdale, but the plans reflect a large building that is taking up key open space and it doesn't really offer a front porch feel. It was explained that there will still be quite a bit of open space on the front side of the pump station building and since the building will be about fifty feet back from where it currently sits, it will open up that part of the waterfront. Ms. Merkel questioned the height of the building. It was explained that would not be determined until the design phase sometime in the future. Commissioner Aus said that at that time the old-time façade can also be determined. Commissioner Reese said that at this time we do not know the specifics. This is just the general concept.

Monica (via phone) said that she also is interested in seeing the Port draw the public to the Old Town area. Commissioner Scholfield said that an issue he is aware of for Old Town is that most of the businesses are closed on Sundays. When he has been asked by the public on a Sunday where they can get a bite to eat, he has had to direct them up to the mall area. Dave Montoure of Kettle Fish said that they are opened on Sundays. Commissioner Scholfield took note. Mr. Hughes asked what the draw was for the non-motorized boating community. He was surprised by the focus on non-motorized boating. Commissioner Scholfield said that the regattas that are held at the Port have drawn a lot of the public including people from out of town, but he has noticed that the regatta crowds seem to be self-sufficient by bringing their own refreshments, etc. as a lot of other venues don't offer any stores and/or restaurants. Commissioner Scholfield said that the draw is just to come down and sit on the beach and that is one reason the Port

plans to enhance the beachfront. Commissioner Reese added that the Port wants to make that area more user friendly. Mr. Havers said that the plan seems to be lacking other improvements as there is no non-boat related concrete plan and that's an issue. Ms. Hunt asked why, other than the Waterfront Park, would families want to bring their kids to this area.

The future of the Old Town Pub (OTP) was questioned. Ms. Merkel suggested a building similar in size to the OTP be built in that area and include an upstairs restaurant. The view is so stunning, it would surely be a draw. The downstairs could be retail. Commissioner Scholfield said that the building that houses the pump station may include a large open area on the upper floor with a commercial kitchen to accommodate community functions, weddings, receptions, etc. Mr. Hughes suggested having a stage on the top of that building. Phil Best said that it is intended to be a multi-use facility with the main floor housing the pump station and a shell house. Commissioner Scholfield said that he would refer to it more as an activity center rather than shell house so that as future needs change so can the use of the building. Ms. Merkel questioned parking. Commissioner Scholfield explained that the Port has about 100 parking stalls if you include the property along Lowell Street and permission may be granted from the Recreation Conservation Office (RCO) to use the boat trailer parking at times. Mr. Havers said that there was also discussion about working with the Central Kitsap School District (CKSD) to possibly use the Jenne-Wright parking area.

Phil Best explained that the CIP is aimed at projects that are inclined to receive RCO grant funding. The plan is wide ranged and generalized to avoid having to continually amend it. The plan is basically a five-year plan. Commissioner Reese added that RCO funding cannot be used for retail projects. Ms. Downen asked how the potential plan will affect the Port's existing tenants. Commissioner Scholfield said that he would hope the existing tenants could temporarily be relocated to other Port properties at the time of construction and then they would be offered first option to lease once construction is complete, but he doesn't see any of this happening anytime soon as it probably won't be included until the next comp plan update. Ms. Downen questioned if the OTP would remain as is for the next five years. Commissioner Scholfield said that he views it as a placeholder because the Port doesn't want to do anything too soon and lose the building's footprint if/when future construction is decided. Phil Best said that maybe the Port could discuss this with the County and possibly obtain an agreement with them so that the building can be taken down soon without losing anything for the future. Mr. Havers suggested the Port do just that. Commissioner Reese said that the Port would not be eligible for any kind of refund from the RCO if the building is torn down prior to the award of grant funding. Mr. Havers said that the building has a negative effect on the community as it sits. He sees it as money well spent to do it sooner rather than waiting for possible grant funding. Mr. Hughes agreed that if the public sees progress with the removal of a derelict building it sends a big positive message. The thought of waiting five years for something to possibly take place is a little disheartening. Commissioner Scholfield said that if the Board

agrees to move forward with the demo of the building the first item that will need to take place is removal of the asbestos.

Commissioner Scholfield explained to everyone that the land the pump station currently sits on will still be County land after it is moved from that area. The Port will have no authority on the future of that property. Ms. Hunt said that it is important for the Port, the County and the CKSD to form partnerships in an effort to work together for the betterment of the taxpayers. She doesn't see the necessary communication for that to happen and that is a concern. It was agreed that there are many players including the library, Old Town property owners/residents, Old Town business owners, etc. Mr. Hughes agreed partnerships are key. He gave an example of when the library was able to save over five million dollars by partnering with the CKSD.

Phil Best asked if the attendees thought the Port was at least on the right track? Mr. Hughes said that the more the plan was discussed, the more comfortable he became especially hearing that the building that houses the pump station can be used for many things. Ms. Hunt said that the Port needs to show something being done down here. She suggested the Commissioners widen their view other than just the waterfront. Commissioner Scholfield said that there is a requirement that any construction within 200 feet of the water must be water related. And currently all of the Port properties are rented. Ms. Hunt mentioned that maybe they shouldn't be so that the Port can move forward with other development.

Kay Wilson explained that she was a City Planner for Bremerton for many years. She said that they have constructed a stage on top of one of their pump stations. She isn't sure of how it turned out, but this shows it can be done. Gig Harbor's Skansie Brothers Park is similar with an observation area on top of the pump station. It seemed to be agreed that having an area to be used as an entertainment stage/observation deck would be a nice addition to the area. Ms. Hunt liked the idea of having the area a walkable community. Commissioner Scholfield said that once the County's Bayshore Drive/Washington Avenue project is complete the area will definitely be more walkable with the wider sidewalks and lighting, but that is about two to three years out. Ms. Hunt said that the public wants something sooner than that.

Mr. Hughes asked what is the next step. Phil Best said that Steve Rice with RFM is planning to provide the Port with a proposal for a preliminary design of the pump station building. The Port may need the support of the CAC members and community by writing letters to the County supporting demolishing the Old Town Pub building and requesting them to allow the Port to keep the current footprint for future use. If that is the case, they will be notified and asked to help.

Commissioner Scholfield asked if anyone in the group was adamantly against the plan. Ms. Merkel said that she just doesn't want anything too tall near the waterfront. She would like to have an unobstructed view for everyone to enjoy. She would rather the boat storage be built

to the west and thought that was discussed at one point. Phil Best explained that there were mixed reactions from the Department of Fish and Wildlife (DFW) having anything near the wetlands, even having a boardwalk within the wetlands was not favored.

Everyone was thanked for their support and input on this effort.

At 5:20PM this portion of the meeting ended.

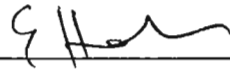
Phil explained that the Washington Public Ports Association (WPPA) is hosting a four hour Continuing Legal Education (CLE) session catered to attorneys representing Ports. It is scheduled for tomorrow in Tacoma and Phil asked if the Commissioners would like for him to attend. ***Phil Best was authorized to attend the WPPA sponsored CLE session at the Hotel Murano in Tacoma on November 19, 2019;*** Commissioner Reese looked up the agenda for the CLE via his cell phone. He questioned if the items presented would be beneficial to the Port. This was discussed. ***The motion carried*** (motion by Aus; second by Reese; unanimous).

Phil has been working on the permit for the paving of the alley between 9004 Washington Avenue and Silverdale Auto Works. The project covers approximately 11,000 square feet and requires a special permit that cost over \$4,000. It was decided that the area of paving should be reduced to just under 5,000 square feet. Phil will work on this and will be in communication with Lakeside Industries, LLC.


At 5:40PM the meeting adjourned (motion by Reese, second by Aus, unanimous).



Commissioner



Commissioner



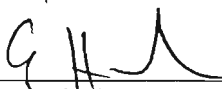
Commissioner

WAIVER OF NOTICE OF SPECIAL MEETING

The undersigned, Port Commissioners for the Port of Silverdale, hereby waive the requirement of notice in writing of the special meeting of the Port of Silverdale held on 11/18/2019, at 4:00PM Port of Silverdale office present at such meeting, and agrees to the conduct of the Port business as announced by the President in calling this meeting.



Commissioner



Commissioner



Commissioner

Port of Silverdale
Minutes of Regular Meeting
November 21, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Tim Knapp of TIKAR Services; John Bouck and Travis Bouck of Kitsap Sailing and Rowing Foundation (KSRF); Ron Easterday and Steve Rice of Rice Fergus Miller (RFM); Roy Sahali of Sahali Farms; Ken Adams; Carla Larson; Ron Perkowitz; and Corky and Bonnie Sunkel.

2. PLEDGE OF ALLEGIANCE

Commissioner Reese led the pledge of allegiance.

3. CONSENT AGENDA

The November 21, 2019 meeting agenda; October 17, 2019 Regular meeting minutes; October 23-25, 2019 Special meeting minutes; October 28, 2019 Kitsap All Ports meeting minutes; October 30, 2019 Special meeting minutes and November 18, 2019 Special meeting minutes were approved as submitted (motion by Reese; second by Aus; unanimous).

4. PUBLIC COMMENT –

4.1. Old Town Pub (OTP) – Dan Sullivan was scheduled on the agenda to discuss his interest in the future of the OTP, but he was not present.

4.2. Waterfront Center and Pump Station – a Letter of Proposal from RFM to provide architectural predesign services for the proposed Waterfront Center was reviewed. Commissioner Reese asked that an e-mail from Kay Wilson dated November 18, 2019, that was set to be addressed later in the meeting, be reviewed at this time. Ms. Wilson attended the recent Comprehensive Plan Update Special Meeting that was recently held.

She also served as a member on the Citizen's Advisory Committee (CAC). She arrived late to the Special Meeting so missed some of the discussion. She explains in the e-mail that she had thought that she was in support of the plan as it was presented, but upon reviewing the Executive Summary Plan more closely after the meeting she realized that the pump station building will also include storage for non-motorized boats. She is concerned that the size of the building will create much less open space and so would not be in favor of that. Commissioner Reese explained that since the Special Meeting he had a chance to go back and review the notes from the CAC meetings and realized that the request to leave public open space was a common thread within the meeting notes. He is concerned that the Commissioners are not hearing the public's wishes and/or concerns. Mr. Rice said that in the early phases of putting the concept use plan together the question was what should be done with the Port's waterfront properties. The comp use plan determines how land will be used and/or approached. The details of the building are not specified in the comp use plan. It's more of a placeholder. When the Port and the Central Kitsap School District (CKSD) met, keeping view access to the waterfront available to the public was a topic of discussion along with adding more retail businesses to the area. It was also agreed that the Port's limited space lessens the possibilities of what can be accomplished and they quickly realized the waterfront area will not be everything for everyone. When discussing the Lowell Street properties, it was thought that area may aim more towards hospitality and/or a possible educational partnership opportunity between the Port and CKSD as they are looking for an educational facility near the water, but again that would be for discussion at a later date. The Commissioners were clear that the priority of the comp plan should focus on the over-the-water facilities and immediate uplands. Now the Port and the County have entered into a

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Memorandum of Understanding (MOU) to move the pump station back off of the prime waterfront property, which will open that area up for public use. Mr. Rice said that once that building is moved back from the water it will be surprising how open it becomes. He said that there has been a lot of good discussion, but there are no details at this point. In order to move forward and apply for and make a case for grant funding the predesign is necessary. At that point the public will be included to give input. He said that if it's done right it could be a landmark facility for Silverdale and since it is next to the water it should have water dependent uses; otherwise, funding will not be granted. Commissioner Scholfield said that since Ms. Wilson missed the first portion of the meeting, she probably didn't hear that the building would have windows on the lower floor all the way around, so that it lessens any obstruction of the view. Mr. Rice said that there is an appropriate scale and that is something RFM would be studying and be asking for input. It wouldn't be a huge community center building that would require a lot of parking. It is thought it may accommodate up to 200 people. He said that he and Ron Easterday have been discussing the building and if the Port decides to hire RFM to provide the predesign services they plan to seek, as part of the project, the Leadership in Energy and Environmental Design (LEED), which is a green building rating system and is mandated by the State for projects that exceed \$5,000,000. It's a pilot project that has been around for about 20 years. The building would basically be a green facility and RFM may look at having the building run on wind and solar alone with no use of electrical. Lots of jurisdictions have incorporated it when using public funds. Mr. Easterday said that another thing that RFM would be completing is trying to determine if there will be any possible red flags. They will complete the due diligence that the RCO will be looking for in a viable project, including making sure the necessary permits

are in place and proof that the Port will have enough money to cover the matching funds to complete the project. Mr. Rice said that the chance to trade a pump station for a well-loved, well-used building is exciting. Commissioner Scholfield asked Phil if the Port would need to enter into a new contract with RFM. Phil said no because the way the original contract was written made it extensible. Mr. Rice said that once the money is secured then it usually goes out for bid and sometimes RFM is awarded the job and sometimes they aren't. The Proposal is set at a fixed fee totaling \$31,380 for the tasks laid out within the proposal. If there is something that comes up above and beyond that then it is charged at the hourly rate, which is also outlined within the proposal. Commissioner Reese questioned the timeline of the final predesign report. Mr. Rice said anywhere between fifteen and twenty weeks, with a six-month maximum. Mr. Rice and Mr. Easterday thanked the Commissioners for their time and consideration. ***It was agreed to accept RFM's Letter of Proposal dated November 20, 2019 Re: Waterfront Center & Pump Station #3 Predesign Report*** (motion by Aus; second by Reese; unanimous).

Roy Sahali questioned the use of the building. At this point it is unknown. Parking was discussed. Phil said that the planning phase gives the Port a chance to reach out to the RCO about possibly using the overflow vehicle/trailer lot periodically and showing them that the public will benefit from the new building. Commissioner Scholfield said that he hoped eventually the vehicle/trailer parking can be moved off of the waterfront.

Item 6.1. Use of Port Property application was moved up in the agenda as Ken Adams and Ron Perkowitz were in attendance. Mr. Adams had submitted the application in hopes to use the Port's parking lot at the corner of Byron Street and Washington Avenue on June 21, 2020 to hold a Father's Day Car Show. Mr. Perkowitz explained that they are both

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members of Oldtimers Northwest, Inc. Car Club and will have the required proof of insurance listing the Port as additionally insured. The club regularly sponsors car shows in and around the area. They do not plan to have food trucks, but would rather work with the local restaurants/eateries in the area and maybe offer coupons. If this works out, they would like to have it become an annual event. They anticipate 250 cars in the show and have been in contact with the County to close Byron street from McConnel to Washington. Commissioner Scholfield asked Tim if he thought the Port facilities could handle a possible large crowd. Tim said it shouldn't be a problem. Parking was discussed. Mr. Perkowitz said that he plans to talk to Ron Templeton about using his building's parking lot, since it will not be opened anyway. Commissioner Reese informed them that they will need to have someone stationed in the Port's vehicle/boat trailer lots to direct the public to park elsewhere if they do not have a boat trailer attached to their vehicle. Admission will be charged. Mr. Perkowitz said that the majority of the money raised goes back into the community. Commissioner Reese asked if the Port will need to get a waiver from the RCO for the use of the parking area. Phil suggested the Port begin talking to the RCO about periodically allowing the lots to be used for events. *It was agreed to allow the Oldtimers Northwest, Inc. to hold a Father's Day car show on June 21, 2020 from 7:00AM to 7:00PM at the Port's parking lot at the corner of Byron Street and Washington Avenue* (motion by Reese; second by Aus, unanimous). Mr. Adams and Mr. Perkowitz thanked the Commissioners and said that they will move forward with the event and keep the Port informed if anything changes.

5. UNFINISHED BUSINESS

5.1. Port Programs

a. Sailing – John Bouck reported that the regatta held three weeks ago was the big event

for the end of summer. It was a well-attended event and by far the largest of the fall sailing season with nearly 200 sailors coming all the way from Portland to the Canadian border and in between. The weather was beautiful, it could have used a little more wind, but it was great. Monica's Bakery and Kettle Fish both provided food for the sailors at a discount and to the non-sailors at the regular cost. Tim said that it was really beautiful to watch the event and seeing so many people enjoying the waterfront. Mr. Bouck said that they had winterized the motors at the end of 2018 and although have done a lot of volunteer work on all the equipment they have no receipts to submit for 2019. They are aware that the 2014 – 2018 Leasehold/Excise Tax repayment is required by the end of the year and that the 2019 Leasehold/Excise Tax will need to be paid in January 2020. Mr. Bouck reported that it's time to winterize the safety boats. One of the motors is having an issue, but it is still under warranty with Aqua Tech Marine Services in Bremerton. Since the Port will now be paying for the maintenance to the motorized boats, Mr. Bouck will find out if Aqua Tech has an account set up for the Port or if they will just bill the Port directly. The 2019 Sailing season is finished. The 2020 season will begin at the end of February. Commissioner Scholfield said that the safety checklist for the trailers is now available via the Port's website.

b. Rowing – the move to the waterside of the OTP has not happened yet. Phil reported that a security guard came into the Port office because he had found an unattended gas can near the boat launch restroom. He had remembered seeing another gas can stored in the plastic storage container on the dock, so he placed this gas can in the container. A couple days later an individual came into the Port office and showed Phil a picture of the storage container, where it was totally broken like it had fallen apart. Mr. Bouck said that it is actually the sailing club's

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storage container and they are aware of the problems with it, but they do not own any of the gas cans as they do not store their fuel in the storage container. Tim was tasked with removing the fuel from the dock. It was mentioned that the chase boat for CIR was tied to the dock for most of the season.

c. New Lease – reimbursing KSRF and CIR for the 2019 maintenance completed on the motorized boats was discussed. An e-mail dated November 12, 2019 from Steve Trunkey was reviewed. It included a list of all of the volunteer work that had been conducted to maintain the safety boats in 2019. Mr. Bouck said that most of the maintenance to the motors, besides all of the volunteer hours outlined in Mr. Trunkey's e-mail, was completed at the end of 2018. So, they do not have any receipts to remit for reimbursement. He said that they have spent a lot of money in 2019 for repair/maintenance to the sailboats. They realize that the leasehold excise tax needs to be paid back to the Port and they are working on it.

5.2. Port Facilities

a. Department of Natural Resources (DNR) – Phil e-mailed Don Olmsted the proposed updated outer-water boundary map. Mr. Olmsted responded saying that he didn't think the new boundaries would be a problem, as long as the adjacent property owners, County Parks Department and Public Works sign off on it.

b. Dredge – Phil recapped the teleconference that was held on November 8, 2019. Sound Action filed the appeal and had several concerns. An e-mail dated November 14, 2019 from Meg of Marine Surveys and Assessments (MSA) was reviewed. She explained that the appeals coordinator will take all of the information gathered and debrief with their Habitat Program Director and senior managers to decide if any changes will need to be made to the permit. This will take a few

weeks and she will keep the Port updated. Phil explained that the Department of Fish and Wildlife (DFW) will make the final decision. Phil has also requested that the start date be extended.

c. Sailboat (WN6823JF) – a daytime Safe Security officer found the boat to look a bit disheveled as if someone was again using it. Tim was tasked with removing everything from the boat. Phil will work on getting the necessary paperwork to put it up for sale.

d. Benches – a picture of Commissioner Aus sitting in a bench while in Leavenworth. The bench was similar in style to that of the Port's benches, but it had a slight curve to it that made it appear a bit more comfortable. Commissioner Aus asked if the Port would want to consider a different more comfortable design. This was discussed. It was generally agreed that the current Port bench design was adequate and some even suggested they are more comfortable than those in Leavenworth. Phil provided Tim with the Notice of Award and the contract for the bench project. They were both signed.

e. Port Rules and Regulations Policy – Nothing to report

f. Electrical Repair – it was questioned if a contract was necessary to hire Bainbridge Island Electric to complete the work on the damaged pedestal at the floating moorage facility. Last month the contract provided by Bainbridge Island Electric was approved, so that should be adequate. They will be contacted to determine what is needed to get on their schedule.

Commissioner Scholfield reported that the oil separator lid near the boat launch is failing.

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Mike made a repair to the lid several years ago, but that repair is starting to fail. Mike said that it is a critical safety issue that should be taken care of immediately. The current lid was stationed in the grass prior to the development of that area, so it is not rated to sustain vehicles of any kind. Tim reported that he was tasked with getting a general cost for the installation of a new lid and determined it would cost approximately \$13,000. He said that he could bill it as time and material. ***TIKAR Services was authorized to replace the lid of the oil separator near the boat launch on a time and material basis*** (motion by Reese; second by Aus; unanimous).

g. Sea Plane Crash Expenses – the certified return receipt letter was sent back to the Port as Return to Sender – Unclaimed. A bill was then generated addressed to Central Kitsap Fire and Rescue (CKFR) and hand delivered, since CKFR was the entity that requested the Port's assistance in removing the sea plane. Jeff Sorenson of CKFR called the Port office shortly after receiving the bill questioning why CKFR was being billed the costs. It was explained that after the owner of the sea plane didn't respond it was decided to send it to CKFR since they had requested the Port's assistance and it was a possible hazardous material cleanup situation. Mr. Sorenson said that he would have to look into it as he was unsure if CKFR would be agree to make payment. An e-mail dated November 10, 2019 from Mr. Gotschall, the owner of the sea plane, was reviewed. He acknowledged receiving the Port's initial bill for damages in the amount of \$2,359.34. He claims that the Port's machine operator caused \$6,500 in damages to his "boat" and attached pictures to

the e-mail. This was discussed and it was mentioned that those damages very well could have been caused by the actual crash into Dyes Inlet. Phil will review the laws related to such an incident.

h. Eagle Scout Life Jacket Loaner Station Project on Pier – an e-mail dated November 19, 2019 from Jack West was reviewed. He explained that he and his scout troop completed the construction of the life jacket loaner station and a picture was attached. He said that it will be installed sometime in the future, hopefully soon.

i. Pier – an e-mail dated October 18, 2019 from Lee of TIKAR Services was reviewed. Lee was reporting that the rub rail near the gangway had several issues and he attached several pictures. There were still two pieces of wood attached under the rub rail with screws protruding out that needed to be removed. These are the same screws that damaged a boat last month. They were left behind by Olympic Outdoor Center's (OOC) kayak assist arm. The wood was used to reinforce the arm to the dock, but when the arm was removed, it seems the wood was just broken off instead of unscrewed, which left a portion of the wood in tact and the screws protruding. TIKAR's e-mail states that the damage seems to be caused by improper installation and removal of signs and screws by the kayak vendor at the end of the pier. On September 25th an e-mail was sent to John Kuntz of OOC informing him that a boat was damaged by the protruding screws and he was requested to remove them immediately. He had obviously not done that by October 18th when Lee noticed the issues. TIKAR's e-mail was forwarded to Mr. Kuntz on October 22nd

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and it was explained to him that OOC would be responsible to reimburse the Port for any and all costs associated with repairing the damage. It was also requested that in the future OOC ask permission from the Port prior to installing anything into the Port's pier, dock, floating docks, railings, etc. Mr. Kuntz came into the Port office a day or two after the e-mail was sent. He said that he removed the screws and the broken wood. Nothing was in that area that would damage a boat. He disagreed with TIKAR's assessment that OOC damaged the rub rail further west from where the kayak assist arm was located. He asked that future lease negotiations be discussed outside of a public meeting. Commissioner Reese said that he would be willing to meet with Mr. Kuntz in the future.

5.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the sewer line connection to the existing outlet is tabled until the County's Bayshore/Washington Improvement Project is underway.

b. 9004 Washington Avenue/DRC – an e-mail dated October 21, 2019 from Mary Hancock of the DRC was reviewed. She explains that the DRC will not be pursuing interior renovations as previously requested, but would like new flooring in the kitchen and main floor restroom as well as installation of exhaust fans in both restrooms. It was agreed these items are basically maintenance and should be completed. Mike will work with Ms. Hancock to determine the best time to start. DRC's lease is through the end of 2019. A spreadsheet showing the per square foot rental fees was reviewed. It was noticed that the square footage rate was lower than the other

rentals. It was agreed that the rent for the 9004 Washington Avenue property should be increased by ten percent ($\$1280 + \$128 = \$1408$).

It was also noticed that the rental fees based on the square footage for 3295 Lowell were also significantly lower than the other rentals. It was explained that the rent did increase \$100 last year in an effort to make it closer to the others, but it was still low. It was agreed that the rent should be raised by ten percent ($\$1475 + 147 = \1622.50) Commissioner Scholfield suggested the tenants, Tina Abeel and David Holman, be notified as they had wanted to renegotiate their lease at the beginning of the year, but decided to hold off since the lease was already in place. Ms. Abeel and Mr. Holman will be informed of the rental increase and asked to attend the December Port meeting to further discuss.

c. 3215 Lowell Street, Suite 161/Sefton Enterprises – notice of past due rent for October and November was send via e-mail. Contact will be attempted via telephone.

An e-mail dated November 8, 2019 from attorney Bill Broughton, who has an office in the Templeton building which neighbors the 3215 Lowell Street building, was received. Mr. Broughton said that the trees between the two buildings needed to be limbed in an effort to alleviate the pest problem they were having in their building. He asked permission from the Port to allow his "tree guy" to cut to the trunks of trees. It was agreed that Mr. Broughton could cut the trees to the trunk on Port property. The tree guy and his crew ended up driving heavy equipment on the water-side lawn of the Port's property, damaging the lawn.

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Mr. Broughton was asked to have it repaired as soon as possible in which he replied he would.

d. Painting contract – the job is complete and the work has been found to be satisfactory. Payment in the amount of \$10,022 is set to be authorized at tonight's meeting. There was discussion about the failure to complete the job before the October 31, 2019 deadline. *It was agreed to waive the liquidated damages outlined in Item 2 of contract #CR2019-02 since the weather conditions were a contributing factor in the delay of completion* (motion by Reese; second by Aus; unanimous).

e. Leases are through 2019 – it was agreed that all but the two leases (9004 Washington and 3295 Lowell) will stay at the same amount and terms for 2020.

5.4. Easement – in an e-mail dated November 21, 2019 Suzie Pride of Kitsap County Public Works explains that she will be processing the easement at the width of 15 feet in an effort to ensure the Port is able to maintain a continuity of size with the other easements in the area.

5.5. Paving of the alley – Phil has been working on obtaining the appropriate permits, but has determined that due to the nearly 11,000 square feet of space to be covered it will require an additional permit that costs over \$4,000. Phil has since talked with Commissioner Scholfield, who suggested the project be reduced to and approximate 5,000 square foot surface area; starting at the alley between 9004 Washington and Silverdale Auto Works heading east just rounding the corner of 3255 Lowell Street. Commissioner Scholfield said that the alleyway between the 3255 and

3215 Lowell Street properties should be vacated before asphaltting it anyway. Phil questioned if that was wise as it may hinder the ability to use it as a passthrough. He will look into that. Commissioner Reese said that the Port needs to work with Lakeside Industries, the contractor that was awarded the job, because they have already provided a performance bond and necessary insurance paperwork. Phil will inform them that the scope of the contract is being reduced to approximately 5,000 square feet.

5.6. Comprehensive Plan – Commissioner Reese will be providing a presentation during the Central Kitsap Community Council (CKCC) meeting scheduled for December 4th at the Silverdale Beach Hotel at 5:00PM. Commissioner Reese said that he is concerned that the others aren't listening to what the public is requesting. He reviewed the notes from all of the Community Advisory Committee meetings and the common thread was that they didn't want to lose the space and/or the view. It was agreed that a viewing platform on the top of the building will be a significant view, but Commissioner Reese thought that the size of that building parallel to the water will close it off making the water less viewable from the ground. Tim said that with the pump station moved back off of the waterfront, it is going to open up that area nicely and provide more public access to the water. Commissioner Reese said that he just wants to make sure that they are hearing the community.

Commissioner Scholfield said that an employee at 3425 Byron told him that she nearly was hit by a car driving way too fast in the Port's vehicle/boat trailer overflow lot. She asked if the boat launch handling pier was

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going to be spread out in the lot as in years past to help alleviate the erratic driving in that area.

5.7. Grant Funding – nothing to report.

5.8. Pump Station 3 – already covered under Public Comment.

5.9. Benefits – Commissioners Scholfield and Reese and administrator Haaland's insurance covers a portion of medical transport so Airlift Northwest of Washington would work as a supplemental air transport insurance. Commissioner Aus plans to verify with his insurance that it covers medical transport. This will continue on the agenda.

5.10. 2020 Final Budget was reviewed. *It was agreed to adopt Resolution 2019-02 Adoption of the 2020 Final Budget* (motion by Aus; second by Reese; unanimous).

Resolution 2019-03 was reviewed. Commissioner Reese questioned the percentage increase from last year's levy. It was explained that it is a total \$3,392.00, which is a .545 percent increase over last year's levy amount. *It was agreed to adopt Resolution 2019-03 Levy Limit Increase* (motion by Reese; second by Aus; unanimous).

5.11. State Audit – during the Special Meeting held on October 30, 2019 with Assistant State Auditor Tammy Lane and Assistant State Audit Manager Cheryl Friesen the question of a Port contractor using the Port's line-of-credit was discussed. They weren't aware of any specific statute not allowing it, but did caution it and suggested legal counsel be asked to look into it further. Since then an e-mail dated November 1, 2019 from Ms. Lane was received and she

suggested the question be submitted to the State Auditor's help desk. Upon further discussion amongst themselves they agreed that it could be considered lending of credit, which is not allowed. TIKAR has been allowed the use of the Port's line of credit for many years with no issues. This has allowed the Port to not pay the twenty percent markup costs that would otherwise be charged. Tim said that they can begin just paying for items outright and billing the Port and include the receipts. He said that they will follow the direction of the Port with whichever way is decided. He said, in fact many businesses he purchases materials from offer TIKAR a decent discount which would offset the twenty percent markup to the Port. Phil said that the cleanest thing is for Tim to start purchasing the material directly and bill the Port for what is used. Tim agreed.

5.12. Purchased Services contracts – Phil will be working on this.

6. NEW BUSINESS

6.1. Use of Port Property application was already covered under Public Comment.

6.2. Water leak in the alley behind the Port office building was discovered by Silverdale Water District. The Port's landlord repaired the leak. The Port may receive a larger than normal water bill for the office next billing period.

6.3. Silverdale Marina – in an e-mail dated November 8, 2019 Susan Johnson asked if the Port will ever offer long-term moorage. Since there is no breakwater there is no protection for a long-term moorage facility. Commissioner Scholfield said that the Inter-Agency

Committee (IAC) now known as the RCO partially funded the pier and a transient moorage facility not a permanent moorage facility, so that is also now an issue.

6.4. Bri Communications an e-mail dated November 5, 2019 from Paul Sherwin was reviewed. The Boater's Guide is being updated. *It was agreed to advertise in the Boater's Guide at the \$1460 rate* (motion by Aus; second by Reese; unanimous).

6.5. County Commissioner Ed Wolfe reached out to Commissioner Reese to meet on November 25th. Commissioner Reese said that he thinks it is because he was recently elected – congratulations Commissioner Reese.

7. SAFETY/SECURITY

7.1. A client of the tenant at 3425 NW Byron Street/Kitsap Art fell in the parking lot outside of the building. The client's child was pulling her to their car and that is what made her fall. Enduris as a precaution. Commissioner Scholfield went down to inspect the area and didn't see any tripping hazards. He did notice that an outside light wasn't working properly and determined the tenant removed the lightbulb to use the outlet for Christmas lights. Mike will convert that light to an LED.

Commissioner Scholfield said that two of the parking barricades in that area are going to be moved out a few feet to free up room for the fence replacement to accommodate CIR.

Removing the Old Town Pub was discussed. It was agreed that if it is officially decided to tear it down the asbestos would first need to be removed as well as the compressor, which could possibly be sold.

8. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$66,246.40, checks numbering 12245 through 12277 and Electronic Funds Transfer 2019-11 to the U.S. Treasury in the amount of \$2,277.10, were approved (motion by Aus; second by Reese; unanimous).

9. EXECUTIVE SESSION – None

Ms. Larson said that the tractor rides for the upcoming tree lighting festival were almost discontinued due to the high insurance costs, but they have since obtained a different/lower quote.

Tim said that Monica Downen from the bakery asked him if the fire pit could be set up for the lighted boat parade scheduled for December 21st. Tim offered to set it up for Ms. Downen, but told her she needs to discuss it with the Port. The Commissioners seemed to be positive about it.

10. ADJOURN

At 10:15PM the meeting adjourned (motion by Aus; second by Reese; unanimous).

Approved:



Commissioner



Commissioner



Commissioner

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November 21, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Tim Knapp of TIKAR Services; John Bouck and Travis Bouck of Kitsap Sailing and Rowing Foundation (KSRF); Ron Easterday and Steve Rice of Rice Fergus Miller (RFM); Roy Sahali of Sahali Farms; Ken Adams; Carla Larson; Ron Perkowitz; and Corky and Bonnie Sunkel.

2. PLEDGE OF ALLEGIANCE

Commissioner Reese led the pledge of allegiance.

3. CONSENT AGENDA

The November 21, 2019 meeting agenda; October 17, 2019 Regular meeting minutes; October 23-25, 2019 Special meeting minutes; October 28, 2019 Kitsap All Ports meeting minutes; October 30, 2019 Special meeting minutes and November 18, 2019 Special meeting minutes were approved as submitted (motion by Reese; second by Aus; unanimous).

4. PUBLIC COMMENT –

4.1. Old Town Pub (OTP) – Dan Sullivan was scheduled on the agenda to discuss his interest in the future of the OTP, but he was not present.

4.2. Waterfront Center and Pump Station – a Letter of Proposal from RFM to provide architectural predesign services for the proposed Waterfront Center was reviewed. Commissioner Reese asked that an e-mail from Kay Wilson dated November 18, 2019, that was set to be addressed later in the meeting, be reviewed at this time. Ms. Wilson attended the recent Comprehensive Plan Update Special Meeting that was recently held.

She also served as a member on the Citizen's Advisory Committee (CAC). She arrived late to the Special Meeting so missed some of the discussion. She explains in the e-mail that she had thought that she was in support of the plan as it was presented, but upon reviewing the Executive Summary Plan more closely after the meeting she realized that the pump station building will also include storage for non-motorized boats. She is concerned that the size of the building will create much less open space and so would not be in favor of that. Commissioner Reese explained that since the Special Meeting he had a chance to go back and review the notes from the CAC meetings and realized that the request to leave public open space was a common thread within the meeting notes. He is concerned that the Commissioners are not hearing the public's wishes and/or concerns. Mr. Rice said that in the early phases of putting the concept use plan together the question was what should be done with the Port's waterfront properties. The comp use plan determines how land will be used and/or approached. The details of the building are not specified in the comp use plan. It's more of a placeholder. When the Port and the Central Kitsap School District (CKSD) met, keeping view access to the waterfront available to the public was a topic of discussion along with adding more retail businesses to the area. It was also agreed that the Port's limited space lessens the possibilities of what can be accomplished and they quickly realized the waterfront area will not be everything for everyone. When discussing the Lowell Street properties, it was thought that area may aim more towards hospitality and/or a possible educational partnership opportunity between the Port and CKSD as they are looking for an educational facility near the water, but again that would be for discussion at a later date. The Commissioners were clear that the priority of the comp plan should focus on the over-the-water facilities and immediate uplands. Now the Port and the County have entered into a

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Memorandum of Understanding (MOU) to move the pump station back off of the prime waterfront property, which will open that area up for public use. Mr. Rice said that once that building is moved back from the water it will be surprising how open it becomes. He said that there has been a lot of good discussion, but there are no details at this point. In order to move forward and apply for and make a case for grant funding the predesign is necessary. At that point the public will be included to give input. He said that if it's done right it could be a landmark facility for Silverdale and since it is next to the water it should have water dependent uses; otherwise, funding will not be granted. Commissioner Scholfield said that since Ms. Wilson missed the first portion of the meeting, she probably didn't hear that the building would have windows on the lower floor all the way around, so that it lessens any obstruction of the view. Mr. Rice said that there is an appropriate scale and that is something RFM would be studying and be asking for input. It wouldn't be a huge community center building that would require a lot of parking. It is thought it may accommodate up to 200 people. He said that he and Ron Easterday have been discussing the building and if the Port decides to hire RFM to provide the predesign services they plan to seek, as part of the project, the Leadership in Energy and Environmental Design (LEED), which is a green building rating system and is mandated by the State for projects that exceed \$5,000,000. It's a pilot project that has been around for about 20 years. The building would basically be a green facility and RFM may look at having the building run on wind and solar alone with no use of electrical. Lots of jurisdictions have incorporated it when using public funds. Mr. Easterday said that another thing that RFM would be completing is trying to determine if there will be any possible red flags. They will complete the due diligence that the RCO will be looking for in a viable project, including making sure the necessary permits

are in place and proof that the Port will have enough money to cover the matching funds to complete the project. Mr. Rice said that the chance to trade a pump station for a well-loved, well-used building is exciting. Commissioner Scholfield asked Phil if the Port would need to enter into a new contract with RFM. Phil said no because the way the original contract was written made it extensible. Mr. Rice said that once the money is secured then it usually goes out for bid and sometimes RFM is awarded the job and sometimes they aren't. The Proposal is set at a fixed fee totaling \$31,380 for the tasks laid out within the proposal. If there is something that comes up above and beyond that then it is charged at the hourly rate, which is also outlined within the proposal. Commissioner Reese questioned the timeline of the final predesign report. Mr. Rice said anywhere between fifteen and twenty weeks, with a six-month maximum. Mr. Rice and Mr. Easterday thanked the Commissioners for their time and consideration. *It was agreed to accept RFM's Letter of Proposal dated November 20, 2019 Re: Waterfront Center & Pump Station #3 Predesign Report* (motion by Aus; second by Reese; unanimous).

Roy Sahali questioned the use of the building. At this point it is unknown. Parking was discussed. Phil said that the planning phase gives the Port a chance to reach out to the RCO about possibly using the overflow vehicle/trailer lot periodically and showing them that the public will benefit from the new building. Commissioner Scholfield said that he hoped eventually the vehicle/trailer parking can be moved off of the waterfront.

Item 6.1. Use of Port Property application was moved up in the agenda as Ken Adams and Ron Perkowitz were in attendance. Mr. Adams had submitted the application in hopes to use the Port's parking lot at the corner of Byron Street and Washington Avenue on June 21, 2020 to hold a Father's Day Car Show. Mr. Perkowitz explained that they are both

members of Oldtimers Northwest, Inc. Car Club and will have the required proof of insurance listing the Port as additionally insured. The club regularly sponsors car shows in and around the area. They do not plan to have food trucks, but would rather work with the local restaurants/eateries in the area and maybe offer coupons. If this works out, they would like to have it become an annual event. They anticipate 250 cars in the show and have been in contact with the County to close Byron street from McConnel to Washington. Commissioner Scholfield asked Tim if he thought the Port facilities could handle a possible large crowd. Tim said it shouldn't be a problem. Parking was discussed. Mr. Perkowitz said that he plans to talk to Ron Templeton about using his building's parking lot, since it will not be opened anyway. Commissioner Reese informed them that they will need to have someone stationed in the Port's vehicle/boat trailer lots to direct the public to park elsewhere if they do not have a boat trailer attached to their vehicle. Admission will be charged. Mr. Perkowitz said that the majority of the money raised goes back into the community. Commissioner Reese asked if the Port will need to get a waiver from the RCO for the use of the parking area. Phil suggested the Port begin talking to the RCO about periodically allowing the lots to be used for events. *It was agreed to allow the Oldtimers Northwest, Inc. to hold a Father's Day car show on June 21, 2020 from 7:00AM to 7:00PM at the Port's parking lot at the corner of Byron Street and Washington Avenue* (motion by Reese; second by Aus, unanimous). Mr. Adams and Mr. Perkowitz thanked the Commissioners and said that they will move forward with the event and keep the Port informed if anything changes.

5. UNFINISHED BUSINESS

5.1. Port Programs

a. Sailing – John Bouck reported that the regatta held three weeks ago was the big event

for the end of summer. It was a well-attended event and by far the largest of the fall sailing season with nearly 200 sailors coming all the way from Portland to the Canadian border and in between. The weather was beautiful, it could have used a little more wind, but it was great. Monica's Bakery and Kettle Fish both provided food for the sailors at a discount and to the non-sailors at the regular cost. Tim said that it was really beautiful to watch the event and seeing so many people enjoying the waterfront. Mr. Bouck said that they had winterized the motors at the end of 2018 and although have done a lot of volunteer work on all the equipment they have no receipts to submit for 2019. They are aware that the 2014 – 2018 Leasehold/Excise Tax repayment is required by the end of the year and that the 2019 Leasehold/Excise Tax will need to be paid in January 2020. Mr. Bouck reported that it's time to winterize the safety boats. One of the motors is having an issue, but it is still under warranty with Aqua Tech Marine Services in Bremerton. Since the Port will now be paying for the maintenance to the motorized boats, Mr. Bouck will find out if Aqua Tech has an account set up for the Port or if they will just bill the Port directly. The 2019 Sailing season is finished. The 2020 season will begin at the end of February. Commissioner Scholfield said that the safety checklist for the trailers is now available via the Port's website.

b. Rowing – the move to the waterside of the OTP has not happened yet. Phil reported that a security guard came into the Port office because he had found an unattended gas can near the boat launch restroom. He had remembered seeing another gas can stored in the plastic storage container on the dock, so he placed this gas can in the container. A couple days later an individual came into the Port office and showed Phil a picture of the storage container, where it was totally broken like it had fallen apart. Mr. Bouck said that it is actually the sailing club's

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storage container and they are aware of the problems with it, but they do not own any of the gas cans as they do not store their fuel in the storage container. Tim was tasked with removing the fuel from the dock. It was mentioned that the chase boat for CIR was tied to the dock for most of the season.

c. New Lease – reimbursing KSRF and CIR for the 2019 maintenance completed on the motorized boats was discussed. An e-mail dated November 12, 2019 from Steve Trunkey was reviewed. It included a list of all of the volunteer work that had been conducted to maintain the safety boats in 2019. Mr. Bouck said that most of the maintenance to the motors, besides all of the volunteer hours outlined in Mr. Trunkey's e-mail, was completed at the end of 2018. So, they do not have any receipts to remit for reimbursement. He said that they have spent a lot of money in 2019 for repair/maintenance to the sailboats. They realize that the leasehold excise tax needs to be paid back to the Port and they are working on it.

5.2. Port Facilities

a. Department of Natural Resources (DNR) – Phil e-mailed Don Olmsted the proposed updated outer-water boundary map. Mr. Olmsted responded saying that he didn't think the new boundaries would be a problem, as long as the adjacent property owners, County Parks Department and Public Works sign off on it.

b. Dredge – Phil recapped the teleconference that was held on November 8, 2019. Sound Action filed the appeal and had several concerns. An e-mail dated November 14, 2019 from Meg of Marine Surveys and Assessments (MSA) was reviewed. She explained that the appeals coordinator will take all of the information gathered and debrief with their Habitat Program Director and senior managers to decide if any changes will need to be made to the permit. This will take a few

weeks and she will keep the Port updated. Phil explained that the Department of Fish and Wildlife (DFW) will make the final decision. Phil has also requested that the start date be extended.

c. Sailboat (WN6823JF) – a daytime Safe Security officer found the boat to look a bit disheveled as if someone was again using it. Tim was tasked with removing everything from the boat. Phil will work on getting the necessary paperwork to put it up for sale.

d. Benches – a picture of Commissioner Aus sitting in a bench while in Leavenworth. The bench was similar in style to that of the Port's benches, but it had a slight curve to it that made it appear a bit more comfortable. Commissioner Aus asked if the Port would want to consider a different more comfortable design. This was discussed. It was generally agreed that the current Port bench design was adequate and some even suggested they are more comfortable than those in Leavenworth. Phil provided Tim with the Notice of Award and the contract for the bench project. They were both signed.

e. Port Rules and Regulations Policy – Nothing to report

f. Electrical Repair – it was questioned if a contract was necessary to hire Bainbridge Island Electric to complete the work on the damaged pedestal at the floating moorage facility. Last month the contract provided by Bainbridge Island Electric was approved, so that should be adequate. They will be contacted to determine what is needed to get on their schedule.

Commissioner Scholfield reported that the oil separator lid near the boat launch is failing.

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Mike made a repair to the lid several years ago, but that repair is starting to fail. Mike said that it is a critical safety issue that should be taken care of immediately. The current lid was stationed in the grass prior to the development of that area, so it is not rated to sustain vehicles of any kind. Tim reported that he was tasked with getting a general cost for the installation of a new lid and determined it would cost approximately \$13,000. He said that he could bill it as time and material. ***TIKAR Services was authorized to replace the lid of the oil separator near the boat launch on a time and material basis*** (motion by Reese; second by Aus; unanimous).

g. Sea Plane Crash Expenses – the certified return receipt letter was sent back to the Port as Return to Sender – Unclaimed. A bill was then generated addressed to Central Kitsap Fire and Rescue (CKFR) and hand delivered, since CKFR was the entity that requested the Port's assistance in removing the sea plane. Jeff Sorenson of CKFR called the Port office shortly after receiving the bill questioning why CKFR was being billed the costs. It was explained that after the owner of the sea plane didn't respond it was decided to send it to CKFR since they had requested the Port's assistance and it was a possible hazardous material cleanup situation. Mr. Sorenson said that he would have to look into it as he was unsure if CKFR would be agree to make payment. An e-mail dated November 10, 2019 from Mr. Gotschall, the owner of the sea plane, was reviewed. He acknowledged receiving the Port's initial bill for damages in the amount of \$2,359.34. He claims that the Port's machine operator caused \$6,500 in damages to his "boat" and attached pictures to

the e-mail. This was discussed and it was mentioned that those damages very well could have been caused by the actual crash into Dyes Inlet. Phil will review the laws related to such an incident.

h. Eagle Scout Life Jacket Loaner Station Project on Pier – an e-mail dated November 19, 2019 from Jack West was reviewed. He explained that he and his scout troop completed the construction of the life jacket loaner station and a picture was attached. He said that it will be installed sometime in the future, hopefully soon.

i. Pier – an e-mail dated October 18, 2019 from Lee of TIKAR Services was reviewed. Lee was reporting that the rub rail near the gangway had several issues and he attached several pictures. There were still two pieces of wood attached under the rub rail with screws protruding out that needed to be removed. These are the same screws that damaged a boat last month. They were left behind by Olympic Outdoor Center's (OOC) kayak assist arm. The wood was used to reinforce the arm to the dock, but when the arm was removed, it seems the wood was just broken off instead of unscrewed, which left a portion of the wood in tact and the screws protruding. TIKAR's e-mail states that the damage seems to be caused by improper installation and removal of signs and screws by the kayak vendor at the end of the pier. On September 25th an e-mail was sent to John Kuntz of OOC informing him that a boat was damaged by the protruding screws and he was requested to remove them immediately. He had obviously not done that by October 18th when Lee noticed the issues. TIKAR's e-mail was forwarded to Mr. Kuntz on October 22nd

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and it was explained to him that OOC would be responsible to reimburse the Port for any and all costs associated with repairing the damage. It was also requested that in the future OOC ask permission from the Port prior to installing anything into the Port's pier, dock, floating docks, railings, etc. Mr. Kuntz came into the Port office a day or two after the e-mail was sent. He said that he removed the screws and the broken wood. Nothing was in that area that would damage a boat. He disagreed with TIKAR's assessment that OOC damaged the rub rail further west from where the kayak assist arm was located. He asked that future lease negotiations be discussed outside of a public meeting. Commissioner Reese said that he would be willing to meet with Mr. Kuntz in the future.

5.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the sewer line connection to the existing outlet is tabled until the County's Bayshore/Washington Improvement Project is underway.

b. 9004 Washington Avenue/DRC – an e-mail dated October 21, 2019 from Mary Hancock of the DRC was reviewed. She explains that the DRC will not be pursuing interior renovations as previously requested, but would like new flooring in the kitchen and main floor restroom as well as installation of exhaust fans in both restrooms. It was agreed these items are basically maintenance and should be completed. Mike will work with Ms. Hancock to determine the best time to start. DRC's lease is through the end of 2019. A spreadsheet showing the per square foot rental fees was reviewed. It was noticed that the square footage rate was lower than the other

rentals. It was agreed that the rent for the 9004 Washington Avenue property should be increased by ten percent ($\$1280 + \$128 = \$1408$).

It was also noticed that the rental fees based on the square footage for 3295 Lowell were also significantly lower than the other rentals. It was explained that the rent did increase \$100 last year in an effort to make it closer to the others, but it was still low. It was agreed that the rent should be raised by ten percent ($\$1475 + 147 = \1622.50) Commissioner Scholfield suggested the tenants, Tina Abeel and David Holman, be notified as they had wanted to renegotiate their lease at the beginning of the year, but decided to hold off since the lease was already in place. Ms. Abeel and Mr. Holman will be informed of the rental increase and asked to attend the December Port meeting to further discuss.

c. 3215 Lowell Street, Suite 161/Sefton Enterprises – notice of past due rent for October and November was sent via e-mail. Contact will be attempted via telephone.

An e-mail dated November 8, 2019 from attorney Bill Broughton, who has an office in the Templeton building which neighbors the 3215 Lowell Street building, was received. Mr. Broughton said that the trees between the two buildings needed to be limbed in an effort to alleviate the pest problem they were having in their building. He asked permission from the Port to allow his "tree guy" to cut to the trunks of trees. It was agreed that Mr. Broughton could cut the trees to the trunk on Port property. The tree guy and his crew ended up driving heavy equipment on the water-side lawn of the Port's property, damaging the lawn.

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Mr. Broughton was asked to have it repaired as soon as possible in which he replied he would.

d. Painting contract – the job is complete and the work has been found to be satisfactory. Payment in the amount of \$10,022 is set to be authorized at tonight's meeting. There was discussion about the failure to complete the job before the October 31, 2019 deadline. *It was agreed to waive the liquidated damages outlined in Item 2 of contract #CR2019-02 since the weather conditions were a contributing factor in the delay of completion* (motion by Reese; second by Aus; unanimous).

e. Leases are through 2019 – it was agreed that all but the two leases (9004 Washington and 3295 Lowell) will stay at the same amount and terms for 2020.

5.4. Easement – in an e-mail dated November 21, 2019 Suzie Pride of Kitsap County Public Works explains that she will be processing the easement at the width of 15 feet in an effort to ensure the Port is able to maintain a continuity of size with the other easements in the area.

5.5. Paving of the alley – Phil has been working on obtaining the appropriate permits, but has determined that due to the nearly 11,000 square feet of space to be covered it will require an additional permit that costs over \$4,000. Phil has since talked with Commissioner Scholfield, who suggested the project be reduced to and approximate 5,000 square foot surface area; starting at the alley between 9004 Washington and Silverdale Auto Works heading east just rounding the corner of 3255 Lowell Street. Commissioner Scholfield said that the alleyway between the 3255 and

3215 Lowell Street properties should be vacated before asphaltting it anyway. Phil questioned if that was wise as it may hinder the ability to use it as a passthrough. He will look into that. Commissioner Reese said that the Port needs to work with Lakeside Industries, the contractor that was awarded the job, because they have already provided a performance bond and necessary insurance paperwork. Phil will inform them that the scope of the contract is being reduced to approximately 5,000 square feet.

5.6. Comprehensive Plan – Commissioner Reese will be providing a presentation during the Central Kitsap Community Council (CKCC) meeting scheduled for December 4th at the Silverdale Beach Hotel at 5:00PM. Commissioner Reese said that he is concerned that the others aren't listening to what the public is requesting. He reviewed the notes from all of the Community Advisory Committee meetings and the common thread was that they didn't want to lose the space and/or the view. It was agreed that a viewing platform on the top of the building will be a significant view, but Commissioner Reese thought that the size of that building parallel to the water will close it off making the water less viewable from the ground. Tim said that with the pump station moved back off of the waterfront, it is going to open up that area nicely and provide more public access to the water. Commissioner Reese said that he just wants to make sure that they are hearing the community.

Commissioner Scholfield said that an employee at 3425 Byron told him that she nearly was hit by a car driving way too fast in the Port's vehicle/boat trailer overflow lot. She asked if the boat launch handling pier was

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going to be spread out in the lot as in years past to help alleviate the erratic driving in that area.

5.7. Grant Funding – nothing to report.

5.8. Pump Station 3 – already covered under Public Comment.

5.9. Benefits – Commissioners Scholfield and Reese and administrator Haaland's insurance covers a portion of medical transport so Airlift Northwest of Washington would work as a supplemental air transport insurance. Commissioner Aus plans to verify with his insurance that it covers medical transport. This will continue on the agenda.

5.10. 2020 Final Budget was reviewed. *It was agreed to adopt Resolution 2019-02 Adoption of the 2020 Final Budget* (motion by Aus; second by Reese; unanimous).

Resolution 2019-03 was reviewed. Commissioner Reese questioned the percentage increase from last year's levy. It was explained that it is a total \$3,392.00, which is a .545 percent increase over last year's levy amount. *It was agreed to adopt Resolution 2019-03 Levy Limit Increase* (motion by Reese; second by Aus; unanimous).

5.11. State Audit – during the Special Meeting held on October 30, 2019 with Assistant State Auditor Tammy Lane and Assistant State Audit Manager Cheryl Friesen the question of a Port contractor using the Port's line-of-credit was discussed. They weren't aware of any specific statute not allowing it, but did caution it and suggested legal counsel be asked to look into it further. Since then an e-mail dated November 1, 2019 from Ms. Lane was received and she

suggested the question be submitted to the State Auditor's help desk. Upon further discussion amongst themselves they agreed that it could be considered lending of credit, which is not allowed. TIKAR has been allowed the use of the Port's line of credit for many years with no issues. This has allowed the Port to not pay the twenty percent markup costs that would otherwise be charged. Tim said that they can begin just paying for items outright and billing the Port and include the receipts. He said that they will follow the direction of the Port with whichever way is decided. He said, in fact many businesses he purchases materials from offer TIKAR a decent discount which would offset the twenty percent markup to the Port. Phil said that the cleanest thing is for Tim to start purchasing the material directly and bill the Port for what is used. Tim agreed.

5.12. Purchased Services contracts – Phil will be working on this.

6. NEW BUSINESS

6.1. Use of Port Property application was already covered under Public Comment.

6.2. Water leak in the alley behind the Port office building was discovered by Silverdale Water District. The Port's landlord repaired the leak. The Port may receive a larger than normal water bill for the office next billing period.

6.3. Silverdale Marina – in an e-mail dated November 8, 2019 Susan Johnson asked if the Port will ever offer long-term moorage. Since there is no breakwater there is no protection for a long-term moorage facility. Commissioner Scholfield said that the Inter-Agency

Committee (IAC) now known as the RCO partially funded the pier and a transient moorage facility not a permanent moorage facility, so that is also now an issue.

6.4. Bri Communications an e-mail dated November 5, 2019 from Paul Sherwin was reviewed. The Boater's Guide is being updated. *It was agreed to advertise in the Boater's Guide at the \$1460 rate* (motion by Aus; second by Reese; unanimous).

6.5. County Commissioner Ed Wolfe reached out to Commissioner Reese to meet on November 25th. Commissioner Reese said that he thinks it is because he was recently elected – congratulations Commissioner Reese.

7. SAFETY/SECURITY

7.1. A client of the tenant at 3425 NW Byron Street/Kitsap Art fell in the parking lot outside of the building. The client's child was pulling her to their car and that is what made her fall. Enduris as a precaution. Commissioner Scholfield went down to inspect the area and didn't see any tripping hazards. He did notice that an outside light wasn't working properly and determined the tenant removed the lightbulb to use the outlet for Christmas lights. Mike will convert that light to an LED.

Commissioner Scholfield said that two of the parking barricades in that area are going to be moved out a few feet to free up room for the fence replacement to accommodate CIR.

Removing the Old Town Pub was discussed. It was agreed that if it is officially decided to tear it down the asbestos would first need to be removed as well as the compressor, which could possibly be sold.

8. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$66,246.40, checks numbering 12245 through 12277 and Electronic Funds Transfer 2019-11 to the U.S. Treasury in the amount of \$2,277.10, were approved (motion by Aus; second by Reese; unanimous).

9. EXECUTIVE SESSION – None

Ms. Larson said that the tractor rides for the upcoming tree lighting festival were almost discontinued due to the high insurance costs, but they have since obtained a different/lower quote.

Tim said that Monica Downen from the bakery asked him if the fire pit could be set up for the lighted boat parade scheduled for December 21st. Tim offered to set it up for Ms. Downen, but told her she needs to discuss it with the Port. The Commissioners seemed to be positive about it.

10. ADJOURN

At 10:15PM the meeting adjourned (motion by Aus; second by Reese; unanimous).


Approved:



Commissioner



Commissioner



Commissioner

Port of Silverdale

Minutes of Regular Meeting
December 19, 2019

1. CALL TO ORDER

Commission Chairman Ed Scholfield called the meeting to order at 7:00 PM in the Port office. Also present were Commissioner Henry Aus; Commissioner Caleb Reese; Attorney Phil Best; Administrator Theresa Haaland; Mike Vasquez of MTV Home Repair; Tim Knapp of TIKAR Services; John Bouck of Kitsap Sailing and Rowing Foundation (KSRF); Bridget Burke of Clam Island Rowing (CIR); Ron Easterday of Rice Fergus Miller (RFM); Roy Sahali of Sahali Farms; Sabyasachi and Sarmila Basu; Ruth Harris; David Holman; Marvel Hunt; Carla Larson; and Jack West.

2. PLEDGE OF ALLEGIANCE

Everyone stood for the pledge.

3. CONSENT AGENDA

The December 19, 2019 meeting agenda and the November 21, 2019 Regular meeting minutes were approved as submitted (motion by Reese; second by Aus; unanimous).

4. PUBLIC COMMENT

4.1. Waterfront Center & Pump Station #3 Predesign – Ron Easterday provided an update on RFM's predesign efforts. He explained that the budget is being determined and wanted to make sure everyone understood that when they are discussing the project's "hard" costs, they are primarily referring to the construction costs. The "soft" costs refer to multiple items such as, but not limited to, permitting fees, independent inspection fees, architectural and engineering fees, attorney fees if needed, and sales tax on the construction costs. He provided a picture of a white board exercise RFM performed regarding the project. It included a rough timeline showing the completion of the predesign in May of 2020. He explained that they would like to hold four public/stakeholder workshops

between now and May and also plan to continue to meet with Kitsap County Public Works. It was agreed that all of the Community Advisory Committee members, Port tenants, Central Kitsap School District (CKSD), as well as the Silverdale Chamber of Commerce will be invited to attend the predesign planning workshops. It was decided that the meetings would be held monthly on the Tuesday a week prior to the Port meeting at 6:00PM. Mr. Easterday offered to find an adequate space since more room will be needed. Once the meeting space is secured an invitation will be sent via email. Roy Sahali and Marvel Hunt voiced their concerns of an uninformed public. It was suggested the invitation be advertised in the newspaper. This was discussed. It was decided to ask the Silverdale Chamber to pass the invitation along to members. RFM will contact the Central Kitsap Community Council (CKCC) and ask them to also pass along the invitation. Commissioner Scholfield said that he will update the Port's website and Facebook page. It was thought these efforts should stir up adequate interest.

The Bainbridge Island Rowing Center was discussed. Mr. Easterday explained that it is partially below grade similar to a daylight basement and is really an extraordinary facility. Bridget Burke said that she would love to take the Commissioners on a field trip up to view it. Mr. Easterday along with Commissioner Scholfield and Phil attended a meeting with the County regarding pump station 3 earlier in the day. Mr. Easterday said it was a good meeting as RFM was able to determine more of the details the County will require to house the pump station equipment. The County's timeline for design is in 2021 with construction to follow in 2022. A drawing the County had prepared was reviewed. David Holman asked if this project is related to the Bayshore/Washington County project. It was explained that it is not as that is a separate project. Carla Larson

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reported that the County is supposed to be going out for bid on that project this spring with construction to start sometime in the summer, probably August. Roy Sahali asked if this will include the Clear Creek trail end or connection. Mr. Easterday said that's what they have been discussing. He thanked the Commissioners for their time.

It was decided to discuss Item 5.2.h. Eagle Scout Life Jacket Loaner Station Project on Pier since Jack West was in attendance. Mr. West explained that the project took longer than he had anticipated but he was happy to report it is complete. An e-mail dated December 14, 2019 from Mr. West was included in the agenda packet. A picture of the station was attached to the email. Mr. West explained that there are five hooks for life jackets with two jackets are currently available. He provided final paperwork for signature. Commissioner Scholfield explained that the life jackets will probably be pulled out during the winter months. Mr. West was thanked for his work on the project.

4.2. 3295 NW Lowell Street/Abeel Studios – David Holman the tenant explained that he and his wife, Tina Abeel, were notified about the ten percent rental increase. He said there was an error in communication because when the rent was increased last year, they were under the impression that it would not be raised again in the near future. So, when they received notice of the ten percent increase, they were surprised. He explained that if they had known the rent was going to be increased again so soon, they would have requested a longer lease last year. They have been interested in entering into a longer lease with the Port ever since they began renting, but were verbally told early on that a longer lease would not be possible. When they found out that other Port tenants, such as the DRC and Elizabeth's House of Wax, have longer leases with the Port they were a bit confused. He explained

that they too would like a longer lease and requested it to be three years and hoped it could be set at the current lease amount. He explained that they do have a small list of maintenance-type items that they would like to have fixed. The back outdoor light has been non-functioning for quite a while now, the hot water to the front restroom takes about four minutes before it gets warm, some of the linoleum in the back restroom is pulling away from the floor. Mr. Holman explained that he had purchased flooring to replace the linoleum, but hasn't installed it. He had hoped the side door could be converted to a push-bar door, similar to the back door. Mike explained that the side door pushes into the building, so that would not be possible. Mr. Holman said a while back one of the toilets was constantly running, the Port was informed and it was fixed or so they thought. The Silverdale Water Department showed up at the building after the toilet had be fixed and informed the tenants that there was a major leak. At that point Mr. Holman and his wife decided to just replace both of the toilets in the back. They later received an \$800 water bill, which they paid. Mr. Holman said that they would also like the toilet in the front restroom be replaced, since the other two are new and they don't want to chance another large water bill. He said that they were so surprised to receive two rental increases within two consecutive years. A spreadsheet that showed the rental amounts for each of the Port's properties was reviewed. Commissioner Aus explained that they are and have been paying a lower square footage amount than the other properties and that is why there has been two minimal increases and there will be more until the rent is equivalent to the others. The other alternative was for the Port to significantly increase their rent last year, but it was decided to take a more gradual approach. Commissioner Scholfield explained that the long-term lease basically ensures the tenants will be there long enough to pay back for some of the improvements. He said that if

Mr. Holman and Ms. Abeel want new paint and new carpet/flooring, doors, toilets, etc. then they will need to enter into a five-year lease at the \$.90 per square foot like the other tenants, but that will increase the rent quite a bit. Mr. Holman said that he would have to talk this over with his wife. He said that in the meantime, they will pay the increase and asked that the lease continue to be renewable annually. It was explained that the outdoor light will be fixed and it was requested that any other maintenance-type issues be reported to the Port office via e-mail. Mr. Holman concurred.

4.3. 3425 NW Byron Street/Kitsap Art – Ruth Harris explained that she has owned Kitsap Art for the past twenty-one years and operated it out of Silverdale for twenty of those years. She had been considering retirement when one of her students, Sarmila Basu, approached her about the possibility of purchasing the business. They have since come to an agreement and Ms. Basu will be the new owner as of February 1, 2020. Ms. Harris explained that she recently entered into a new one-year lease with the Port and was hoping the Commissioners would agree to allowing her to transfer the lease to Ms. Basu as of February 1, 2020. Ms. Basu introduced herself and explained that she has lived in Kingston for the past two years. She currently works at Microsoft and plans to retire in a couple years which will free up her time to focus on the art school. She was involved with the Kirkland Art Center for over seven years and she has always been very interested in art. She would like to eventually turn the Kitsap Art School into the Kitsap Art Center, which would require much more room. She asked that the Commissioners keep her and her business in mind as other Port rentals become available and also with future development plans. She envisions the Art Center as a place that brings people with different backgrounds and interests together through art. She would also like to

reach out to the elderly and get them involved as well as the youth via the school district. Phil explained that the current lease has a provision that it cannot be assigned without an agreement by the Port. *It was agreed to allow the assignment of Ruth Harris' lease for the 3425 NW Byron Street property be transferred to Sarmila Basu and authorize the Port Chairman, Commissioner Scholfield, to sign it* (motion by Reese; second by Aus; unanimous). Ms. Harris explained that not many people are yet aware of this transaction. There should be very little changes to the business including no change in personnel and Ms. Harris plans to volunteer as needed to insure a smooth transition. Ms. Harris and Ms. Basu thanked the Commissioners.

5. UNFINISHED BUSINESS

5.1. Port Programs

a. Sailing – John Bouck reported that they plan to take the motorized boats to Aqua Tech sometime in January. If the Port cannot be billed, Commissioner Scholfield will pay for the services via his Port-issued credit card. Mr. Bouck provided the signed 2019-2020 Sail Boat and Equipment Use Agreement. It was accepted and signed by the Commissioners. Mr. Bouck also provided a check in the amount of \$1,497.50 for reimbursement of the 2014 – 2018 Leasehold Excise taxes that the Port had paid on KSRF's behalf. He explained that they plan to make the 2019 tax payment by the end of January.

b. Rowing – Bridget Burke explained that she will have to send in payment of the Leasehold Excise tax reimbursement. She reported that there is one shell that is in really bad shape and is no longer usable. It was gifted to the program from the Bainbridge Island Rowing Club several years ago and wasn't in the best of shape at that time. She questioned how they can get rid of it. Phil explained that they will need to determine the value and then it should be sold for that

Port of Silverdale – Minutes of Regular Meeting on December 19, 2019

amount and payment issued to the Port. A clear history of how the worth was determined needs to be provided with the payment to the Port. Ms. Burke explained that they will be renewing their insurance at the first of the year and will be sure to include the Port as additionally insured. She provided CIR's 2019 Annual Report. It was reviewed. She said that there are now eighteen rowing participants in the high school team, which is very exciting. They may be getting a third coach for that team, which will be a total of five coaches. Saturday rowing will start back up at the end of January/early February with the official rowing season beginning in March. They are planning to make the move from rowing alley to the boatyard, behind the Old Town Pub, on January 25th. Ms. Burke apologized for the chase boat being tied up to the floating moorage facility for nearly the entire summer. She explained that she was out of town a lot for personal reasons and had thought other members would be removing the boat periodically.

c. New lease – KSRF's agreement was signed. Ms. Burke plans on submitting CIR's agreement by the end of the month.

5.2. Port Facilities

a. Department of Natural Resources (DNR) – Nothing to report.

b. Dredge – Mail from the Washington State Department of Fish and Wildlife (WDFW) dated December 6, 2019 was reviewed. It was regarding the decision on the informal appeal submitted by Sound Action. WDFW did not believe that the proposed dredging would cause net loss of fish life. They did require modification to the Hydraulic Project Approval (HPA) permit which changes the window of opportunity for the deep water dredging around and under the floating moorage facility from September 1st through February 15th and the intertidal dredging around the boat ramp from

September 1st through October 15th. Commissioner Aus questioned if the Port will have a problem hiring a contractor to complete the work within the required timeframe. Phil said that dredging contractors generally work within similar timelines, so wouldn't anticipate any issues. Commissioner Scholfield asked if Kathleen Byrne-Barrantes is aware of this. Phil explained that there is a thirty-day appeal period, so once that has passed, he will inform Ms. Byrne-Barrantes.

c. Sailboat (WN6823JF) – Commissioner Scholfield said that he was going to look into the necessary steps to get the boat sold. *It was agreed that the sailboat "Wren" WN6823JF, which is Port of Silverdale property, be sold* (motion by Reese; second by Aus; unanimous).

d. Benches – Tim reported that he received the delivery of all the materials last week, so they plan to begin working on the benches the first of the year.

e. Port Rules and Regulations Policy – Nothing to report

f. Electrical Repair – e-mails were sent to Bainbridge Island Electric asking to get on their schedule. They responded stating that they needed to talk to Commissioner Scholfield and were provided his cell number.

g. Sea Plane Crash Expenses – Phil prepared a set of facts of the events surrounding the incident and has asked Tim to review them. Phil explained that before moving forward he needs more information, a bigger description of what actually transpired. Commissioner Scholfield said that he sent Phil an e-mail regarding the incident. Phil plans to review it do further research. The first step is to have all the facts laid out before proceeding.

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h. Eagle Scout Life Jacket Loaner Station Project on Pier – already discussed under Public Comment.

i. Oil separator lid – Tim explained that the material to replace the lid is about six to eight weeks out, so he plans to cone off the area around the lid.

j. Bonfire – Monica Downen of Monica's Bakery is planning to use the Port's fire pit to hold a bonfire near the boat launch during the lighted boat parade this coming Saturday. Commissioner Scholfield questioned if a Use of Port Property Application had been submitted and asked who from the Port was planning to man the event. It was explained that an application had not been received and no one from the Port was able to attend the event. Tim explained that Ms. Downen had approached him about it and he told her he could set it up, but he was under the impression that she had approval from the Port. Phil said that he talked to Ms. Downen just in passing when he was at the bakery, but nothing official. Ms. Downen will be contacted and asked to submit the application and also provide proof of insurance listing the Port as additionally insured. She will also be reminded that she is responsible to man the fire for the entire event and insure it is extinguished afterwards. Tim said that he plans to move the pit from the boat launch area the following morning.

5.3. Port Properties

a. 9020 Washington Avenue/Elizabeth's House of Wax – the sewer line connection to the existing outlet is tabled until the County's Bayshore/Washington Improvement Project is underway. Commissioner Aus reported that

the County is planning to micro-tunnel the Bayshore Drive/Washington Avenue project. They are supposed to go out for bid in the Spring.

b. 9004 Washington Avenue/DRC – Mary Hancock was informed of the rent increase to \$1,408. She said it seemed fair. Mike plans to contact Ms. Hancock within the next couple weeks to discuss the installation of the new flooring and restroom ventilation.

c. 3215 Lowell Street Suite 161/Sefton Enterprises – notice of past due rent for October, November and now December was sent via e-mail. The lease will not be renewed. They have been asked to drop the keys off at the Port office along with payment. Bob Guardino, the upstairs Suite 231 tenant, is interested in moving to a larger office within the building. He has been notified that suite 161 will be available soon. Commissioner Scholfield plans to access suite 161 after the 1st.

3215 Lowell Street, Suite 191/Deborah Lyons – notice of past due rent for November and December was sent via e-mail. Ms. Lyons responded explaining that she plans to set up bill pay through her bank.

Lawn repaired – Commissioner Scholfield will verify tomorrow.

5.4. Easement – Phil reported that the last he had heard the easement was being processed.

5.5. Paving of the alley – Phil reported that he spoke to Lakeside Industries personnel and explained the situation with having to reduce the asphalted area. It was agreed that the contract will be renegotiated after the first of

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the year. Commissioner Reese reported that he received a call from Lakeside requesting that at the appropriate time to meet with someone from the Port on site to discuss.

5.6. Comprehensive Plan – Commissioner Reese made the presentation during the Central Kitsap Community Council (CKCC) meeting on December 4th at the Silverdale Beach Hotel. It seemed to be well received. Ms. Downen, who besides being an Old Town Small business owner is also a CKCC board member, requested that the Port find an offsite storage area for the sailboat floats and boat launch handling pier. Commissioner Scholfield said that for the past few years the handling pier has been spread out throughout the overflow vehicle boat trailer lot to help alleviate erratic driving/racing in that area. This year the handling pier has been stacked off to the side because a sailing regatta was held a week after it had been removed from the water. Just last week Kitsap Art personnel asked Commissioner Scholfield if the handling pier was going to be spread out in the parking lot this season as she was hoping it would be because she nearly was hit by a vehicle that was being driven erratically in the parking area.

5.7. Grant Funding – the final comprehensive plan is due to the Recreation Conservation Office (RCO) by March 1st, but prior to that Ms. Byrne-Barrantes should be providing it to them for pre-approval.

5.8. Benefits – Commissioner Aus said that his insurance covers medical transport. *It was agreed to pay for the supplemental medical transport insurance from Airlift Northwest of Washington for Port Commissioners and staff that*

are in need of it (motion by Reese; second by Aus; unanimous).

5.9. Purchased Services contract – (Dreaney's, MTV Home Repair, Safe Security and TIKAR) during the 2017 audit it was recommended that the the purchased services contracts be sent out for bid every three to five years. TIKAR had prepared a draft invitation to bid form that was thought could be used in general. Phil plans to work on the invite and have notice published in the newspaper. The bid packages will be sent to anyone that requests it and also will be sent to all potential vendors listed on the Small Works Roster.

5.10. County Commissioner – Commissioner Reese was recently invited to meet with County Commissioner Wolfe. Commissioner Reese explained that it was a good meeting and Commissioner Wolfe just wanted to meet with him since he was newly elected. The County has \$402,000 budgeted for the planning and permitting of a new bulkhead at the Waterfront Park. Commissioner Reese told Commissioner Wolfe that he was sure the Port would want to work with the County on that project.

6. NEW BUSINESS

6.1. Resolution 2019-04 the County Treasurer's office contacted the Port office and explained that the Port has a Donation Fund that has had no balance and/or activity in over twenty years. They requested the fund be closed. *It was agreed to adopt Resolution 2019-04 Closure of Fund No. 88746* (motion by Reese; second by Aus; unanimous).

6.2. Special Meeting – Commissioner Reese recently attended the New Commissioner

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Seminar hosted by the Washington Public Ports Association (WPPA). He learned a lot from the seminar and it was thought maybe it should be shared with everyone.

Commissioner Reese said that at this point he plans to sit down with Port staff to further discuss and determine the best avenue to take in informing all three Commissioners, possibly via a Special Meeting.

6.3. Proposed Sewer Rate Increase letter dated November 26, 2019 from Kitsap County Public Works was reviewed.

6.4. Oath of Office – Commissioner Reese's Oath was administered by Phil

7. SAFETY/SECURITY – Nothing to report.

8. APPROVE EXPENDITURES & ELECTRONIC TRANSFER

The attached voucher approval totaling \$27,337.41, checks numbering 12278 through 12305 and Electronic Funds Transfer 2019-12 to the U.S. Treasury in the amount of \$1,998.98, were approved (motion by Aus; second by Reese; unanimous).

9. EXECUTIVE SESSION – At 9:58PM it was announced the meeting would be going into Executive Session for approximately twenty minutes to discuss litigation.

At 10:15PM the meeting returned to Regular Session. No one from the public was waiting to attend Regular Session.

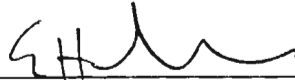
10. ADJOURN

At 10:17PM the meeting adjourned (motion by Aus; second by Reese; unanimous).

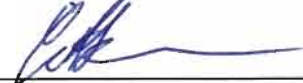
Approved:



Commissioner



Commissioner



Commissioner



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

APPENDIX D: CKCC PRESENTATION

PORT OF SILVERDALE

Comprehensive Plan Update
2017-2018

Steve Rice
Architect & LEED AP
Principal

RICE *fergus* **MILLER**

 **ART ANDERSON ASSOCIATES**
VALUE BEYOND ENGINEERING.™

RFM/AAA

Team Charge

- Update 2007 Port of Silverdale Comprehensive Plan
- **Create ‘Concept Use Plan’ focused on Port’s waterfront properties**

- Public outreach
- Collaborate with Kitsap County

- Carry out projects 2018 - -

Master Plan Updates

2007 Makers 10-year Comprehensive Plan

2012 revision

2017 November RFM/AAA hired



Port of Silverdale Mission

- Promote economic activity
- Provide recreational opportunities and access





Port-Owned Properties

Activities to Date

- Met with Kitsap County Public Works - ongoing
- Co-authoring KC/POS electronic survey for Silverdale waterfront
- Held listening and idea session with Citizens POS Advisory Committee
- Hosted POS Commissioner Workshop
- CKCC visit today

TRANSITIONAL

OLD TOWN GRID

**WATER DEPENDENT /
WATER ENJOYMENT**



KITSAP SAILING & ROWING

Kitsap Sailing & Rowing Foundation Supports Sailing and Rowing at the Port of Silverdale

Mission

The Kitsap Sailing & Rowing Foundation believes that learning to sail and row teaches important life skills like teamwork, decision making, and appreciation of the natural environment. Competitive sailing and rowing requires tactical and strategic thinking and physical ability. With each experience on the water, young people gain self-confidence, self-esteem and a sense of camaraderie and accomplishment.



High School Recreational Rowing starts April 10

[More info](#)

Free Cox Clinic April 14

[More info](#)

Masters Try Rowing April 21

[More info](#)

Learn to Row starts April 24

[More info](#)

Masters Rowing starts April 16 - 18

[More info](#)

Concept Use Plan

Commissioner Priorities

- **Develop Silverdale's waterfront as a people place**
- Keep nearshore open – **“feel the expanse”**
- New facilities must support Port mission
- Respect & promote small business
- Keep Old Town scale and character
- Support non-motorized boating programs
- Balance parking vs. activity

Concept Use Plan

RFM/AAA Mindset

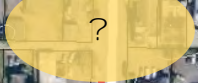
- More reasons to come here!
- Needs landmark/destination project
 - Consider unmet needs/opportunities
- Connect people to waterfront
- **Bring Silverdale a better ‘front porch’**
- Plan for synergies
- What can OT/WF be known for?

Concept Use Plan

RFM/AAA Mindset

- More reasons to come here!
- Needs **landmark**/destination project
 - Consider unmet needs/opportunities
- **Connect** people to waterfront
- Bring Silverdale a better **'front porch'**
- Plan for **synergies**
- What can OT/WF be known for?

in progress
Concept Use Plan



- 1 sail & small boat loft w/street retail
- 2 iconic corner business
- 3 mixed use community event center
- 4 human powered boat center
- 5 outpost
- 6 kayak and paddleboard launch float

Assets & Investments Connect the Dots?



Next Steps

- Meet with Central Kitsap School District
- Complete electronic survey
- Continue to develop use plan for Port properties
- Find good answer for Pump Station 3
- Decide on OTP evaluation
- Set project timelines
- Publish formal POS comprehensive plan update
- Work with Kitsap County on **‘front porch’ integration**

A scenic sunset over a body of water. The sky is a mix of deep blue and orange, with wispy clouds. A long pier extends into the water from the right side. The shoreline is covered in dark trees and some buildings. The sun is low on the horizon, creating a bright glow.

Thank you! Questions?

Steve Rice
Architect & LEED AP
Principal
srice@rfmarch.com

RICE*fergus***MILLER**

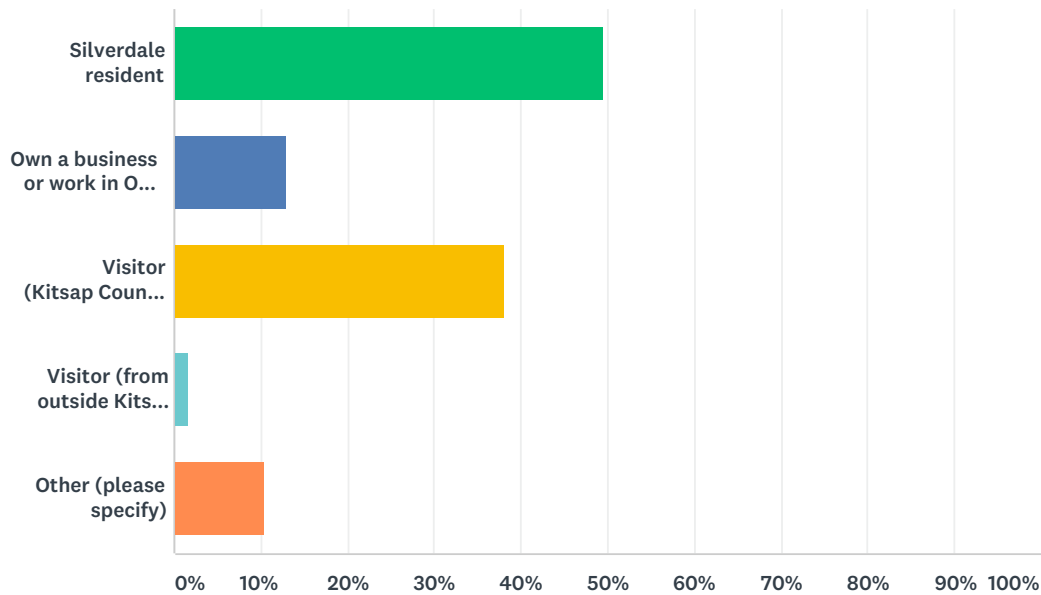


PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN

APPENDIX E: OLD TOWN WATERFRONT PUBLIC SURVEY RESULTS

Q1 What is your relationship to the Old Town Silverdale Waterfront? (Check all that apply)

Answered: 532 Skipped: 3



ANSWER CHOICES	RESPONSES	
Silverdale resident	49.62%	264
Own a business or work in Old Town Silverdale	12.97%	69
Visitor (Kitsap County resident)	38.16%	203
Visitor (from outside Kitsap County)	1.69%	9
Other (please specify)	10.53%	56
Total Respondents: 532		

#	OTHER (PLEASE SPECIFY)	DATE
1	Student	7/1/2018 12:10 AM
2	Windsurfer/Sailor/power boat operator	6/30/2018 11:02 PM
3	Own a multimedia business that occasionally utilizes the Silverdale waterfront.	6/29/2018 1:49 PM
4	I belong too Clam Island Rowing Club. I am in OTS 4-6 times a week	6/29/2018 8:27 AM
5	Clam Island Rowing and Kitsap Sailing and Rowing Foundation	6/28/2018 5:44 PM
6	Volunteer with Clear Creek Trail at Waterfront	6/25/2018 8:22 AM
7	waterfront property owner Dyes Inlet	6/21/2018 7:49 PM
8	Poulsbo - South Keyport Resident	6/21/2018 1:14 PM
9	Lived here from 1958 to 2016 than moved to Bremerton	6/18/2018 8:39 AM
10	Park board member	6/17/2018 11:24 PM
11	East Bremerton resident; near Silverdale	6/17/2018 9:59 AM
12	Parents grew up in Silverdale	6/15/2018 6:45 PM

Old Town Silverdale Waterfront Public Survey

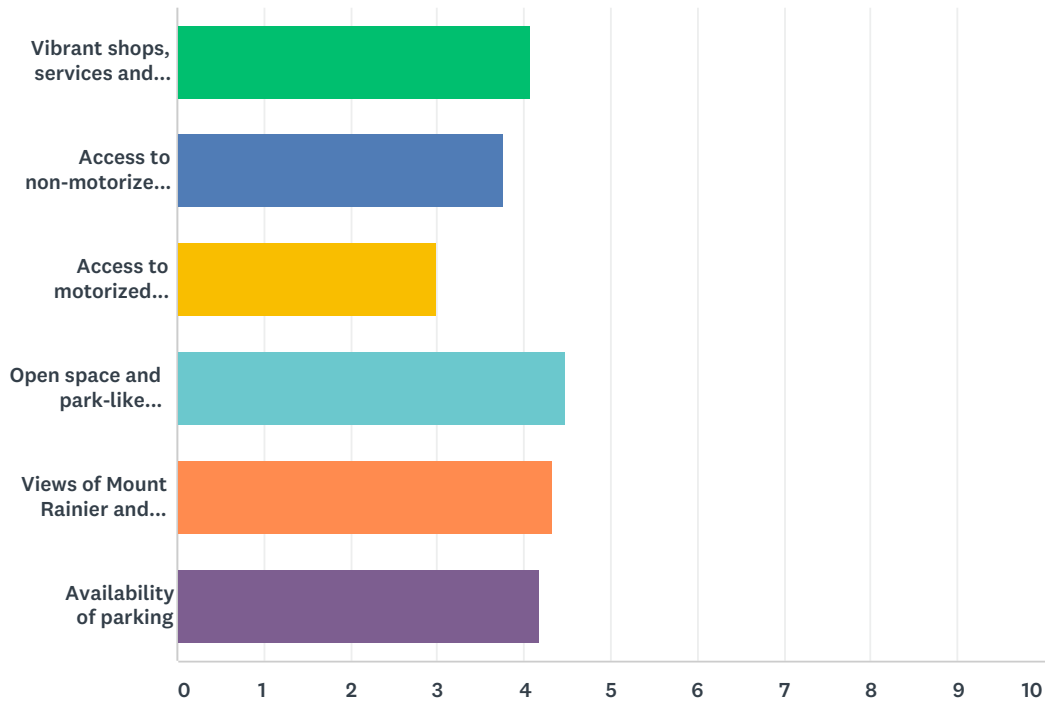
13	Live right outside Silverdale, and used to own a business in Silverdale	6/9/2018 5:29 PM
14	Work in silverdale	6/9/2018 5:28 PM
15	Sailing team uses dock everyday	6/5/2018 5:00 PM
16	Work in old town Silverdale	6/4/2018 1:11 PM
17	Patron of Old Town Silverdale businesses, park, and library	6/4/2018 8:45 AM
18	Live in Poulsbo, but my family in the 30's, 40's, 50's had a business there and growing up in Tracyton for a number of years I have ties to Silverdale, plus I retired from teaching there with 34 years of service	6/4/2018 7:55 AM
19	Moving to Silverdale	6/4/2018 2:51 AM
20	county resident on CKCC	6/3/2018 2:49 PM
21	The park and waterfront is a favorite place to take my grandchildren.	6/3/2018 2:31 PM
22	Concerned citizen	6/3/2018 11:21 AM
23	I use the dock. To	6/3/2018 8:09 AM
24	Lived her my entire 74 years	6/3/2018 7:19 AM
25	Coach at Clam Island Rowing on Byron St.	6/2/2018 6:52 PM
26	Row with Clam Island	6/2/2018 5:53 PM
27	grew up in Silverdale	6/2/2018 5:29 PM
28	high school sailing team member	6/2/2018 3:36 PM
29	Grandson of Peter William Torpey-(original manager of the Old Town Silverdale Co-Op.)	6/2/2018 9:52 AM
30	I am a board member a of Rowing Foundation. http://www.kitsapsailingrowing.org/	6/2/2018 8:39 AM
31	Lived in Silverdale 32 yrs, worked in old towne vet 5 yrs, have been, to many of the stores in, the area	6/2/2018 2:58 AM
32	I grew up in Silverdale, Live on Chico Way now.	6/2/2018 12:10 AM
33	Silverdale Pioneer Family	6/1/2018 10:45 PM
34	My Dad owned Silverdale Fuel & Transfer which was located in Old Town Silverdale.	6/1/2018 9:59 PM
35	Live in Seabeck, but shop in Silverdale	6/1/2018 9:56 PM
36	Rowing Club Member, parent of a sailor	6/1/2018 8:49 PM
37	Taxpayer	6/1/2018 7:47 PM
38	do business with several businesses located in old town Silverdale; memories at the gazebo at the old town Silverdale waterfront park	6/1/2018 6:19 PM
39	born and raised here	6/1/2018 5:49 PM
40	Vendor for Farmers Market	6/1/2018 5:41 PM
41	Live very close to park but technically I'm listed as a Bremerton resident. I have previously lived in Silverdale for 17 years.	6/1/2018 5:27 PM
42	Big supporter of smartly planned development of Old Town	6/1/2018 2:13 PM
43	Sailing Coach and Instructor	6/1/2018 1:18 PM
44	Prior resident and frequent visitor	6/1/2018 12:34 PM
45	Live 5 miles west of Silverdale but there every day.	6/1/2018 12:22 PM
46	I use to live and work in the Old Town. Now I frequent the park with my dog.	6/1/2018 11:45 AM
47	Outrigger canoe club that stores their boats on the waterfront behind the Wells Fargo building	6/1/2018 11:41 AM
48	Used to work and live in Old Town	6/1/2018 11:10 AM
49	Kitsap County Park employee	6/1/2018 11:02 AM

Old Town Silverdale Waterfront Public Survey

50	Live on Chico Way N.W., Bremerton	6/1/2018 10:56 AM
51	CK High School sailor parent	6/1/2018 10:32 AM
52	Old Town resident and property owner.	6/1/2018 10:21 AM
53	Coach of the CK high school sailing team	6/1/2018 10:16 AM
54	Silverdale Business Owner	6/1/2018 10:11 AM
55	prior business owner	6/1/2018 9:48 AM
56	Run the CK Farmers Market at the Waterfront Park	6/1/2018 9:40 AM

Q2 What do you value most about the Old Town Silverdale waterfront area?(Rank 1-5 with 1 being not important to you/5 most important to you)

Answered: 535 Skipped: 0



	1 - NOT IMPORTANT	2 - LESS IMPORTANT	3 - NEUTRAL	4 - IMPORTANT	5 - VERY IMPORTANT	TOTAL	WEIGHTED AVERAGE
Vibrant shops, services and eateries	2.28% 12	4.94% 26	12.93% 68	41.83% 220	38.02% 200	526	4.08
Access to non-motorized watercraft activities and amenities	7.17% 38	6.60% 35	23.21% 123	28.87% 153	34.15% 181	530	3.76
Access to motorized watercraft activities and amenities	16.89% 89	18.98% 100	26.38% 139	23.15% 122	14.61% 77	527	3.00
Open space and park-like activities	0.56% 3	1.50% 8	4.88% 26	35.83% 191	57.22% 305	533	4.48
Views of Mount Rainier and Dyes Inlet	1.32% 7	2.07% 11	10.55% 56	34.27% 182	51.79% 275	531	4.33
Availability of parking	2.26% 12	3.58% 19	11.32% 60	39.62% 210	43.21% 229	530	4.18

#	OTHER (PLEASE SPECIFY)	DATE
1	Cleanliness of the Area & Public Safety such as vivid crosswalks and bright tall lights.	7/1/2018 12:10 AM
2	Public boat storage/Marina	6/30/2018 11:02 PM
3	Safe access from buildings to the water.	6/29/2018 8:27 AM
4	Non Motorized Boat storage	6/28/2018 5:44 PM

Old Town Silverdale Waterfront Public Survey

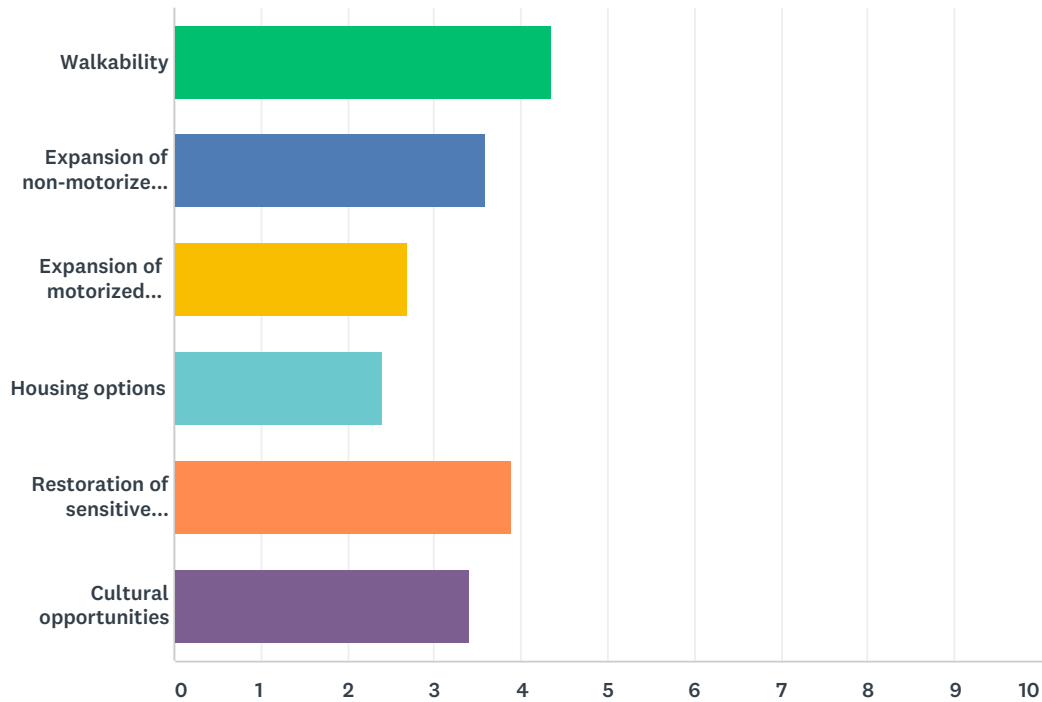
5	Streetlamps, hanging baskets, benches, etc with a central theme to bring the area together aesthetically	6/26/2018 1:23 PM
6	Safety	6/21/2018 5:12 PM
7	Launch area for kayaks	6/20/2018 1:08 PM
8	Kid friendly amenities. We love the park!	6/17/2018 11:47 AM
9	Keep the historical pub and create as chamber or something along those lines	6/14/2018 4:18 PM
10	It maintains an old-town quaintness, it's unique flavor	6/14/2018 1:04 PM
11	Improved sidewalks and street landscaping	6/12/2018 6:32 AM
12	Safe adequate parking for non-motorized watercraft (kayaks)	6/11/2018 7:17 PM
13	Need more disabled parking	6/8/2018 4:07 PM
14	It's an adorable area, but not utilized to its potential. There s/b interesting shopping choices, so people can walk around. Also more restaurants. There are too many businesses and not enough shopping	6/8/2018 11:09 AM
15	wildlife obsevation	6/5/2018 2:06 PM
16	If there were more eateries and fun shops, I'd say they were very important. There just aren't many.	6/4/2018 4:00 PM
17	I would like to see quaint shops and restaurants... no chains	6/4/2018 2:38 PM
18	Green trees, open spaces, slow pace, minimal traffic, family-owned businesses with small footprints, original Kitsap feel - PLEASE DO NOT MAKE THIS PLACE INTO BELLEVUE.	6/4/2018 8:45 AM
19	Maintaining clean waterways, storm systems, open space, native plants, more rain gardens. Please no chain restaurants! You can find those anywhere in the U.S. Also, enjoy farmer's markets and local residents, again, no chain restaurants. Keep it attractive without bright, glaring commercial, ,tacky chain restaurants	6/4/2018 7:55 AM
20	retain historical feel and make pedestrian frendly	6/3/2018 1:30 PM
21	Retain old character.	6/3/2018 12:44 PM
22	And the historic buildings are very important as well	6/3/2018 7:19 AM
23	retention of historic character	6/2/2018 5:29 PM
24	Too much boat parking for people leaving Silverdale. Needed for customers staying.	6/2/2018 12:01 PM
25	Relevant Historical Signage	6/2/2018 9:52 AM
26	Parking should be moved away from the waterfront to make room for a maritime center which houses sailboats, rowing shells, and flexible use space. Port Townsend's Maritime Center models this concept beautifully.	6/2/2018 8:39 AM
27	need good playground - more swings	6/2/2018 7:34 AM
28	Public park needs parking close by	6/2/2018 2:58 AM
29	Preservation of the heratige of Silverdale and the charecter that defines Silverdale.	6/1/2018 8:01 PM
30	Emphasis should be on preserving the history of the community.	6/1/2018 6:24 PM
31	The PARK PLAYGROUND	6/1/2018 5:27 PM
32	historical buildngs	6/1/2018 3:37 PM
33	Facilities for guest moorage, including sailboats	6/1/2018 2:35 PM
34	1) Scenic neighborhood (quiet, trees, landscaping, rehab of old buildings or intentional new construction). Positive mixed-business environment, i.e. please let's not be overrun with expensive boutiques..	6/1/2018 2:21 PM
35	I like the Farmer's Market	6/1/2018 2:21 PM
36	Option for having special events year around (rain or shine)	6/1/2018 2:13 PM
37	Appealing beach/dock space	6/1/2018 1:18 PM

Old Town Silverdale Waterfront Public Survey

38	Historical references, eco-education, housing	6/1/2018 12:10 PM
39	Keeping the feel and look of the old town will help continue the history of Silverdale, as this area as it is called "Old Town" was the original Silverdale.	6/1/2018 12:01 PM
40	Inline/Roller Skating park	6/1/2018 11:42 AM
41	Preserving historical buildings!	6/1/2018 11:10 AM
42	Being left natural to offer some tranquility, not a money making operation.	6/1/2018 11:09 AM
43	Safe sidewalks, Walking paths. Family friendly atmosphere. No lottering	6/1/2018 10:46 AM
44	Year round youth sailing and rowing programs	6/1/2018 10:32 AM
45	Silverdale Waterfront Park is the gem of Silverdale!	6/1/2018 10:21 AM
46	This is one of the best venues for regattas in the NW district.	6/1/2018 10:16 AM
47	the view of the waterfront area from residential areas to the west has already been unfavorably impacted by the new HUGE power poles.	6/1/2018 9:48 AM
48	Shoreline access for the general public	6/1/2018 9:48 AM
49	Should only store where people can gather (restaurant, shopping, etc.) on waterfront street.	6/1/2018 9:46 AM
50	More lighting on Washington Ave	6/1/2018 9:31 AM
51	family friendly atmosphere	6/1/2018 9:28 AM

Q3 What are some ideas for improvements to the waterfront that should be considered?(Rank 1-5 with 1 being not important to you/5 most important to you)

Answered: 534 Skipped: 1



	1 - NOT IMPORTANT	2 - LESS IMPORTANT	3 - NEUTRAL	4 - IMPORTANT	5 - VERY IMPORTANT	TOTAL	WEIGHTED AVERAGE
Walkability	0.99% 5	1.58% 8	7.72% 39	39.41% 199	50.30% 254	505	4.36
Expansion of non-motorized watercraft activities and associated facilities	7.56% 40	8.32% 44	27.98% 148	28.54% 151	27.60% 146	529	3.60
Expansion of motorized watercraft activities and associated facilities	23.24% 122	17.14% 90	34.48% 181	16.76% 88	8.38% 44	525	2.70
Housing options	30.71% 160	23.03% 120	26.10% 136	14.78% 77	5.37% 28	521	2.41
Restoration of sensitive environmental areas	4.54% 24	6.99% 37	18.71% 99	34.59% 183	35.16% 186	529	3.89
Cultural opportunities	9.28% 49	8.14% 43	31.44% 166	34.28% 181	16.86% 89	528	3.41

#	OTHER (PLEASE SPECIFY)	DATE
1	Maintenance to power wash and Repaint current surfaces.	7/1/2018 12:10 AM
2	Fresh water rinse station at boat parking area like Kingston	6/30/2018 11:02 PM
3	An indoor space for rowers and sailors and their equipment.	6/29/2018 8:27 AM
4	Holiday lights and decorations	6/26/2018 1:23 PM

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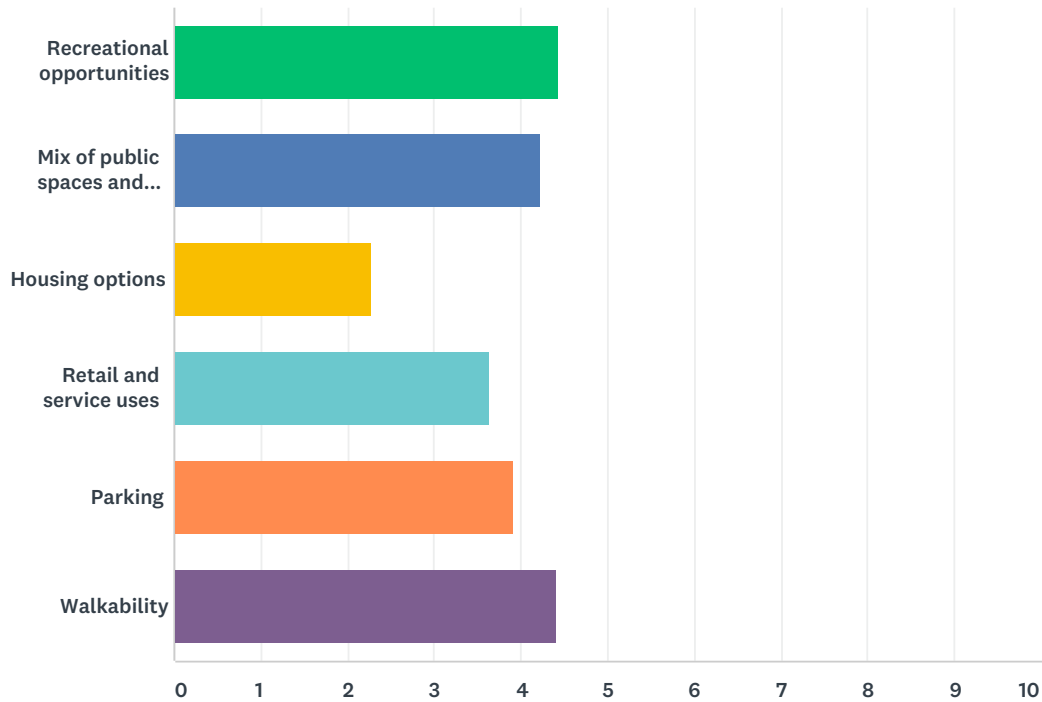
5	Think Gig Harbor, Skanskie Park areas/community gathering & activity areas	6/22/2018 7:21 PM
6	The Port folk have the authority to tax and do amazing things with the Old Town waterfront, but are not exactly visionaries.	6/21/2018 7:49 PM
7	Safety	6/21/2018 5:12 PM
8	Outdoor eating areas - lawn game/activities - charm	6/21/2018 1:14 PM
9	PARKING	6/17/2018 12:58 PM
10	Water quality is #1	6/13/2018 7:43 PM
11	We DON'T need any "community centers" that attract homeless indigent to "take up residence". Whatever development that takes place needs to be inviting to families with an atmosphere of safety and relaxation.	6/11/2018 11:50 AM
12	Make parking available where people can look at the view. There are always people parked by the boat launch to enjoy the view but it says no parking. Also discourage it from being a teen hangout.	6/10/2018 10:04 AM
13	Movie theater	6/8/2018 11:09 AM
14	Stay on top of the graffiti! If you let it go, it will only be the beginning of the breakdown of a community.	6/7/2018 5:19 PM
15	I rather have a nicer park, just like the ones in Bremerton. Plus more businesses there.	6/4/2018 12:18 PM
16	improved parking in areas a bit farther from the park	6/4/2018 9:05 AM
17	Preservation of original residents, small businesses, trees, slow/minimal traffic, and open spaces.	6/4/2018 8:45 AM
18	Clean up the area around Clam Island Rowing Club and the rental area for non-motorized watercraft. Build the Rowing club it's own area on the water like other rowing clubs	6/4/2018 7:05 AM
19	Restoration of historic buildings!	6/3/2018 7:19 AM
20	small business friendly	6/2/2018 5:29 PM
21	Add an arboretum attraction	6/2/2018 3:48 PM
22	Adequate trash clean-up and disposal	6/2/2018 9:52 AM
23	Building a Maritime Center (similar to Port Townsend's Maritime Center) would allow for a wide variety of cultural opportunities. For example, sailing and rowing programs encourage recreation for people of all ages. Regattas bring folks to Silverdale from all over the Northwest. A recent high school sailing regatta held at Waterfront Park brought over 150 sailors and their families.	6/2/2018 8:39 AM
24	better and bigger playground for kids	6/2/2018 7:34 AM
25	Swimming Beach	6/1/2018 9:59 PM
26	A community with walking access to business	6/1/2018 8:01 PM
27	Access for mobility challenged	6/1/2018 7:47 PM
28	Eradicate the foul smell of the pump station - it's terrible. Improve parking space	6/1/2018 6:24 PM
29	There is ONLY 1 small Park/Playground for the entire town of Silverdale!! That is crazy! We shouldn't have to drive to other communities to go to a park! The somewhat recent renovations to the park were a huge disappointment to EVERY parent/person I've spoke with since it was done! The climbing structure is a great addition for the bigger kids. For younger kids, they all get to cram into the pirate ship. Where are the swings!!! Swings are enjoyed by all ages of residents, from infants to seniors and yet the park now has ZERO swings! Ask any parent, grandparent or nanny what a child's favorite thing to do at the park and I'll guarantee they response will be overwhelmingly...the SWINGS!!! I have to take	6/1/2018 5:27 PM
30	I would like the smelly pump station moved. It ruins a beautiful section of waterfront.	6/1/2018 3:53 PM
31	benches	6/1/2018 3:37 PM
32	Concerts or outdoor movies would be great	6/1/2018 2:23 PM
33	More trees and landscaping, narrower streets so cars don't go so fast.	6/1/2018 2:21 PM
34	Get rid of the smelly pump house right in the center of the beach!	6/1/2018 1:18 PM

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35	There are many small Washington coastal towns that have nurtured a strong sailing and boating community. Silverdale can too!	6/1/2018 12:34 PM
36	Restore historical buildings and retain significance	6/1/2018 12:11 PM
37	foot ferry transit to Bremerton, Poulsbo, Pt. Orchard or Bainbridge	6/1/2018 12:10 PM
38	Inline/Roller Skating park	6/1/2018 11:42 AM
39	Don't turn into some sort of entertainment center. People need to be in touch with nature, not noise and entertainment. This beach area lost forever if it is redefined.	6/1/2018 11:09 AM
40	Moving the pumpout station away from the park (ultra important!)	6/1/2018 10:49 AM
41	Better mix of retail businesses. Cafes with outdoor seating,	6/1/2018 10:46 AM
42	Community facility for non-motorized classroom learning and events, shop/storage for program equipment	6/1/2018 10:32 AM
43	A community center with sailboat storage and an educational area, to help promote sailing.	6/1/2018 10:16 AM
44	Expansion of the potential gathering area for vigils, presentations and very small performances	6/1/2018 10:09 AM
45	NO HOUSING!	6/1/2018 9:58 AM
46	Currently heading in wrong direction with regard to types of businesses there. Should be a place where people go out at night or during day for leisure activities.	6/1/2018 9:46 AM

Q4 What are the best uses for the Old Town Silverdale waterfront? (Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

Answered: 535 Skipped: 0



	1 - NOT IMPORTANT	2 - LESS IMPORTANT	3 - NEUTRAL	4 - IMPORTANT	5 - VERY IMPORTANT	TOTAL	WEIGHTED AVERAGE
Recreational opportunities	0.38% 2	0.94% 5	6.77% 36	39.10% 208	52.82% 281	532	4.43
Mix of public spaces and activities	1.13% 6	1.89% 10	9.07% 48	48.77% 258	39.13% 207	529	4.23
Housing options	34.92% 183	22.14% 116	26.34% 138	12.21% 64	4.39% 23	524	2.29
Retail and service uses	4.36% 23	8.92% 47	24.67% 130	42.69% 225	19.35% 102	527	3.64
Parking	3.61% 19	6.26% 33	14.42% 76	45.92% 242	29.79% 157	527	3.92
Walkability	1.32% 7	1.32% 7	7.16% 38	34.27% 182	55.93% 297	531	4.42

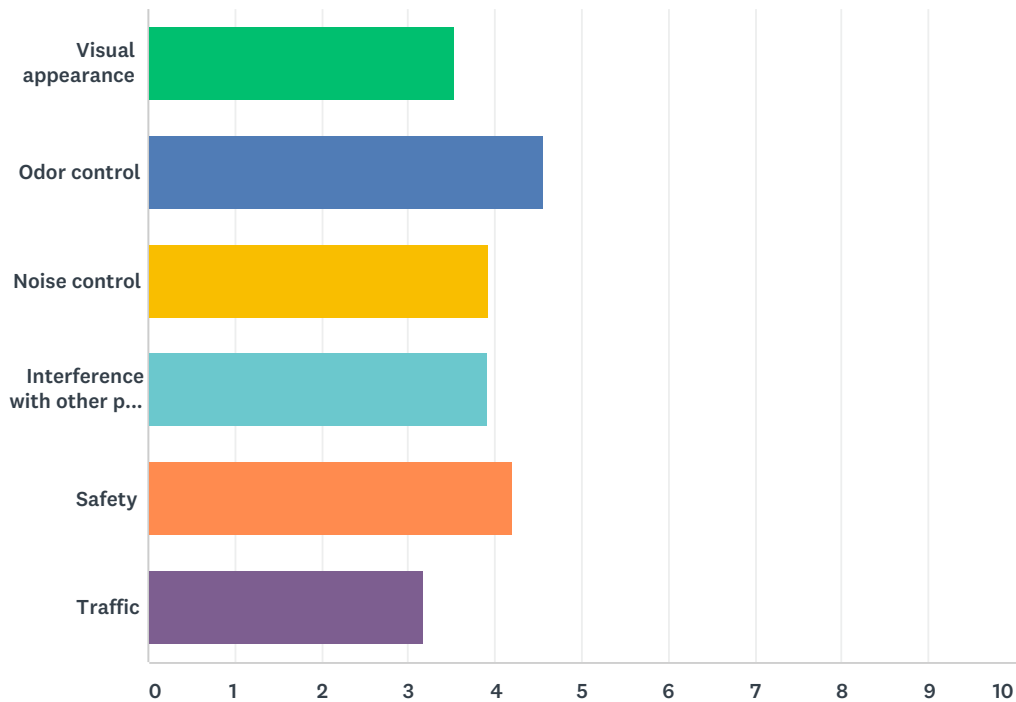
#	OTHER (PLEASE SPECIFY)	DATE
1	Annual events such as Whaling Days and the techniques on how to attract and retain the revenue of attendees. Such as improving the aesthetic designs of the area and buildings. The most simple is to groom the greens, and detail the buildings. To retain, attendees will spend more time there if more food services and selections.	7/1/2018 12:10 AM
2	No "Farmer's Market" / Flea market at a waterfront facility.	6/30/2018 11:02 PM
3	Safety	6/21/2018 5:12 PM

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4	More room for sailing	6/17/2018 7:18 PM
5	Would like the parking modified to discourage the speeding/racing crowd	6/14/2018 1:04 PM
6	No sewage spills	6/13/2018 7:43 PM
7	There are not enough crosswalks! Why aren't there crosswalks at all the intersections?	6/10/2018 10:04 AM
8	kitsap as a whole should be transforming to a travel and liesure model like leavonworth and mondovi italy. stimulates business and commerce without higher infa structure costs to support environmentally damaging high density housing to nearly support king county.	6/8/2018 1:05 PM
9	It's a great area, just too many offices and not enough retail and rest5	6/8/2018 11:09 AM
10	Access for the handicapped to the views of Dyes Inlet/Mt. Ranier and not being roused from a public property we get to pay for but not enjoy on a starlit night. It is not fair to those who have difficulty walking.	6/4/2018 10:49 PM
11	Community spirit preservation - it must retain the Kitsap feel.	6/4/2018 8:45 AM
12	Please focus on creating a vibrant downtown like Poulsbo	6/3/2018 5:20 PM
13	maintain the hisoirical feel with whatever you do	6/3/2018 1:30 PM
14	There are enough businesses. Preserve Silverdale's history.	6/3/2018 7:19 AM
15	historic setting	6/2/2018 5:29 PM
16	Preserve what is left of history rather than destroy it like what has already happened.	6/1/2018 9:59 PM
17	There is not enough space for both parkiing and growth!. Partnership with Kitsap Transit and community to provide ridership and delivery to various areas of the community.	6/1/2018 8:01 PM
18	Disability access	6/1/2018 7:47 PM
19	create a history center featuring the founding members of the original community and provide a showcase for what was once the "Egg capital of the world." Silverdale is rich in history but only the Historical Society brings attention to it - there is no place in Silverdale that is recounting our history except for a few pictures in the Harlow building and a little in the Bowling Alley.	6/1/2018 6:24 PM
20	variety of income levels inclusion	6/1/2018 3:37 PM
21	Already too much business, let's keep the charm	6/1/2018 2:23 PM
22	Both housing and recreation (other than boating) can be included in areas not right by the water. Tho I find them important, I wouldn't want them to crowd out pathways and small eateries and shops which could fit in well where walkabilty is key	6/1/2018 2:13 PM
23	Unimpeded access to the beach	6/1/2018 1:18 PM
24	Inline/Roller Skating park	6/1/2018 11:42 AM
25	A unique grocery or sundries store that boaters could take advantage of while docked at the pier. Evening Farmers market a great idea. Let the boating community know about it. Concerts, family activities	6/1/2018 10:46 AM
26	Maintaining Silvedale Waterfront Park for the many kids!	6/1/2018 10:21 AM
27	The waterfront is under-utilized for community activities, tourist opportunities and exposure to non-motorized recreation.	6/1/2018 10:16 AM
28	If you changed the facades (e.g like Seabeck Pizza) and focused on social types of business, etc. you would get a lot of us locals and non-locals to spend money in Silverdale like they do in Poulsbo, Gig Harbour, etc.	6/1/2018 9:46 AM
29	Farmers Market	6/1/2018 9:40 AM

Q5 What are your primary concerns with the existing pump station?(Rank 1-5 with 1 being not important to you/5 most important to you)

Answered: 490 Skipped: 45



	1 - NOT IMPORTANT	2 - LESS IMPORTANT	3 - NEUTRAL	4 - IMPORTANT	5 - VERY IMPORTANT	TOTAL	WEIGHTED AVERAGE
Visual appearance	7.20% 35	9.26% 45	30.04% 146	29.42% 143	24.07% 117	486	3.54
Odor control	0.82% 4	1.03% 5	5.75% 28	25.67% 125	66.74% 325	487	4.56
Noise control	2.90% 14	3.94% 19	21.99% 106	38.59% 186	32.57% 157	482	3.94
Interference with other park activities	3.53% 17	6.22% 30	21.58% 104	32.37% 156	36.31% 175	482	3.92
Safety	3.31% 16	2.27% 11	16.12% 78	28.10% 136	50.21% 243	484	4.20
Traffic	11.00% 53	12.86% 62	37.97% 183	22.20% 107	15.98% 77	482	3.19

#	OTHER (PLEASE SPECIFY)	DATE
1	Yes it is a clear obstruction; do we have the resources to relocate it? What would the challenges be? A void in its spot will obviously a clearance for space potential to the already limited area.	7/1/2018 12:16 AM
2	Move it as far as possible from the waterfront.	6/30/2018 11:06 PM
3	After multiple sewage spills during the Bucklin Hill project (did anybody ever pay a fine or get fired???), let's keep the sewage out of Dyes Inlet, for pity's sake.	6/21/2018 7:51 PM
4	Move it	6/17/2018 7:19 PM
5	conforms to environmental protection goals	6/14/2018 1:06 PM

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6	The pump station should get a makeover possibly like the bathrooms at the Port Orchard marina where they have built a look out deck with a couple of those pay telescopes or something. That would help incorporate the pump station into the park and make it an destination attraction.	6/11/2018 11:55 AM
7	It is such an eyesore in such a beautiful location! It's too bad it can't be moved and use this place to enjoy the view.	6/10/2018 10:08 AM
8	concerned about failures during storm/high tide occurences	6/8/2018 11:27 AM
9	No leaks or overflow	6/5/2018 5:02 PM
10	What do you mean by "Traffic" in this conext?	6/4/2018 4:27 PM
11	Possible spills	6/4/2018 4:01 PM
12	Safety from small children hurting themselves on equipment associated with the station. Appearance needs to be beautified since it sits right in front of the water. Add some art to it, paint the outside, maintain the outside aesthetics.	6/4/2018 12:57 PM
13	It never bothered me and never knew what it was for	6/4/2018 12:20 PM
14	I do not ever get in the water here, because there are always warnings about toxins in the water from surrounding sewage spills or localized marine bacterial activity.	6/4/2018 8:46 AM
15	why would it interfere with traffic at its current location	6/3/2018 7:45 PM
16	If you change the size if the pump station pipes you also have to change the size of the pump, the foundations and the electrical inputs. The pipes don't care where the pumps are located. The pumps could be located anywhere along Washington Avenue. The vacant lot at the corner of Carlton and Washington. The old Fire Station-Silverdale Water-Kitsap Housing building, whatever. Building an entirely new pump station while still in the process of replacing the old piping with new, with a little planning, could actually reduce the change-over impact. Th argument for not relocating the pump station is short sited.	6/3/2018 12:56 PM
17	Place below raised ground to hide and muffle noise.	6/2/2018 3:52 PM
18	please move it somewhere else	6/2/2018 3:39 PM
19	I am shocked to hear that the pump station cannot be moved. We should revisit this issue. What a shame that we would consider any upgrades to Old Town Silverdale without considering movement of the pump station.	6/2/2018 8:46 AM
20	better use of exisiting park area	6/2/2018 7:36 AM
21	This site serves as no traffic impact to the area. Visibility of the area could be improved.	6/1/2018 8:03 PM
22	I'm glad you are addressing the odor control - that is significant to me.	6/1/2018 6:26 PM
23	The overflow of toxic waste into the water, closing the beaches of shell fish.	6/1/2018 5:42 PM
24	MOVE IT!! Plan adequately for a rapidly growing community!	6/1/2018 5:32 PM
25	The decision to keep this on the waterfront needs to be re-examined. This is total nonsense and a mistake. What other commumunity around Puget sound has decided to put waste management convenience as more important that their primary town focal point? We can spend millions of dollars on a bridge but were trying to save some money by keeping the pump station in the middle of the primary recreation area for Silverdale? It was a bad decision that needs to be fixed.	6/1/2018 3:56 PM
26	It's. It that bad, but spot is premium and could be a great site for boat rentals, pole and bait shop, kayak bike rental, just about anything.	6/1/2018 2:29 PM
27	This is a horrible visual and smelly building to be on our waterfront. This is the most scenic place in Silverdale and needs to be removed PERIOD in spite of cost.	6/1/2018 2:21 PM
28	Safety to the environment	6/1/2018 2:14 PM
29	Ugly part of this park. How to make it a part of something functional to public like a viewing platform.	6/1/2018 12:49 PM
30	longevity of updates and budget	6/1/2018 12:12 PM
31	Spillage into water ways.	6/1/2018 11:44 AM
32	Environmental/human impact of leaks	6/1/2018 10:51 AM

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33	Sailors and beach users are exposed to contaminated water, this is a safety concern	6/1/2018 10:33 AM
34	Relocation of Dumpsters would be a big plus!	6/1/2018 10:25 AM
35	Occupies prime waterfront space that could be used for recreational and tourist activity.	6/1/2018 10:17 AM
36	We enjoy taking out our sailboat, kayaks, and paddle boards and will often invite friends to meet us there at the waterfront. But the smell is horrible! If we pack a picnic dinner, it's hard to eat because of the smell coming from the pump station. Please relocate (it takes up prime waterfront real estate) or deal with the smell!	6/1/2018 10:16 AM
37	Shouldn't really be wasting the only public access with this. It should be moved.	6/1/2018 9:52 AM

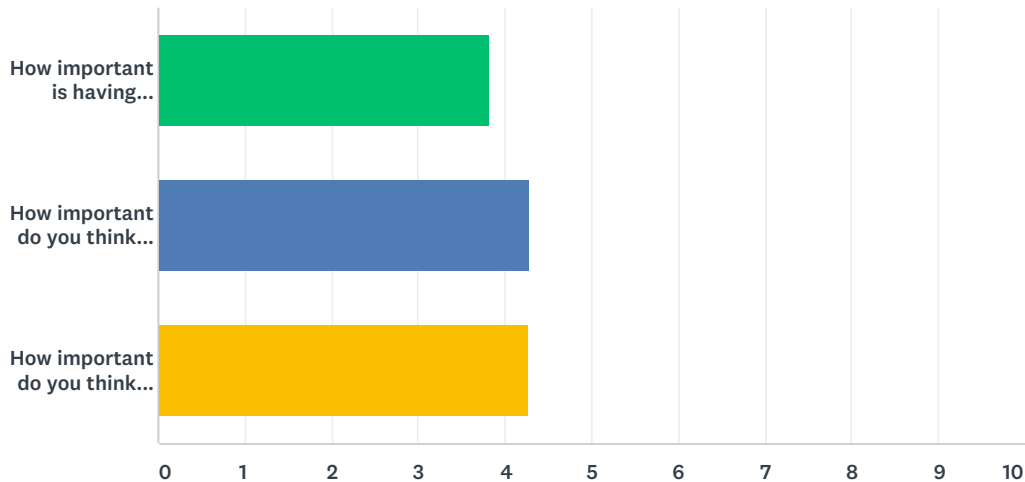
Q6 Please enter your e-mail address (or mobile phone number for text messages) below if you would like to receive project updates from Kitsap County Public Works.

Answered: 204 Skipped: 331

ANSWER CHOICES	RESPONSES	
Name	0.00%	0
Company	0.00%	0
Address	0.00%	0
Address 2	0.00%	0
City/Town	0.00%	0
State/Province	0.00%	0
ZIP/Postal Code	0.00%	0
Country	0.00%	0
Email Address	92.16%	188
Phone Number	55.39%	113

Q7 Please share with us your interests in use of Port facilities.(Rank 1-5 with 1 being not used-not important to you/5 most used-most important to you)

Answered: 456 Skipped: 79



	1 - NOT IMPORTANT	2 - LESS IMPORTANT	3 - NEUTRAL	4 - IMPORTANT	5 - VERY IMPORTANT	TOTAL	WEIGHTED AVERAGE
How important is having active and useful Port facilities to you personally?	6.36% 29	5.48% 25	20.61% 94	33.77% 154	33.77% 154	456	3.83
How important do you think having active and useful Port facilities is to Old Town Silverdale?	1.32% 6	1.10% 5	10.96% 50	41.01% 187	45.61% 208	456	4.29
How important do you think having active and useful Port facilities is to the Silverdale community as a whole?	1.32% 6	1.54% 7	11.43% 52	40.00% 182	45.71% 208	455	4.27

#	ARE THERE FACILITIES AT THE PORT THAT YOU DO NOT LIKE/FEEL ARE INAPPROPRIATE?	DATE
1	Not sure. However, perhaps re-paint rusty surfaces and renovate equipment. General safety check.	7/1/2018 12:41 AM
2	Old Town Pub	6/30/2018 11:29 PM
3	Too many hair salons	6/30/2018 8:44 AM
4	The Port owns a lot of property. They have vacant land that is used to store Port equipment. WHY can't that equipment be stored out of Old Town and use the Old Town land for boat storage or at least look decent and well kept.	6/28/2018 5:54 PM
5	Bars / Pubs /	6/21/2018 9:15 PM
6	I just wish they would do MORE. C'mon!!! Sitting on your hands is NOT being a good steward of taxpayer resources.	6/21/2018 8:01 PM
7	Need to get rid of old town pub asap	6/17/2018 11:30 PM
8	the routy bar which has now been closed	6/15/2018 8:44 PM

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9	No	6/13/2018 8:11 PM
10	I love watching sailboat lessons	6/13/2018 8:02 PM
11	No.	6/13/2018 1:15 PM
12	The sewer pump station.	6/11/2018 7:22 PM
13	Proper development of "old town" will enhance the entire Silverdale area and help give the "city" a core identity.	6/11/2018 12:04 PM
14	The Old Town Pub building needs to be demolished!	6/10/2018 10:19 AM
15	Sewer pump station	6/10/2018 9:45 AM
16	A top priority of improving Old Town Silverdale should be the removal of the mobile home, occupied by two people, which has been parked in front of the library for months and months!	6/8/2018 9:22 PM
17	The remodeled playground is very limiting to only young children.	6/7/2018 8:26 PM
18	Having properties taken off the tax rolls is in direct conflict with the port's mission for economic development.	6/7/2018 4:56 PM
19	Pump Station location should be moved away from water's edge.	6/5/2018 5:07 PM
20	Silverdale Port people are like the Gestapo, they think it belongs to them.	6/5/2018 10:36 AM
21	it needs more police the entire area is an open air drug market	6/5/2018 7:35 AM
22	I don't know what port facilities are.	6/4/2018 8:20 PM
23	That pump station could go away!	6/4/2018 7:44 PM
24	Housing, auto shops and medical.	6/4/2018 1:36 PM
25	I would love to see less boat storage & more public use; similar to Pouslbo waterfront. SO much potential!	6/4/2018 1:00 PM
26	some buildings are run down and ugly looking.	6/4/2018 9:20 AM
27	I need to be able to park by the bathrooms for 5 minutes, but during the summer this is prohibited for anyone without a boat trailer. Can you please dedicate 1-4 parking spots for 15 minute bathroom usage? I would even pay an annual park fee to support this and help cover facility maintenance costs.	6/4/2018 8:55 AM
28	No	6/4/2018 8:55 AM
29	Old Town doesn't need any more hair salons and business offices/law firms should not be on the waterfront (waterfront should be saved for community spaces)	6/4/2018 8:22 AM
30	Please, keep open space within the natural environment. Keep it an area that "teaches" people how to safely but enjoy environmental practices, ie. rain gardens, native shoreline plant area, a possibly signage of shorebirds. NO tacky chain restaurants, please and not stacked upon stack of condos. Keep the natural shoreline and views in tact	6/4/2018 8:03 AM
31	a lot of attention seems to be focused on non motorized. They are nice, but I would like to see Silverdale as a destination for more power and sailboats.	6/3/2018 1:41 PM
32	I feel that the county not helping the business.property owners with the impact of the county's construction projects is very inappropriate.	6/3/2018 1:03 PM
33	No boat. Don't use the port facilities.	6/3/2018 7:37 AM
34	No	6/2/2018 7:13 PM
35	The condemned bar	6/2/2018 6:00 PM
36	Not sure	6/2/2018 4:14 PM
37	As a non-resident of the area now, I will leave these questions to local residents.	6/2/2018 10:05 AM
38	The parking lot adjacent to the park should be moved (relocated to north blocks of Old Town Silverdale. Currently, there is a lot of misuse of the parking area where cars speed around, peel out, drink alcohol, smoke marijuana, play loud music.	6/2/2018 9:14 AM
39	I don't know what the port offers	6/2/2018 4:48 AM

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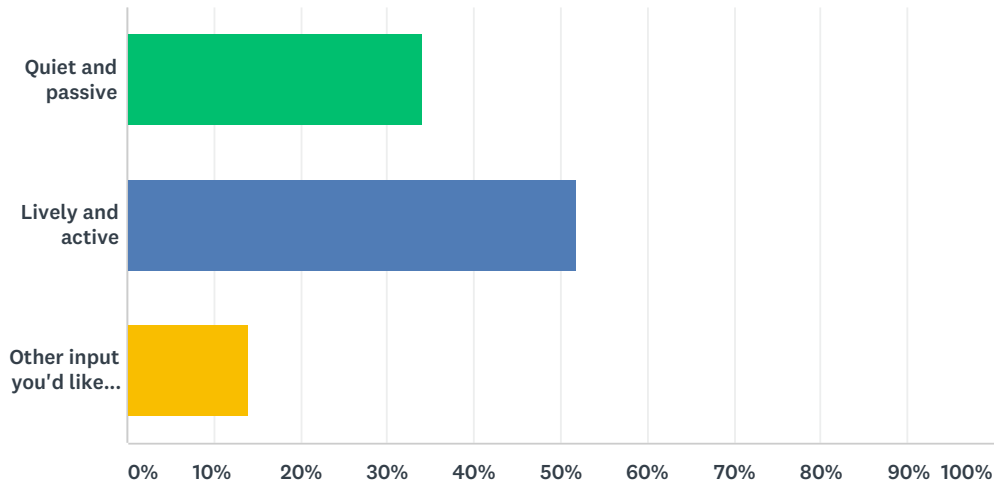
40	I feel like there are too many salons in the area	6/1/2018 11:20 PM
41	Condos/apartments	6/1/2018 10:56 PM
42	The sewage treatment plant is an eyesore and should be moved	6/1/2018 10:28 PM
43	No	6/1/2018 10:17 PM
44	I would like the Port to move the dumpster, update their website and non motorized boating facilities	6/1/2018 9:04 PM
45	Port facility is not exclusive of water and water craft services solely. Mubh more land based property is available for improvement. The culture and history of the community must be sustained.	6/1/2018 8:24 PM
46	I think they are providing adequate facilities currently and they are benefiting the community.	6/1/2018 6:47 PM
47	No	6/1/2018 6:07 PM
48	Expand the 1 and only playground for the entire town of Silverdale!!!!!!	6/1/2018 5:40 PM
49	The old town bar is an eyesore and needs to come down.	6/1/2018 4:04 PM
50	no	6/1/2018 3:53 PM
51	Homeless People	6/1/2018 2:52 PM
52	I love our waterfront. I love the sailing lessons my kids got to enjoy, the park is great, what could make it better is perhaps a community store, parks and rec rental shop, ice cream perhaps tie it in with some history. Silverdale waterfront used to be the hub of egg ferry traffic to Seattle! Add some old historic images and give it a sense of history.	6/1/2018 2:48 PM
53	To be honest I have not used them much. I walk through the park periodically.	6/1/2018 2:36 PM
54	Anything that would overshadow variety of uses.	6/1/2018 2:26 PM
55	The current parking lot is hideous and not user friendly.	6/1/2018 1:47 PM
56	Very well used beautiful part of Silverdale. Must be preserved for all public	6/1/2018 12:57 PM
57	no	6/1/2018 12:26 PM
58	Pump house	6/1/2018 12:20 PM
59	I believe this park should be treated like Poini No Point, which is left more natural. Not structured with programmed uses and entertainment.	6/1/2018 12:10 PM
60	no	6/1/2018 11:49 AM
61	My concern is with the lighting in the parking lot and the facilities over by the boat launch. It is often not well lit and I feel like the facilities are in a dark not easily accessible corner. I have a small business in Old Town and often park in the port lot. Sometimes there is no lighting and with the increasing amount of activity in the park it's a concern.	6/1/2018 11:31 AM
62	Auto repair	6/1/2018 11:30 AM
63	Poor parking at the park.	6/1/2018 11:14 AM
64	not certain, no information	6/1/2018 11:09 AM
65	No	6/1/2018 11:02 AM
66	Parking is a big problem and must be considered.	6/1/2018 10:47 AM
67	The old town pub is in such poor condition, that despite its history, it is time for it to be replaced. The story can still be told with a museum section in a new facility. The property as a whole needs to be better used and planned for improved future use.	6/1/2018 10:47 AM
68	Surface parking lots	6/1/2018 10:42 AM
69	The waterfront park area is very well designed, and as a result very well used by people, as well as birds and other animals. Maintaining the existing infrastructure should be the goal at this point, because the design is simply outstanding!	6/1/2018 10:26 AM
70	Covered sailboat storage and a community center for educational/recreational use would be nice. Pumphouse is an eyesore. Parking lot needs to be policed - lots of drug use, etc.	6/1/2018 10:24 AM

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71	Glad to see the Pub closed down. I was scared to take my kids to art lessons next door. Not the kind of crowd we want welcoming people to Silverdale.	6/1/2018 10:23 AM
72	I am not sure if this is the Port - but I think Farmland should be closed down	6/1/2018 10:04 AM
73	no opinion at this time	6/1/2018 10:00 AM
74	Huge parking lots next to the water. Parking could be a couple blocks away.	6/1/2018 9:58 AM
75	Limited use of boat trailer parking during non peak season. Could part of it be used for farmers market shoppers?	6/1/2018 9:48 AM
76	The sewer treatment facility	6/1/2018 9:38 AM
77	Housing. Please do not build housing on the waterfront. This resource should be available to the community, not a select few potential residents.	6/1/2018 9:34 AM

Q8 Please share your desired level of activity at Port facilities.

Answered: 451 Skipped: 84



ANSWER CHOICES	RESPONSES	
Quiet and passive	34.15%	154
Lively and active	51.88%	234
Other input you'd like considered:	13.97%	63
TOTAL		451

#	OTHER INPUT YOU'D LIKE CONSIDERED:	DATE
1	Can you put a cabana out there?	7/1/2018 12:41 AM
2	better security during the day.	6/30/2018 11:29 PM
3	The mall is used for shopping. Old Town could be seen as a place where sporting opportunities are available. Running teams, rowing and kayaking and sailing should be the focus. These groups bring in a lot of people of a weekly basis. This is something Old Town could be know for.	6/28/2018 5:54 PM
4	We've used the Port in both capacities; quiet walks as well as enjoying Whaling Day's and lots of activities.	6/26/2018 8:22 PM
5	I love that Silverdale does not have a fuel dock. However, I would love to see our port be a more vibrant and bustling place. How about a community center? A theater? Brew pubs are fun, but we are better than that. Let's bring some cultural appeal to Old Town!	6/21/2018 8:01 PM
6	I think parts that are quiet and passive and others that are lively and active would be a good mix so all people could come to get what they want	6/18/2018 11:23 AM
7	All of the above; diverse	6/17/2018 10:03 AM
8	A mix between the two above options is ideal with spaces to be quiet and passive and spaces with activities and events	6/15/2018 2:58 PM
9	It's a family oriented place. Peaceful enough for a walk but useful enough to have community events from time to time.	6/14/2018 1:12 PM
10	Both	6/13/2018 9:59 PM
11	Make it a destination place that people WANT to go. Like Poulsbo. Offer more things for boaters to when they come.	6/10/2018 10:19 AM

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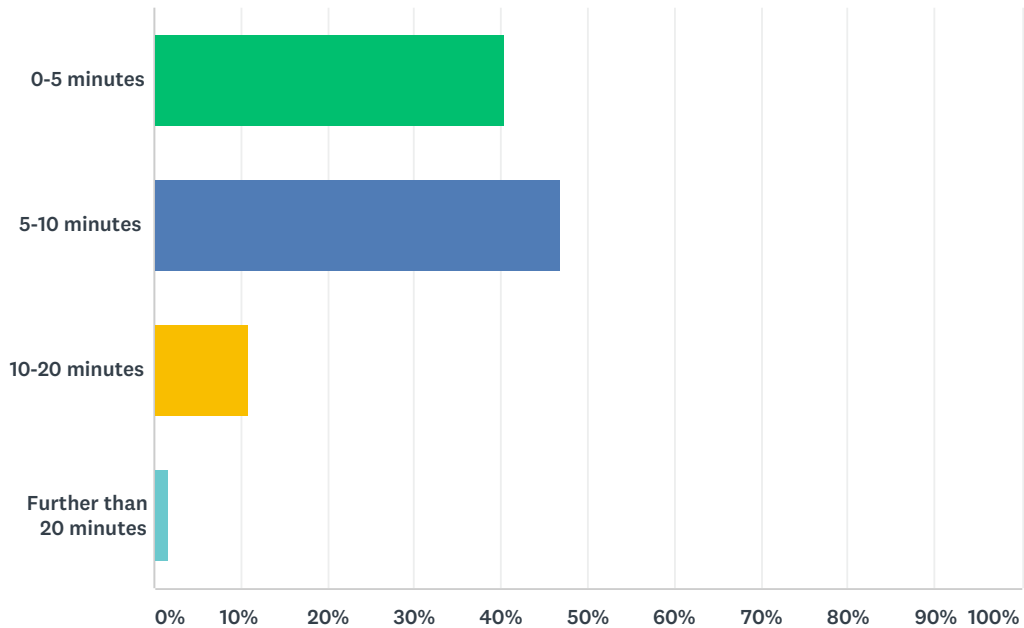
12	These need to be balanced in the total environment in old town. Time to really think and be creative on this one.	6/10/2018 7:59 AM
13	Both types of activities are necessary for entertainment value for many generations that live/visit Silverdale.	6/9/2018 12:29 PM
14	one side quiet, one side lively or different times for quiet and lively	6/8/2018 4:14 PM
15	Daily but growing incredibly difficult	6/8/2018 1:14 PM
16	Economic development of the Silverdale UGA	6/7/2018 4:56 PM
17	Lively and active without a lot of noise so as to be pleasant for nearby residents.	6/5/2018 8:49 AM
18	All!! I like festivals and gatherings but I also enjoy the quieter times. I think Silverdale can accommodate everything!	6/4/2018 7:44 PM
19	Some where between.	6/4/2018 1:36 PM
20	It could help the community if we added a gas pump for motorized vehicles. It would bring in boat customers and give chances to bring visitors to the local shops.	6/4/2018 1:01 PM
21	Activities will vary in their function & scale of noisy levels - it's not important to me.	6/4/2018 11:27 AM
22	I would like quiet for better office work environment, but active as we want people to know where we are and use our services.	6/4/2018 9:20 AM
23	I will help with. desired change.	6/3/2018 8:17 AM
24	Balance between both	6/3/2018 7:27 AM
25	Family friendly	6/2/2018 8:39 PM
26	I am part of Clam Island Rowing, a non-motorized on the water recreational and competitive program for crew racing. We desperately need a building to house our racing shells, equipment, workout facilities.	6/2/2018 6:58 PM
27	a mix of quiet and lively. quiet most days/nights, but occasional celebrations are nice.	6/2/2018 5:40 PM
28	A mix of both	6/2/2018 4:14 PM
29	There should be a combination of activities through out the weeks & months. Some quiet and others more active likee festivals & ourdoor concerts.	6/2/2018 3:18 PM
30	It's level of usage throughout the year seems appropriate	6/2/2018 1:30 PM
31	Moving parking lots away from the waterfront (relocate/build parking garage to north end of Old Town Silverdale). Continuous boardwalk from Waterfront Park to Bucklin Hill.	6/2/2018 9:14 AM
32	it could/should be a combo of both passive and active	6/2/2018 7:47 AM
33	Activity level should vary depending on what's going on.	6/1/2018 11:09 PM
34	I would like to be able to walk along as much shoreline as possible without having to go inland from the beach.	6/1/2018 10:28 PM
35	Lively and active with an increased presence of a walking business area that has an attraction 7 days a week. Small shops, eateries, and amenities which improve the attraction and desirability to come to the Old Town community	6/1/2018 8:24 PM
36	I like the farmer's market located there. We need more family-oriented activities there, too, provided at no cost to the public.	6/1/2018 6:47 PM
37	They can be active as long as it's constructive and there is room for everyone	6/1/2018 6:07 PM
38	It depends, personally I like both as options.	6/1/2018 4:36 PM
39	Quiet and active	6/1/2018 4:29 PM
40	Kid friendly!	6/1/2018 4:13 PM
41	Family oriented	6/1/2018 4:04 PM
42	a mix of the two; inclusion and welcoming	6/1/2018 3:53 PM
43	Question should be reframed. Lively and active is great if it's about outdoors and families enjoying park. But NOT ok if it turns into waterfront bars and roudy drinking area.	6/1/2018 2:48 PM

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44	It should be all at times quiet, passive and lively and active.	6/1/2018 2:26 PM
45	A mix of both.	6/1/2018 12:22 PM
46	I promote outdoor activities for everyone; however, due to this location, quiet and passive should not be a focus with so many other nearby areas to achieve that environment. Kids playing, boaters, and great eateries are not always quiet.	6/1/2018 12:16 PM
47	Don't spoil this wonderful area with what some people might view as "improvement ". It is the only area Silverdale has left which offers a bit of nature and ties so nicely with our trail. THE PARK AND THE TRAIL COMBINATION LEFT IN ITS NATURAL, MORE SERENE STATE IS WHAT MAKES IT SO SPECIAL...AND RARE. THE FUTURE GENERATIONS WILL TREASURE SUCH AN EXPERIENCE.	6/1/2018 12:10 PM
48	Sidewalks that are close to the water that offer walking and inline skating for all.	6/1/2018 11:49 AM
49	uncertain what level of activity is needed	6/1/2018 11:09 AM
50	A mix of quiet and passive on the water and beach Lively active in cafes and special weekly time specific events in the park and business areas.	6/1/2018 11:01 AM
51	Mixed	6/1/2018 10:59 AM
52	I don't like these descriptors. I would say a "quiet and serene" waterfront atmosphere that encourages "active" exploration and enjoyment by water and by foot.	6/1/2018 10:56 AM
53	Safety issues for youth users remains a concern with the amount of inappropriate activity such as drug users and alcohol.	6/1/2018 10:47 AM
54	The two options here should not be mutually exclusive. There is room for both types.	6/1/2018 10:42 AM
55	I think it should be a mix of both quiet and active to accommodate many different function	6/1/2018 10:36 AM
56	Keep the noise/music level down so when the windows are closed in my home up the hill I'm not disturbed.	6/1/2018 10:34 AM
57	Not sure how to answer this question.	6/1/2018 10:16 AM
58	More beach -sand/water- access. For example the Edmonds waterfront by both sides of ferry dock or farther down by the boat docks.	6/1/2018 10:16 AM
59	Lively and active for the local people, their enjoyment, entertainment.	6/1/2018 10:00 AM
60	Lively and active and non-destructive and not overly noisy.	6/1/2018 9:58 AM
61	Both! There should plenty of quiet recreation time there...but it is great for special events, bands, fairs, etc. now and then too.	6/1/2018 9:42 AM
62	A marine boating restaurant availability would be great, Silverdale is sorely lacking in restaurants	6/1/2018 9:38 AM
63	both	6/1/2018 9:31 AM

Q9 When you visit the Port either by water or driving and wish to park once for the day, how far will you walk to access the nearby services/activities?(Check one)

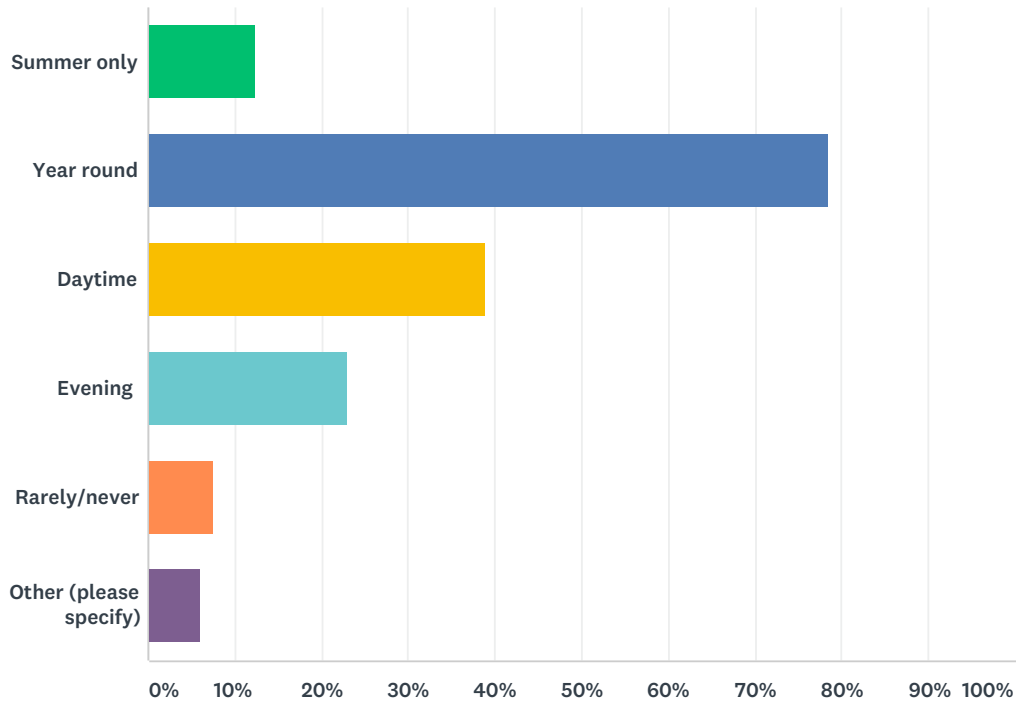
Answered: 460 Skipped: 75



ANSWER CHOICES	RESPONSES	
0-5 minutes	40.43%	186
5-10 minutes	46.96%	216
10-20 minutes	10.87%	50
Further than 20 minutes	1.74%	8
TOTAL		460

Q10 When do you visit Port Facilities?(Check all that apply)

Answered: 460 Skipped: 75



ANSWER CHOICES	RESPONSES
Summer only	12.39% 57
Year round	78.48% 361
Daytime	38.91% 179
Evening	23.04% 106
Rarely/never	7.61% 35
Other (please specify)	6.09% 28
Total Respondents: 460	

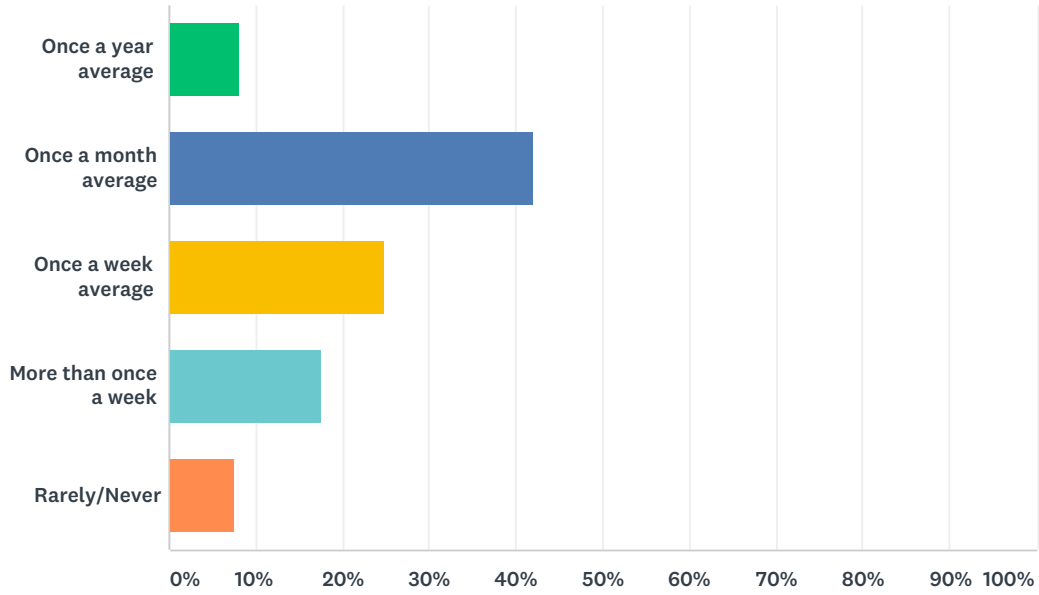
#	OTHER (PLEASE SPECIFY)	DATE
1	When good weather, like most.	7/1/2018 12:41 AM
2	early morning	6/29/2018 8:33 AM
3	Some evening in the summer but mostly day use	6/18/2018 11:23 AM
4	All hours.	6/11/2018 12:04 PM
5	Mostly when grandkids are visiting. The beach and playground areas are great to have.	6/9/2018 12:29 PM
6	occasional evening during Whaling Days. Have also rented paddle board there on a few occasions.	6/8/2018 11:51 AM
7	Evening is to dangerous.	6/5/2018 10:36 AM
8	Sunny days	6/4/2018 12:25 PM
9	Various times and seasons	6/4/2018 11:27 AM

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10	I visit the Port Facilities to use the bathroom or eat lunch about 90% of the times that I visit Silverdale. I do 50-75% of my errands in Silverdale.	6/4/2018 8:55 AM
11	dining evening all year	6/3/2018 3:03 PM
12	When I go biking, visit farmers markets, and eat lunch.	6/2/2018 4:14 PM
13	summer and spring	6/2/2018 3:49 PM
14	Kitsap Sailing & Rowing Foundation and the Port of Silverdale partner to offer youth sailing (summer sailing camps, high school sailing teams) summer, fall, and spring	6/2/2018 9:14 AM
15	probably visit once a month	6/2/2018 7:47 AM
16	Only if invited to a function at the Park.	6/1/2018 10:17 PM
17	A compatible agreement with public transit would provide a way of providing access to various areas of the community without impacting the streets. There is not access for large volume parking.	6/1/2018 8:24 PM
18	It's a very nice place for some quiet time. Buy lunch at Monica's Cafe and sit and watch the bay area. It's very calming. Whaling Days are fun, too. It's something to do and there's not much of anything offered in the Silverdale area.	6/1/2018 6:47 PM
19	Weekends	6/1/2018 6:42 PM
20	Not able to access because of disability	6/1/2018 11:09 AM
21	Old town resident/business owner	6/1/2018 11:02 AM
22	I love the Old Town Silverdale. Many great small businesses here. Road construction is crippling them.	6/1/2018 11:01 AM
23	I am there before work and for lunch almost every day, for an hour or less each time. I am in my car, parked by the boat launch, and am one of many who use the park this way.	6/1/2018 10:26 AM
24	I would visit more often if you brought in the right businesses. You can have insurance, auto, salon, businesses anywhere else in town or off of the main waterfront road.	6/1/2018 10:00 AM
25	never at night - due to homeless and drugs	6/1/2018 9:58 AM
26	When it's nice outside	6/1/2018 9:42 AM
27	We boat there as well as eat dinner at least once a week in the Old Town area (Cash Brewing)	6/1/2018 9:35 AM
28	all the above	6/1/2018 9:31 AM

Q11 How often do you visit Port facilities?(check one)

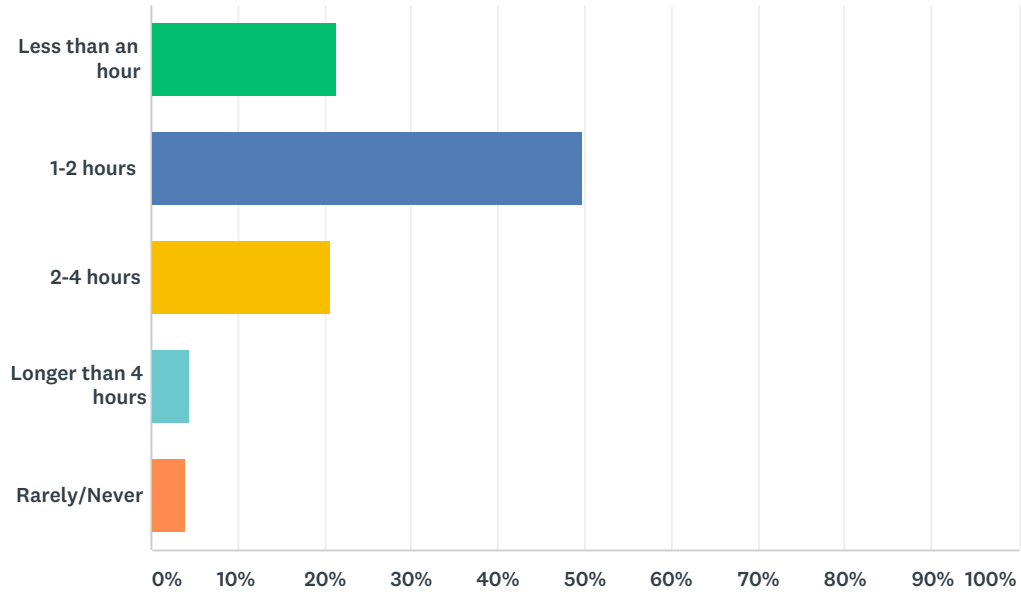
Answered: 455 Skipped: 80



ANSWER CHOICES	RESPONSES	
Once a year average	8.13%	37
Once a month average	41.98%	191
Once a week average	24.84%	113
More than once a week	17.58%	80
Rarely/Never	7.47%	34
TOTAL		455

Q12 How long do you typically stay?(Check one)

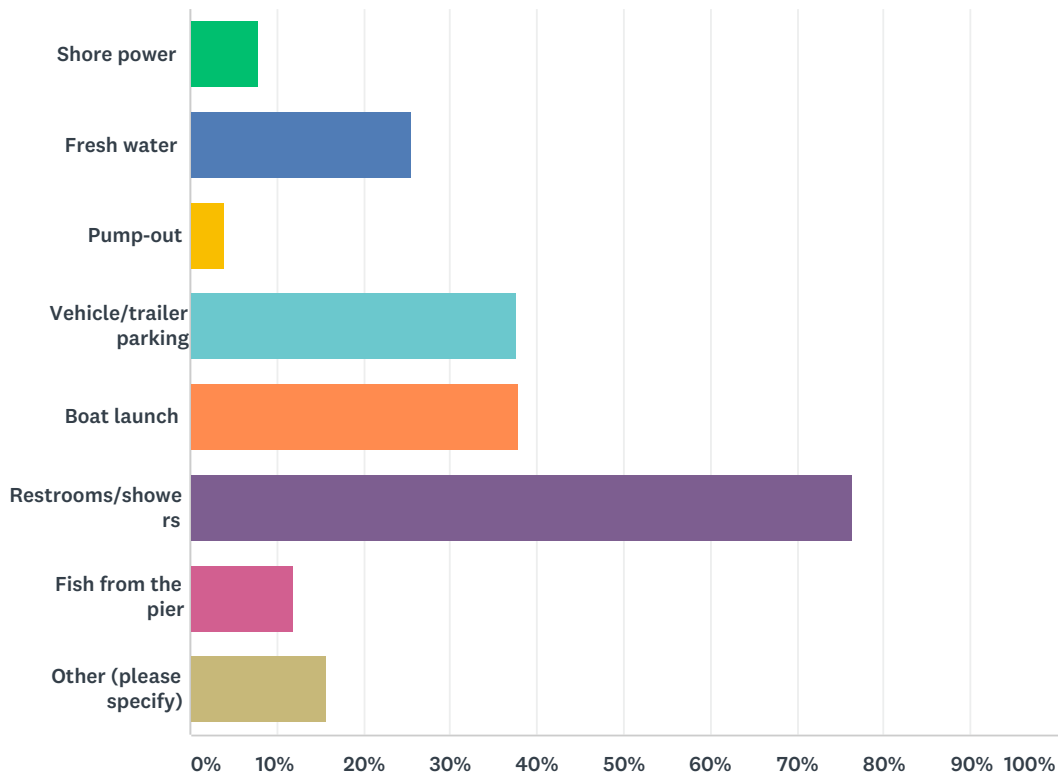
Answered: 460 Skipped: 75



ANSWER CHOICES	RESPONSES	
Less than an hour	21.30%	98
1-2 hours	49.78%	229
2-4 hours	20.65%	95
Longer than 4 hours	4.35%	20
Rarely/Never	3.91%	18
TOTAL		460

Q13 If you use water dependent services, what services do you use? (check all that apply)

Answered: 361 Skipped: 174



ANSWER CHOICES	RESPONSES
Shore power	8.03% 29
Fresh water	25.48% 92
Pump-out	3.88% 14
Vehicle/trailer parking	37.67% 136
Boat launch	37.95% 137
Restrooms/showers	76.45% 276
Fish from the pier	11.91% 43
Other (please specify)	15.79% 57
Total Respondents: 361	

#	OTHER (PLEASE SPECIFY)	DATE
1	Sailing/rowing storage	7/1/2018 6:39 AM
2	None, but probably important to others.	7/1/2018 12:41 AM
3	Windsurfer rigging and launching from the beach	6/30/2018 11:29 PM
4	launching rowing shells	6/29/2018 8:33 AM

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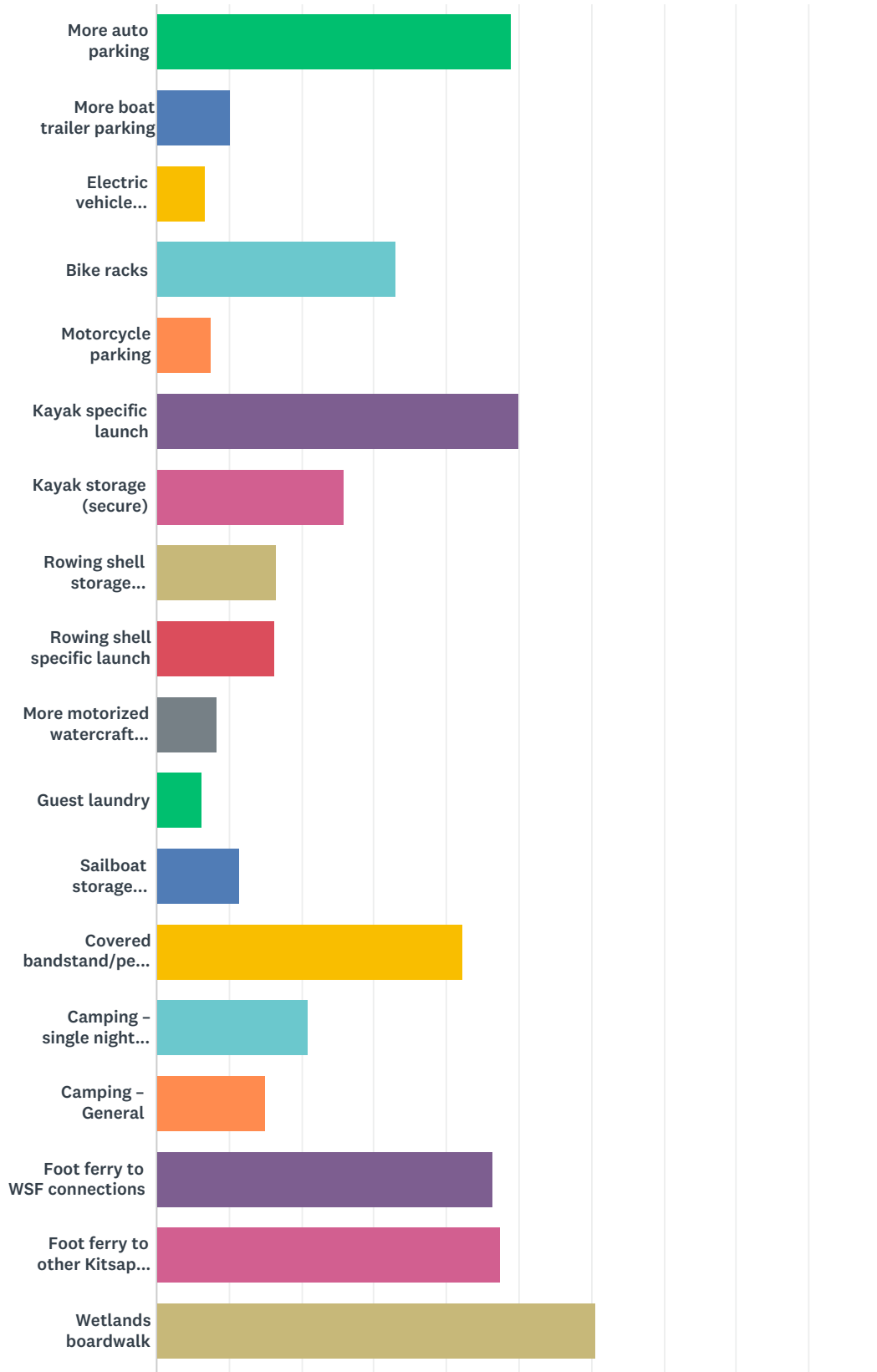
5	Boat storage	6/28/2018 5:54 PM
6	play areas	6/27/2018 10:09 AM
7	walk on pier	6/22/2018 7:30 PM
8	Walk on the pier. Take plankton samples. Walk on the beach.	6/21/2018 8:01 PM
9	launch our kayaks	6/20/2018 1:13 PM
10	Have picnics bring the grand children to play or just have a peaceful walk on the pier teaching the grandkids about history or the sea creatures	6/18/2018 8:54 AM
11	I just walk the trail, but the rest rooms are important.	6/14/2018 12:21 PM
12	na	6/13/2018 11:08 PM
13	Pier and floats for wildlife viewing	6/13/2018 8:53 PM
14	Water fountains	6/13/2018 8:11 PM
15	We walk the dock and stroll through town- we love the canoes and flag memorial and new kids' playground. The parking lot is too big and it would be nice to move the sewer bldg. But it could be remodeled into a building that looked like one of the originals there. Dumpster is on prime view area. Could all that be moved underground?	6/13/2018 8:02 PM
16	Picnic facilities.	6/13/2018 1:15 PM
17	Playground, trail	6/10/2018 10:19 AM
18	Don't use water dependent services	6/9/2018 5:52 PM
19	benches, trails, beach access	6/7/2018 4:56 PM
20	non-motorized boat storage	6/6/2018 8:45 AM
21	Dock	6/5/2018 5:08 PM
22	N/A	6/5/2018 5:07 PM
23	Launch my kayak.	6/5/2018 10:36 AM
24	Covered picnic area	6/5/2018 8:49 AM
25	none	6/5/2018 7:35 AM
26	none	6/4/2018 7:17 PM
27	I live in Chico and kayak down there. Need kayak secure area.	6/4/2018 3:36 PM
28	no services used	6/4/2018 11:27 AM
29	I use it as a pretty place to walk when I get a break from work	6/4/2018 11:21 AM
30	All of these are used in access to my work building in old town	6/4/2018 9:20 AM
31	enjoying the park, the trails around it, the local businesses, and thank goodness no chain restaurants too close to the water in Old Silverdale. Keep its charm.	6/4/2018 8:03 AM
32	Paddle out in a kayak	6/3/2018 4:35 PM
33	walk on pier and waterfront	6/3/2018 3:03 PM
34	The Park	6/3/2018 1:03 PM
35	None	6/3/2018 7:53 AM
36	N/A	6/3/2018 7:37 AM
37	none	6/2/2018 11:11 PM
38	Main wharf for our coach boat, handling wharf for temporary tie-ups.	6/2/2018 6:58 PM
39	all those should be available	6/2/2018 5:40 PM
40	N/A	6/2/2018 5:33 PM
41	Eat	6/2/2018 4:14 PM

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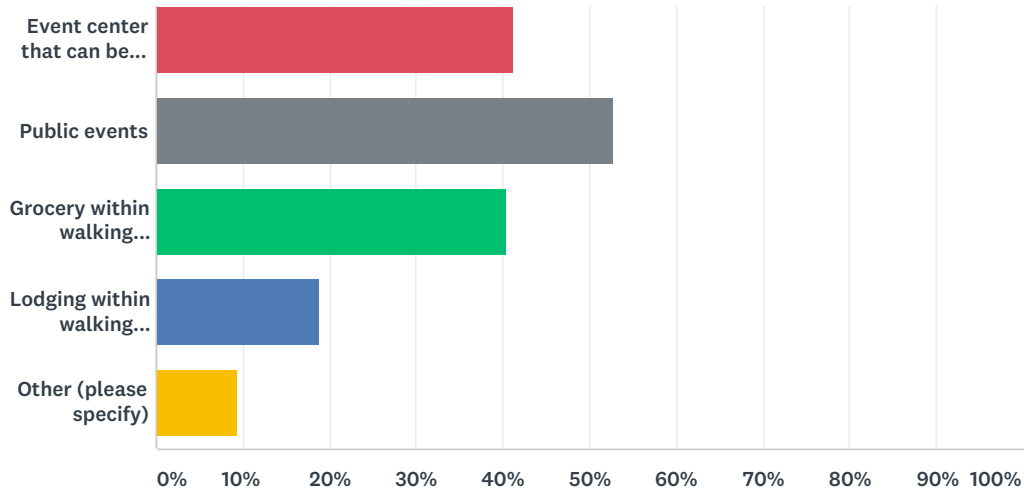
42	covered picnic area	6/2/2018 7:47 AM
43	I am part of the Central Kitsap Sailing Team and we use Port services all the time.	6/1/2018 10:36 PM
44	Walkways along the water	6/1/2018 10:28 PM
45	Rowing Club boat storage sailing club	6/1/2018 9:04 PM
46	Attend the Farmer's Market	6/1/2018 6:47 PM
47	Walk along the beach	6/1/2018 6:07 PM
48	outrigger club	6/1/2018 2:46 PM
49	Walking	6/1/2018 12:57 PM
50	none at present	6/1/2018 11:09 AM
51	Don't use water services.	6/1/2018 11:05 AM
52	none	6/1/2018 11:02 AM
53	Port of Silverdale Boat Yard	6/1/2018 10:56 AM
54	boat yard storage	6/1/2018 10:47 AM
55	High school sailing/regattas from the floating dock.	6/1/2018 10:24 AM
56	sail, row, paddle	6/1/2018 9:58 AM
57	No option to skip	6/1/2018 9:51 AM

Q14 What services would you like to see happen or would personally use if offered?(Check all that apply)

Answered: 456 Skipped: 79



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ANSWER CHOICES	RESPONSES	
More auto parking	48.90%	223
More boat trailer parking	10.31%	47
Electric vehicle charging stations	6.80%	31
Bike racks	33.11%	151
Motorcycle parking	7.46%	34
Kayak specific launch	50.00%	228
Kayak storage (secure)	25.88%	118
Rowing shell storage (secure)	16.45%	75
Rowing shell specific launch	16.23%	74
More motorized watercraft berths	8.33%	38
Guest laundry	6.36%	29
Sailboat storage (secure)	11.40%	52
Covered bandstand/performing arts area	42.32%	193
Camping – single night to support Kitsap Peninsula Water Trail visitors	20.83%	95
Camping – General	15.13%	69
Foot ferry to WSF connections	46.49%	212
Foot ferry to other Kitsap County destinations	47.59%	217
Wetlands boardwalk	60.75%	277
Event center that can be rented for meetings, parties, weddings	41.23%	188
Public events	52.63%	240
Grocery within walking distance	40.35%	184
Lodging within walking distance	18.86%	86
Other (please specify)	9.43%	43

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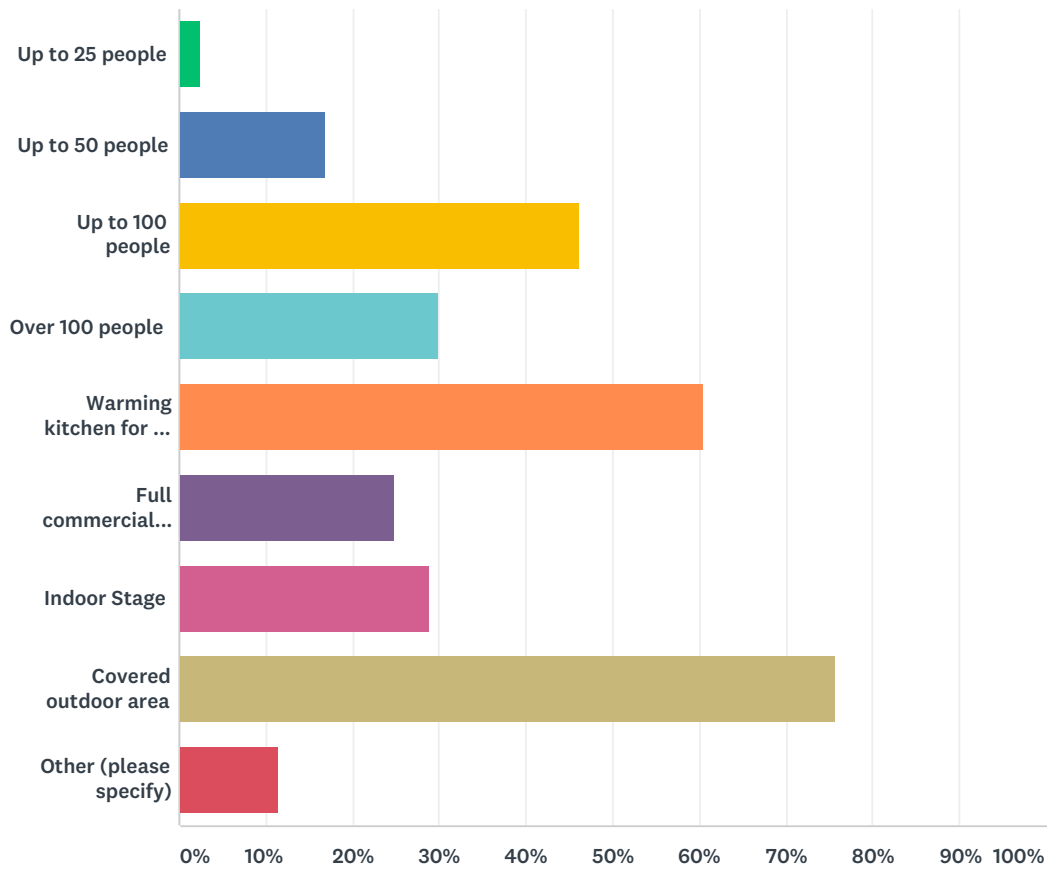
Total Respondents: 456		
#	OTHER (PLEASE SPECIFY)	DATE
1	Boathouse for sailing and rowing activities	7/1/2018 6:39 AM
2	Near food services.	7/1/2018 12:41 AM
3	Maybe a gift shop or our museum back for other visitors to see all the history	6/18/2018 8:54 AM
4	More water rentals, such as kayaks or boats	6/17/2018 9:57 PM
5	There's already covered area which could also work for bandstand/performing arts	6/15/2018 8:44 PM
6	Do not bring foot ferry into the port. That would defeat the overall central feeling of the place. Bremerton's parking issues are Bremerton's issues - not ours.	6/14/2018 1:12 PM
7	Other than some more parking, I do not believe that the port needs to be expanded.	6/14/2018 12:21 PM
8	N/A	6/8/2018 9:22 PM
9	Waterfront restaurants	6/5/2018 5:07 PM
10	Kayak specific parking.	6/5/2018 10:36 AM
11	Secure kayak area. Chico, Erlands Point, and Tracyton folks can kayak to Old Town. But then what? How to secure kayak while in shops and restaurants?	6/4/2018 3:36 PM
12	I don't want camping grounds. Only welcome more homeless to Silverdale. That's another issue on its own.	6/4/2018 12:25 PM
13	swimming	6/4/2018 11:24 AM
14	Nature trails, public restrooms, picnic spaces, gardens.	6/4/2018 8:55 AM
15	Please avoid stack upon stack of condos and overpriced hotels near the waterfront. The Silverdale Hotel needs to be somehow used more for tourists and its restaurant made cost wise. We have that facility; lets promote something that is already there. Needs an update, the rooms and keep them a decent price	6/4/2018 8:03 AM
16	Splash pad!!!	6/3/2018 10:41 PM
17	restaurants, ice cream, family friendly activities	6/3/2018 5:25 PM
18	Historic Old Town pub restoration	6/3/2018 3:03 PM
19	more small shops or eateries would attract more people, plus a better playground for the park.	6/3/2018 1:41 PM
20	Restoration of historic buildings	6/3/2018 7:37 AM
21	Farmers market	6/2/2018 11:11 PM
22	Rowing club equipment and workout space.	6/2/2018 6:58 PM
23	Kayak park	6/2/2018 4:14 PM
24	for the port to be actively involved in attracting small, independent retail businesses; Street lighting, and garbage and recycle receptacles on corners; angled parking along Byron at least; for the port website to have business locator maps that can be interactive with local business websites.	6/2/2018 9:16 AM
25	all of the above need to be in keeping with Old Town history look	6/2/2018 7:47 AM
26	WSF foot ferry connections is a brilliant idea!!	6/1/2018 6:47 PM
27	Perhaps vending machines with cold drinks (especially water) Andy snacks	6/1/2018 4:29 PM
28	ADA accessible park	6/1/2018 3:00 PM
29	Splash park for kids	6/1/2018 2:37 PM
30	Housing options in outer region of old town, where folks can walk to its offerings	6/1/2018 2:26 PM
31	Kayak racks	6/1/2018 2:26 PM

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32	Using this for an existing option, sort of. I'm disabled and would LOVE a foot ferry that combined other Dyes spots like the Tracyton pub, East Bremerton, etc as stops. Nothing would make me happier.	6/1/2018 1:30 PM
33	A sailing center similar to Port Townsend....maybe not as ambitious but it would be great not just for sailors but the local economy.	6/1/2018 12:55 PM
34	Dock space for Non motorized users	6/1/2018 12:52 PM
35	one- and two-person outrigger canoe storage	6/1/2018 12:41 PM
36	NO,NO, NO EVENT CENTER, NO GAS ENGINED BOAT CONVENIENCES, NOTHING THAT INVITES POLUTION , GARBAGE and NOISE!	6/1/2018 12:10 PM
37	Open air market, marina with motorized boat rental	6/1/2018 11:30 AM
38	Sailboat, kayak specific dock and on dock platform storage for program use	6/1/2018 10:47 AM
39	Accomodations for larger motorized boats; launch and moorage	6/1/2018 10:40 AM
40	Physical development of the waterfront park should be as minimal as possible - it's great the way it is, and every additional structure is likely to do more harm than good.	6/1/2018 10:26 AM
41	We currently store our sailboat at the waterfront.	6/1/2018 10:23 AM
42	Unlike other cities, Silverdale does not have obvious town center for celebrations, etc. Have the area help to build a sense of community!	6/1/2018 10:00 AM
43	bike trail	6/1/2018 9:34 AM

Q15 If an event center was available, how many people should it hold and what services would you expect?

Answered: 442 Skipped: 93



ANSWER CHOICES	RESPONSES
Up to 25 people	2.49% 11
Up to 50 people	16.97% 75
Up to 100 people	46.15% 204
Over 100 people	29.86% 132
Warming kitchen for a caterer or yourself to warm and serve food	60.41% 267
Full commercial kitchen to cook and prepare food from scratch	24.89% 110
Indoor Stage	28.96% 128
Covered outdoor area	75.79% 335
Other (please specify)	11.54% 51
Total Respondents: 442	

#	OTHER (PLEASE SPECIFY)	DATE
1	Community meets can be held here instead of hotel rooms.	7/1/2018 12:41 AM

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2	No Kitchen - just power and cold water.	6/30/2018 11:29 PM
3	Wedding Dressing Room	6/24/2018 6:35 AM
4	Current covered area is perfect	6/21/2018 9:15 PM
5	N/A	6/20/2018 1:13 PM
6	I do not support an event center.	6/19/2018 10:13 AM
7	I don't believe it is the best use of space	6/18/2018 11:23 AM
8	Wouldn't like to see an events center there	6/17/2018 10:03 AM
9	There's already an area to accommodate catering & area to prepare food. There is a covered outdoor area already!	6/15/2018 8:44 PM
10	I see no need for event center, if one was needed, and it could support its self, one would have been built already.	6/14/2018 12:21 PM
11	Performing arts center?	6/11/2018 12:04 PM
12	Where is room for event center? Remove some of the housing not shops? Would draw people to Old Town. Not too large as gets too expensive to use. Maybe something more familys can use for celebration events.s	6/10/2018 11:11 AM
13	not enough parking for events but marine traffic overnight moorage needs are incressing	6/8/2018 1:14 PM
14	I can see the value in this, but when you have something like this then parking is an issue. I placed parking as not as important because I wanted to encourage walking.	6/8/2018 11:51 AM
15	N/A	6/5/2018 5:07 PM
16	Although a full functioning kitchen would be nice such that classes might be held	6/5/2018 8:49 AM
17	no comment	6/4/2018 7:44 PM
18	meeting rooms (additional)	6/4/2018 6:44 PM
19	not in favor of an event center	6/4/2018 12:37 PM
20	If we are going to accommodate an events center, the parking situation needs to accommodate this with a smooth flow. Many small lots with narrow entrances and hairpin turns in a maze only causes congestion.	6/4/2018 8:55 AM
21	We have the Silverdale Hotel. Can't you expand there with that large empty lot next to it? No more large buildings when we have something that will work with some remodeling or adding and addition	6/4/2018 8:03 AM
22	Don't	6/3/2018 7:49 PM
23	I do not think there should be an event center at the waterfront	6/3/2018 5:25 PM
24	I do not think we need another event center here.	6/3/2018 4:42 PM
25	This could be a great place for CKHS reunions and Old Timer's Picnic and other events that celebrate Silverdale's past.	6/3/2018 7:37 AM
26	Farmers market	6/2/2018 11:11 PM
27	I think it is too cold much of the year for an outdoor stage	6/2/2018 5:08 PM
28	What is there now is adequate	6/2/2018 1:30 PM
29	Model example is Port Townsend's Maritime Center.	6/2/2018 9:14 AM
30	IF large indoor-built in keeping with history and if just outside area good also	6/2/2018 7:47 AM
31	N/A	6/1/2018 10:31 PM
32	Services for all ages, from babies to grandparents	6/1/2018 10:28 PM
33	Second story deck	6/1/2018 9:04 PM
34	I am beginning to like this survey!	6/1/2018 6:47 PM
35	Indoor play area and water viewing	6/1/2018 6:07 PM

Old Town Silverdale Waterfront Public Survey

36	Think this might be a bit much for this park area	6/1/2018 2:48 PM
37	Don't see a need for event center	6/1/2018 2:46 PM
38	No opinion. My concern is that an event center (with all the associated infrastructure needs, space (for facility, parking, concessions) and noise) would exclude too many of the other activities and resources we already enjoy and have a tradition of using. I am not a fan of the idea.	6/1/2018 2:36 PM
39	could be for over 100 if events spill out to outdoors?	6/1/2018 2:26 PM
40	Event bldg like island lake or fairgrounds eagles nest	6/1/2018 12:57 PM
41	You could have more regional sailing and rowing regattas here in silverdale if you built the right facility. By building a facility that was designed for cross purposes you could have sailing teams, rowing and Hawaiian paddling all housed in one facility. A large boat shed/meeting and event space for aquatic sports teams.	6/1/2018 12:55 PM
42	AV equipment for BKAT or live transmission of events.	6/1/2018 12:26 PM
43	Separate bathroom facility	6/1/2018 12:16 PM
44	No way! Not in this park!	6/1/2018 12:10 PM
45	Not an ideal location for this facility.	6/1/2018 11:05 AM
46	availability of parking and bathrooms!	6/1/2018 10:47 AM
47	Classrooms in addition to main event room, similar to Port Townsend Maritime Center. A viewing room and deck for watching on water events would be very well used and important for regattas and tying events into waterfront activities	6/1/2018 10:47 AM
48	There is an event center, and I've used it. It's open air, which fits perfectly with the use and appearance of the park.	6/1/2018 10:26 AM
49	Room that can be used for educational purposes (i.e. classroom, can bring a projector, etc)	6/1/2018 10:24 AM
50	I have not thought this through. However, no big building event center. Keep everything outdoors which is what a waterfront is all about!!!!	6/1/2018 10:00 AM
51	Event center is a bad idea	6/1/2018 9:40 AM

Old Town Silverdale Waterfront Public Survey

Q16 What services would you not want to see offered or not take place?

Answered: 194 Skipped: 341

#	RESPONSES	DATE
1	Less parking for motorized boat trailers would be good, currently there is too much, creating opportunity for bad behavior and loitering in the parking lot.	7/1/2018 6:39 AM
2	I do not want to see designated smoking areas.	7/1/2018 12:41 AM
3	Commercial fishing / shell fish harvesting	6/30/2018 11:29 PM
4	Skate park, parking garage, the green areas to be removed in place of businesses all three ideas would make me avoid the area. They would take away from the relaxing atmosphere.	6/30/2018 6:19 AM
5	I do not want the area to become too commercial. Part of its attraction is its small town feel.	6/29/2018 4:12 PM
6	I would like to see a boathouse similar to the Port Townsend facility that offers event space; training and education center for boater safety, woodworking, arts and crafts, and environmental learning programs; and storage for small craft boats like rowing shells and kayaks. I also would like to see branded signage that helps direct visitors to the area, information signs about the history of Silverdale, walkable and safe paths with lighting, public art, benches, identifiable trash cans and recycle hubs - clearly marked places to dump trash found on the beach or in the water, clean and safe bathrooms, and shower stations that allow you to rinse off your feed - Fort Warden has these options.	6/29/2018 2:27 PM
7	Do not want to see the Old Town Pub return or additional housing.	6/29/2018 2:12 PM
8	Increased motorized traffic.	6/29/2018 2:01 PM
9	hotels, parking garage	6/28/2018 5:54 PM
10	No housing or commercial launch	6/27/2018 12:10 PM
11	Hydro races (used to have them in Poulsbo waterfront).	6/25/2018 8:37 AM
12	Loud performances	6/24/2018 7:20 AM
13	Shore Camping; bars; pubs;	6/21/2018 9:15 PM
14	Let's not have "Hemp Fest." Other than that... bring it!	6/21/2018 8:01 PM
15	N/A	6/21/2018 6:00 PM
16	Nothing that would attract the homeless, drug use, promiscuity, crime or large crowds.	6/21/2018 5:18 PM
17	Should not support Seafair- Silverdale should have its own festival	6/21/2018 5:10 PM
18	Beer Gardens	6/21/2018 4:19 PM
19	senior housing development	6/20/2018 3:34 PM
20	I don't want to see increased human disruption of ecological habitats.	6/19/2018 10:13 AM
21	I don,t think there should be an events center built, I think the waterfront park is great for community events and they should be outside.	6/18/2018 1:30 PM
22	I would want a rental venue for weddings or other celebrations which takes up space and leaves locals with less space to walk	6/18/2018 11:23 AM
23	No marijuana shops or stuff like that just fun things for kids carnival rides family orientated, music that's fun not screeching!!☺	6/18/2018 8:54 AM
24	Camping	6/17/2018 10:25 PM
25	More retail and restaurants	6/17/2018 1:42 PM
26	Housing!!!	6/17/2018 1:18 PM
27	Don't know	6/17/2018 11:51 AM

Old Town Silverdale Waterfront Public Survey

28	Loud music	6/17/2018 11:20 AM
29	Not to become too commercialized, to be kept as a park to be enjoyed. I love walking the area w/my dogs (ALWAYS on leashes)	6/15/2018 8:44 PM
30	Housing. Public waterfront space is part of what makes our county so special. Housing is for one family, a park is for a whole community.	6/15/2018 6:49 PM
31	No liquor	6/15/2018 6:49 PM
32	NOT keeping the Old Town Pub	6/15/2018 1:02 PM
33	Overnight camping	6/14/2018 10:22 PM
34	National chain/retail. Like to keep it local, unique to area	6/14/2018 7:56 PM
35	Everything in this survey focused on recreation. Ferry service is the opposite - that is about work, commuting, parking, congestion. Bad idea.	6/14/2018 1:12 PM
36	?	6/13/2018 11:08 PM
37	Please leave the wetland areas alone unless helping to restore; partner with Kitsap County Parks to improve existing meeting/event/rental facilities-do not create new or duplicate services	6/13/2018 8:53 PM
38	Additional retail spaces	6/13/2018 8:11 PM
39	Drinking	6/13/2018 8:04 PM
40	Noisy water vehicles like small speedboats or jet skis Big events like Viking Fest Sewage spills	6/13/2018 8:02 PM
41	Dog Park	6/13/2018 7:22 PM
42	Do not have opinion	6/13/2018 7:22 PM
43	?	6/13/2018 1:15 PM
44	RV camping. More boat trailer parking	6/11/2018 7:22 PM
45	A library, or any facility that attracts homeless or panhandlers to take up residence, or scare and harass visitors.	6/11/2018 12:04 PM
46	No large scale drinking facilities. No rock bands or other type activities that are not full family activities.	6/10/2018 11:11 AM
47	Not sure	6/10/2018 10:19 AM
48	Parking lots taking over the area. I would like to see the historic building that was the old town pub revived into something the community could use.	6/9/2018 10:38 PM
49	?	6/9/2018 5:52 PM
50	Remove RV parked for months in front of the library, an eye sore health hazard and foot in the door!	6/8/2018 9:22 PM
51	Drug/ needle exchange or camping	6/8/2018 6:30 PM
52	Resturaunt, art gallery, museum, booksore, watersports rentals	6/8/2018 1:14 PM
53	Often, waterfronts are gated and locked for security of boat owners. I like that Silverdale's waterfront is more for community members, not just boat owners. Also, when we have company we have rented paddle boards and kayaks, giving our guests another viewpoint of our beautiful location. I don't like the LOUD motor sports activities! I much prefer the human powered quiet type.	6/8/2018 11:51 AM
54	For the port to take any more properties off the tax rolls without a master plan in place.	6/7/2018 4:56 PM
55	Housing	6/7/2018 4:19 PM
56	?	6/5/2018 8:45 PM
57	I don't want more powerboats in the water.	6/5/2018 5:08 PM
58	Event Center - Too many cars!	6/5/2018 5:07 PM
59	Unsure	6/5/2018 1:37 PM
60	Parking for Kayakers, kayakers cant use the parking for boat trailers.	6/5/2018 10:36 AM
61	Car or motorcycle shows. It should be about the water.	6/5/2018 8:49 AM

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62	Privatization	6/5/2018 8:11 AM
63	teen homeless shelter	6/5/2018 7:35 AM
64	Loud concerts! Special events is ok, but the peace and natural quiet is best.	6/5/2018 12:15 AM
65	not a lot of noise...more laid back atmosphere...farmers market is great down there....the area is being overtaken by small boat storage and if they could be consolated into 1 location that would be great!	6/4/2018 10:27 PM
66	no camping..don't want to encourage overnight problems.	6/4/2018 7:44 PM
67	government control	6/4/2018 7:17 PM
68	franchise restaurants	6/4/2018 6:44 PM
69	Churched	6/4/2018 4:03 PM
70	Please tear down those ugly apartment buildings near Silverdale Way. Eyesore.	6/4/2018 3:36 PM
71	less vehicles more walk ability	6/4/2018 1:12 PM
72	Not in favor of an event center	6/4/2018 12:37 PM
73	As I mentioned homeless and drug activity is my concern. So no to camping, no to showers, and no to anything that will turn the park into a dumping ground for drugs like the Bremerton waterfronts. As is all the homeless are flocking to Silverdale.	6/4/2018 12:25 PM
74	I think there are too many hair salons	6/4/2018 11:50 AM
75	Loud bands	6/4/2018 11:24 AM
76	camping! Foot Ferry, I dont think the area could accommodate the parking needs for that!	6/4/2018 9:20 AM
77	Unsure	6/4/2018 9:13 AM
78	Motorized sports, increased traffic speeds, high-value homes.	6/4/2018 8:55 AM
79	N/A	6/4/2018 8:55 AM
80	Smoking, Dog Park,	6/4/2018 8:40 AM
81	As stated before - spas and hair salons are aplenty and business offices should not be allowed on the waterfront. Waterfront space should be reserved for businesses serving the recreational community, gathering spaces, etc.	6/4/2018 8:22 AM
82	NOT: Condos close to the water. No chain restaurants.	6/4/2018 8:03 AM
83	Right now I can't think of any services that I would not like to have in our waterfront.	6/4/2018 7:14 AM
84	Please do not take the greenery away	6/3/2018 7:49 PM
85	camping	6/3/2018 7:22 PM
86	public concerts / loud outdoor events	6/3/2018 5:50 PM
87	No event center. Please stop high schoolers and others from parking in spots that are designated for boat trailers.	6/3/2018 5:25 PM
88	Do not want to see events center or other things that would increase the density of people.	6/3/2018 5:20 PM
89	No large commercial franchises, condos, or hotels.	6/3/2018 4:42 PM
90	fast food restaurants	6/3/2018 3:09 PM
91	skate park dog park under age concert venue	6/3/2018 3:03 PM
92	Like to see Non-motorized boat rental, more shops, pedestrian walkways. Would like to see the boat launch go away, or be moved somewhere else to allow for expansion of the park.	6/3/2018 1:48 PM
93	Don't get too commercial or flashy.	6/3/2018 1:41 PM
94	Marijuana shops, Tattoo shops,	6/3/2018 11:27 AM
95	Events or services that would increase use of noise producing motorized watercraft like jet skis	6/3/2018 7:53 AM

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96	Anything that blocks the waterfront view or is not conducive to a relaxed atmosphere in the waterfront area. Particularly don't want a ferry system with all the cars and mechanics it would involve.	6/3/2018 7:37 AM
97	Long term camping Vehicle storage or repair facilities Garbage transfer facility	6/2/2018 11:11 PM
98	I would not like to see housing development occur.	6/2/2018 10:41 PM
99	multi-night camping; anything that generates large scale traffic	6/2/2018 10:21 PM
100	Housing, shops	6/2/2018 8:39 PM
101	Carnival rides, balloon slides, etc.	6/2/2018 7:50 PM
102	Pay for parking	6/2/2018 7:13 PM
103	More housing added.	6/2/2018 6:00 PM
104	NO pot stores!	6/2/2018 5:40 PM
105	N/A	6/2/2018 5:33 PM
106	I think the less emphasis on motor powered watersports is better- particularly jet skis and water skiing. Fishing from powerboats isn't that disturbing.	6/2/2018 5:08 PM
107	I don't know	6/2/2018 4:14 PM
108	Marijuana bar	6/2/2018 4:06 PM
109	the sewage pump	6/2/2018 3:49 PM
110	n/a	6/2/2018 3:36 PM
111	Public tax Supported retail services.	6/2/2018 1:30 PM
112	Live bands	6/2/2018 12:16 PM
113	nothing that would cause too much noise after dusk...serene is key to Silverdales future	6/2/2018 12:15 PM
114	Livestock events.	6/2/2018 10:05 AM
115	Any chains or national brands, non retail or restaurant businesses (offices, salons, daycares, schools, etc) along the Waterfront rather than shops and restaurants; more apartment / condo buildings being built	6/2/2018 9:16 AM
116	Parking adjacent to waterfront park.	6/2/2018 9:14 AM
117	None	6/2/2018 7:54 AM
118	NO ATV or motor vehicles in rec. area but on perimeter but yet close enough to walk. NO structures that impede the view or do not look like fit the historic feel of the area or it's history.	6/2/2018 7:47 AM
119	Services which require more parking	6/2/2018 4:48 AM
120	Event centers for meetings, parties, or weddings.	6/1/2018 10:36 PM
121	Safe place to walk and picnic	6/1/2018 10:31 PM
122	Services that would reduce use by most people should be elsewhere, e.g. long-term marina for large boats.	6/1/2018 10:28 PM
123	Liquor, Smoke shop,	6/1/2018 10:17 PM
124	Camping	6/1/2018 10:12 PM
125	loud noisey watercraft	6/1/2018 10:07 PM
126	NOT: motorized boats	6/1/2018 10:02 PM
127	Parking garage Small hotels Chain stores Apartment buildings Tourist only buisnessed	6/1/2018 9:04 PM
128	Voluminous parking, theme development towards more modern design which does not represent the character and history of the community.	6/1/2018 8:24 PM
129	country music	6/1/2018 8:02 PM
130	I can not think of any at this time.	6/1/2018 6:47 PM
131	Events that stopped the public from accessing nearby things	6/1/2018 6:07 PM

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132	I would not like to see camping or laundry facilities offered.	6/1/2018 5:40 PM
133	The continuation of turning residential homes into businesses. I would like more of a blend of housing.	6/1/2018 4:56 PM
134	Loss of boardwalks, trees, and accessible shoreline	6/1/2018 4:36 PM
135	Large wedding or other large private events that impact public use of the space	6/1/2018 4:29 PM
136	Loud concerts for hundreds of people	6/1/2018 4:21 PM
137	Adult only events where alcohol and/or drug use was commonly taking place.	6/1/2018 4:13 PM
138	I would like to keep the park appropriate for children and families - things that are counter to this would include drinking festivals I suppose.	6/1/2018 4:04 PM
139	smoking and tatoo parlors	6/1/2018 3:53 PM
140	Campers	6/1/2018 3:14 PM
141	No high rise apartments, private condos, etc.	6/1/2018 3:00 PM
142	No hotels or apartments. Keep Old Towne looking "old ". Require new construction to have an aged appearance. Otherwise, Old Towne won't be old.	6/1/2018 2:55 PM
143	No homeless activities	6/1/2018 2:52 PM
144	I think an event center would overtake the waterfront family use and be too loud, it's so enjoyable now, I am concerned about loud drunk never ending events ruining the atmosphere we now enjoy.	6/1/2018 2:48 PM
145	An event center. Anything loud. Anything that does not enhance or support the use of the WATER component of the waterfront: water craft, view, beach walking, fishing, marine wildlife, etc.	6/1/2018 2:36 PM
146	Keep out major chain businesses or what would delute local character	6/1/2018 2:26 PM
147	non recreation	6/1/2018 2:26 PM
148	Camping	6/1/2018 1:39 PM
149	Free use or don't bother	6/1/2018 1:30 PM
150	Camping...not enough space or supervision to support that. Would promote drinking, partying, trash. Would also jeopardize the security of boats stored at the waterfront.	6/1/2018 1:27 PM
151	Private facilities that exclude waterfront public access	6/1/2018 12:57 PM
152	I would like to see silverdale encourage the growth of the sailing community (and other aquatic sports) by building a center. We could have the same vibrant community as other coastal towns do just by investing in it. After all, Silverdale is on the water why not encourage an aquatic-centric lifestyle.	6/1/2018 12:55 PM
153	Overnight camping will turn into homeless camping	6/1/2018 12:52 PM
154	Any services that have an overall negative impact on the health, safety and well-being of Silverdale residents or Dyes Inlet.	6/1/2018 12:26 PM
155	Housing with no parking, Tall structures blocking view.	6/1/2018 12:26 PM
156	None come to mind.	6/1/2018 12:22 PM
157	No large (tall) buildings or advertising signs.	6/1/2018 12:16 PM
158	Would like to see a hop on hop off shuttle that would run between the mall and Old Town that would have a route that could connect to places like the Clear Creek Trail, the Old Mill Park etc.	6/1/2018 12:11 PM
159	No camping, no events center here, nothing that pollutes the air or bay ie moorage offered to gas run boats.when I read your list of ideas for "improvement " I see revenue driven ideas as your focus. What a real shame! Silverdale has an opportunity to offer the rare natural experience here, rather than the revenue driven development of other cities' waterfront areas. Neither the county nor the Port should do anything except protect it as part of the Silverdale trail..	6/1/2018 12:10 PM
160	Inline/Roller Skating park	6/1/2018 11:49 AM
161	n/a	6/1/2018 11:48 AM
162	Kayak is a very limited market and not a good use of limited space.	6/1/2018 11:30 AM

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163	No expansion of motorized boat services	6/1/2018 11:14 AM
164	Anything political or racial discrimination	6/1/2018 11:09 AM
165	Weddings or large private events that tie up the entire park to other visitors.	6/1/2018 11:01 AM
166	Access fro reservations of Park Services for both private and public occasions spring, summer and fall!	6/1/2018 11:00 AM
167	N/a	6/1/2018 10:59 AM
168	More parking. Buildings crowding the waterfront.	6/1/2018 10:56 AM
169	Expansion of sailing programs, windsurfing and kayaking programs. The port property is too small and inappropriate for housing. A leased "private waterfront restaurant" on port property under consideration is not needed when there is limited space for actual water related activities. The limited space should be used for port programs and community related events that educate or bring people in.	6/1/2018 10:47 AM
170	none	6/1/2018 10:45 AM
171	Loud concerts	6/1/2018 10:34 AM
172	No commercial services should be added - though the Farmer's Market has worked well. Services that require any significant physical additions should be avoided.	6/1/2018 10:26 AM
173	No parking garage!	6/1/2018 10:24 AM
174	Less teenagers smoking! Can we have it a clean air area?	6/1/2018 10:23 AM
175	Baseball	6/1/2018 10:20 AM
176	No motor boats or jet skis. Expand the park area along the waterfront if possible. Move parking away from the waterfront (unlike Poulsbo where it consumes some of the best area that should be converted into park).	6/1/2018 10:19 AM
177	loudness	6/1/2018 10:17 AM
178	I would like to see the end of Whaling Days. I wold like to see things that represent the ever expanding diversity that Kitsap county is beginning to foster. Food fairs, car shows, child and adult fairs.	6/1/2018 10:06 AM
179	Anything that would harm the environment greatly	6/1/2018 10:04 AM
180	off-leash dog walking	6/1/2018 10:04 AM
181	Stay focused on local, small town, community focused entertainment and leisure activities. You don't need lots of money to turn this around. You need to zone better for the types of businesses allowed in the area and you need to create a "theme" for the building facades to make it look festive. These small changes will morph this area into our community hub.	6/1/2018 10:00 AM
182	affordable housing	6/1/2018 9:59 AM
183	Valuable public space wasted on parking or smelly sewage pumping.	6/1/2018 9:58 AM
184	Bands - the sound wafts up the hill to residential areas to the west. quite annoying.	6/1/2018 9:58 AM
185	A large facility that would take up available parking for other Port visitors. There is not enough parking as is.	6/1/2018 9:48 AM
186	None	6/1/2018 9:43 AM
187	I don't want to see fee's being place on the park to use and if there is an event center don't make the fee's so outrages that a person who has a middle level income under can not afford it.	6/1/2018 9:42 AM
188	Anytime of camping support that would encourage homeless encampments. Keep it as natural and minimalistic as possible.	6/1/2018 9:42 AM
189	Event center	6/1/2018 9:40 AM
190	Not sure	6/1/2018 9:37 AM
191	none,	6/1/2018 9:34 AM
192	beer gardens. please keep this a family friendly space	6/1/2018 9:33 AM
193	weddings	6/1/2018 9:30 AM

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194

xxxx

5/24/2018 4:12 PM

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Q17 What else would you like us to know?

Answered: 164 Skipped: 371

#	RESPONSES	DATE
1	The sailing program is awesome, and should be provided with more space to grow. It has great potential for benefiting the community. Also, the Old Town Silverdale pub should be torn down. It had made me feel personally unsafe and unwelcome at the waterfront, and is an eyesore.	7/1/2018 6:39 AM
2	Upgrades should be focused on the idea of "How can we give Old Town an identity?". Cleanliness is crucial and is the most basic, but important "upgrade" that needs doing. That means pressure wash, resurface and repaint.	7/1/2018 12:41 AM
3	Port Property should be available for all to use. Renting port property to businesses doesn't benefit public use.	6/30/2018 11:29 PM
4	Don't invite homeless campers and drug users.	6/30/2018 8:44 AM
5	Thank you for all the support in all the events I have been apart of in the last 10 years !! The pier is the keystone to the port and I love going and sitting out there to enjoy the views of dyes inlet and relax. Launching my sailboat from the boat launch , and meeting up with friends and just hanging out by the pavilion.	6/30/2018 6:19 AM
6	I think it is very important to increase access to and usr if the water, particularly for non-motorized use.	6/29/2018 6:12 PM
7	Parking is currently abysmal. PWT to visit Cash Brewing with short term (hourly) gear storage would be ideal!	6/29/2018 2:01 PM
8	Security is vital. Regular patrols of the parking lot and Byron Street is essential to keep people safe and prevent illegal activities and racing up and down the street.	6/29/2018 8:33 AM
9	This is a real opportunity for the Port to leave a legacy. Master plans have a way of being all plan and no action. Please take action and create a beautiful functional Old Town that can be known for integrating our natural environment with our citizens.	6/28/2018 5:54 PM
10	There needs to be more shops and activities to draw people into old town Silverdale. Something similar to Old town Poulsbo.	6/27/2018 10:09 AM
11	The idea of a Kitsap foot ferry is ok, depending on how big it is.	6/25/2018 8:37 AM
12	I'd like to see condos built in a way that wouldn't obstruct views or interfere with the recreational activities of the community as a whole.	6/24/2018 7:20 AM
13	I would like to see the beach cleaned up so I feel comfortable with my family exploring there. It seems very polluted and full of debris. The boardwalk is one trip hazard after another. We enjoy the other Kitsap urban trails, like Fish park and Clear Creek trail, and would love to have a way to enjoy the Old Town Waterfront in a similar way. We have been frequenting the waterfront park since the playground update. A larger play area would be an even larger attraction.	6/21/2018 11:35 PM
14	The Old Town area is perfect for a cultural center. The CK School District property... the old library... Linder Field... all could be utilized for something wonderful instead of... taking up space and falling apart. The Silverdale Waterfront could be the envy of the Northwest.	6/21/2018 8:01 PM
15	N/A	6/21/2018 6:00 PM
16	I would love to see a classy restaurant on the waterfront on the order of the Boat Shed in Manette, or Anthony's. I would also like to see the park turned into an area for adults as well as children. The play area is good for kids, but it would be nice to have a beautiful, serene park for grown ups too, with trees and flowers and nice walking paths. I personally never go there because it's kind of trashy and noisy and full of drunks during the festivals held there in the summer. It's really a pretty spot, but certainly is not being used to its full potential. Look at Bellingham for ideas.	6/21/2018 5:20 PM
17	Keep it safe from crime, designate authorized users to discourage crooks and homeless people.	6/21/2018 5:18 PM
18	The Port needs to tear down the Old Town Tavern building, the cost to keep it is a waste of taxpayer dollars. Same with some of the other old building they own in the area.	6/21/2018 5:00 PM

Old Town Silverdale Waterfront Public Survey

19	Silverdale native	6/20/2018 3:34 PM
20	The motorized boaters need to be more aware of the no wake zone and looking out for others in the water near the launch and tie up areas.	6/20/2018 1:13 PM
21	Rebuild and modernize that pump station, and, have an architect design the building that houses it, this time, so that it compliments the waterfront!	6/18/2018 1:30 PM
22	Some of the questions aren't necessarily going to give you the real answer you are looking to find. For you instance, you asked how often I use the area but none of your answers fit. I go once a month in the winter but a lot more in the spring and summer	6/18/2018 11:23 AM
23	More events rather than just whaling days.	6/17/2018 10:25 PM
24	Old Town is a diamond in the ruff. Old Town should be pedestrian friendly. Out door coffee shops and restaurants.a neighborhood grocery.	6/17/2018 1:42 PM
25	We value having a community space for our kids that is clean and safe.	6/17/2018 1:41 PM
26	I'm glad you're planning improvements	6/17/2018 11:51 AM
27	Don't put another condo building on our precious waterfront. Leave it for the community. With the way housing costs are now, our long-time residents are already stressed about over-development and being priced out.	6/15/2018 6:49 PM
28	Waterfront park is the BEST part of Old Town Silverdale.	6/14/2018 10:22 PM
29	Basically, I like it the way it is. Expand the pump station and leave it as is. There are more important things to spend tax money on.	6/14/2018 12:21 PM
30	?	6/13/2018 11:08 PM
31	Need another dog park	6/13/2018 9:59 PM
32	The current bulkhead at the park is going to be needing repairs or replacement. Please consider alternative design options that could provide habitat improvements while still maintaining or integrating the limited upland recreational opportunities (City of Seattle Seawall, as one example)	6/13/2018 8:53 PM
33	This amazing area is a staple for my family. The park, waterfront, boatlaunch, and kayak/paddle board offerings are so special to us. Please keep this area a community gathering spot and work to restore its environmental security.	6/13/2018 8:11 PM
34	As a resident I would like to see more quiet activities available and not catering to motorcycles and the kids that drift in the park after hours	6/13/2018 8:04 PM
35	The grassy area behind sewer building is so sludgy and stinky. That's a big area that could be beautiful for picnics on blankets. Impossible now prett much. A connection to Olympic Park? The one across from Staples- would be winderful for a long walk. It's almost there already and crossing the road at the light, connects to Clear Creek Trail.	6/13/2018 8:02 PM
36	Nothing I can think of.	6/13/2018 1:15 PM
37	Please build a shell house to help us get more kids rowing!	6/11/2018 7:22 PM
38	For the sewer area. If it could be as small as possible, or turn it into an art work or play area on the outside. Creative thinking here to fit in with the water and family use.	6/10/2018 11:11 AM
39	This area has so much potential to be a beautiful part of our community. Utilize those areas with beautiful Mt. Ranier views and water views. Make more sitting areas in the empty lot next to the boat launch. Make some 30 minute parking for people who want to go eat lunch in their car while looking at the water. Get rid of the mold infested subsidized apartments that are there. Have a code of what color the buildings can be painted. (Jeanne Rene Salon-yikes)	6/10/2018 10:19 AM
40	Please dont build more damn parking lots. Last thing that beautiful area needs is more blacktop	6/9/2018 10:38 PM
41	?	6/9/2018 5:52 PM
42	Please work at getting the RV in front of the library moved, it may be the first of many that will blight the area in the coming months!	6/8/2018 9:22 PM
43	It would be nice to have an actual lookout spot, like by the boat ramp, but made for it	6/8/2018 6:30 PM
44	More pressure on county commissioners to appose high density housing, and more focus on environmentally friendly liesure activities.and local mom and pop business.	6/8/2018 1:14 PM

Old Town Silverdale Waterfront Public Survey

45	See above. I would love to see the Clear Creek Trail further linked to the waterfront park and even an expansion of the waterfront park if it was feasible. The waterfront is a treasure!	6/8/2018 11:51 AM
46	Consider an expansive parking garage (with lower level) to accommodate future increased traffic to this new and improved area. I think it will be important to preserve the beautiful, quiet opportunities to enjoy nature and scenery while increasing "services/facilities." Perhaps develop those sites/functions with some degree of separation.	6/7/2018 8:19 PM
47	Please don't commercialize the area.	6/7/2018 5:24 PM
48	Part of the port's mission is economic development. And as such, they should be actively promoting activities that promote tourism. The cheapest and easiest way to bring outside money into the area.	6/7/2018 4:56 PM
49	My friends, family and I love the view and to walk along the water. Often we will walk to the edge of the dock. More walking trails along the water would be nice with bathrooms open year round instead of just the summer would be nice. Also, a coffee stand or a small cafe near or on the property would do well.	6/6/2018 10:27 PM
50	?	6/5/2018 8:45 PM
51	Move auto turnaround away from street end and dumpsters. Convert end of public R-O-W roadbed to open space providing a pier-side plaza/kayak launch area. Transition waterfront park setting west into Port lawn area, extending Clear Creek Trail across wetlands. Relocate pump station! If possible house in ground floor of new 2-story restaurant building overlooking expanded Great Lawn & waterfront. Downstairs area to be used for special events. Party tents set up on lawn for special events. (Rental fee paid to Port.) Redevelop Byron Street businesses with new commercial businesses on ground floor, residential above. This could be part of the restaurant building.	6/5/2018 5:07 PM
52	Move the pump station	6/5/2018 11:50 AM
53	More police coverage. Port people be nice.	6/5/2018 10:36 AM
54	Silverdale has lost its "heart" and this could become a fine and historically significant site for the community.	6/5/2018 8:49 AM
55	A 4th of July event would be nice. A skate park, concerts, block parties, food carts...	6/5/2018 8:11 AM
56	The entire area is a diaster! During the day it is a place for teens to buy drugs and smoke pot in their cars and at night it is filled with drunks. Where is the police in this area?	6/5/2018 7:35 AM
57	What options are available to the disabled to view Dyes Inlet/Mt. Ranier year-round after dinner without having to park and walk 5 minutes? My wife has difficulty walking and every time I stop to view Dyes Inlet, we get roused by people telling us the facility I pay taxes on will charge me with trespassing?! Where CAN I go to view the Inlet at night on this property? Call me!	6/4/2018 11:05 PM
58	don't push out existing businesses down there	6/4/2018 10:27 PM
59	stop people from drag racing. more visibility of how the port money is spent and more input	6/4/2018 8:46 PM
60	Fresh water showers...even outside...just to rinse off the salt water from you and your clothes would be nice!	6/4/2018 7:44 PM
61	I really don't see how it could be improved. We have sidewalks, underground util., open space to recreate. Boat ramp, dock, childrens play area, covered space .	6/4/2018 7:17 PM
62	Hello from Chico!	6/4/2018 3:36 PM
63	more of a community feel, saftety for kids to run around	6/4/2018 1:12 PM
64	Would like to see a work/live area which would foster a real sense of community.	6/4/2018 12:37 PM
65	just want a quiet park with water access.	6/4/2018 11:24 AM
66	if you have an event center, you should offer a discounted rate for old town business use!	6/4/2018 9:20 AM
67	Old Towne Silverdale is so important to me because it is an oasis of Kitsap culture and feel with slow traffic and beautiful trees. I have thought about opening a small business here to be a part of this community. PLEASE PLEASE PLEASE do not turn this into a Bellevue type community with expensive waterfront homes. PLEASE retain the culture, pace, residents, and businesses that make this Old Towne Silverdale a unique destination for residents and visitors. Silverdale has enough strip malls.	6/4/2018 8:55 AM
68	This is your chance...plan correctly.	6/4/2018 8:22 AM

Old Town Silverdale Waterfront Public Survey

69	Keep it a destination place of environmental beauty . Make use of empty bulidings close by and provide shuttles if necessary to waterfront. Make it economically feasible for all	6/4/2018 8:03 AM
70	For years, I have felt that our lovely Dyes Inlet, which I am looking at from my home right now, was way underused. Then came the sailing club! Followed by the outriggers and then rowing. I am sorry that you didn't accept John Lind's proposal for the Clam Island Rowing Club own area. Maybe soon, this will happen. I would also like to see a dock year round for those of us that wheel our kayaks down the boat ramp to begin a quiet and relaxing paddle around the inlet.	6/4/2018 7:14 AM
71	I would love to see swings at the play ground and a splash park for kids- Similar to a point Ruston feel in Tacoma. It's a beautiful spot but has certainly not reached its full potential.	6/3/2018 10:41 PM
72	Improving with out obstructing views and natural surroundings	6/3/2018 7:49 PM
73	updated regularly on progress of space.	6/3/2018 7:22 PM
74	Use Poulsbo as your model, not Port Orchard or Kingston. Inviting watercraft visitors and less vehicular traffic for the amount of visitors. Claim an identity other than "West Lynnwood", which is what Silverdale is turning into.	6/3/2018 5:50 PM
75	I don't feel safe at the waterfront after dark due to the people who park cars and party at the pier in the evening and night. I would like to see the waterfront be a more vibrant area with more shops and restaurants like downtown Poulsbo rather than an area that dies after dark and attracts people who appear to be doing illegal things.	6/3/2018 5:25 PM
76	Port is a nice relaxing fun family place. The last thing it needs is more congestion.	6/3/2018 5:20 PM
77	I love the waterfront. Please continue public access.	6/3/2018 4:42 PM
78	I would love a walkable downtown area with restaurants, bars, and a vibrant nightlife for personally. Old Town Silverdale should be the primary destination for all of Kitasp. And, If we had a foot ferry to Bremerton, then we could attract home buyers from the overwhelmed Seattle market.	6/3/2018 4:35 PM
79	I am part of the Central Kitsap Sailing Team and it would be really great to have upgraded facilities for our boat storage and maybe a classroom area that's dry would be helpful	6/3/2018 1:49 PM
80	Silverdale Autoworks is an eyesore even after the remodel, broken down cars will likely remain visible detracting from the appeal of visiting Old Town.	6/3/2018 1:48 PM
81	Keep the historical feeling and make it an inviting place. They took out most of the park playground features, and they need to add more to attract families.	6/3/2018 1:41 PM
82	It should be a family oriented, community gathering place available year round. With historical significance to Silverdale. We should look to other cities in Washington that have revived their historical centers.	6/3/2018 11:27 AM
83	I would love to see more small businesses and shops in the old Silverdale area, and better advertising for what is already there. I feel like Silverdale really lacks that cute, walkable downtown that most other towns in Kitsap have!	6/3/2018 8:51 AM
84	Thank you!	6/3/2018 7:54 AM
85	The county should help get the Old Town Pub redeveloped into a nicer restaurant or mixed use facility.	6/3/2018 7:53 AM
86	Please put restoration of historic buildings as a top priority for the Port and the County. Preserve the history of the small area that became Silverdale. Put effort into making the park a more beautiful place to be. Let the businesses be on the outskirts of Old Silverdale--cafe's in the center, within walking distance for boaters. Take a leaf from Poulsbo's book--keep the Old Town theme, as Poulsbo has done with their Nordic theme.	6/3/2018 7:37 AM
87	There should be more public information/public outreach. Is the Kitsap Sun publishing anything about these planning activities?	6/2/2018 11:11 PM
88	Sailing and is very important to and I would highly like for it to expand.	6/2/2018 10:41 PM
89	Places to walk, visit as a family	6/2/2018 8:39 PM
90	I love the view, the quietness and the water access and pier. I think a theme like poulsbo would be a draw and flower baskets would be a nice touch.	6/2/2018 7:50 PM
91	Nothing else comes to mind.	6/2/2018 7:13 PM

Old Town Silverdale Waterfront Public Survey

92	I would appreciate more non motorised boat storage and launching locations for them as a rower with Clam Island Rowing who rows 3 days a week April to December.	6/2/2018 6:00 PM
93	Silverdale could have an historic district & feel similar to Poulsbo and could become a destination if done correctly to attract the right kinds of businesses. If not, the big ugly box stores will prevail and Silverdale will be nothing special to anyone; a has-been piece of nothing.	6/2/2018 5:40 PM
94	I think bringing the fast ferry into silverdale would really help invigorate the area. Don't know if this is even possible, but if it is it would be really nice.	6/2/2018 5:08 PM
95	Add an Aquaponics fish hatchery to increase game fishing and awareness of urban farming.	6/2/2018 4:14 PM
96	n/a	6/2/2018 3:36 PM
97	The waterfront park is supposed to be a public access facility for everyone. Not just folks who can afford a \$15K plus ski boat, Yacht, etc... The facility should be aimed at promoting Low income families the opportunity to engage in salt water activities. A portion of the park should be devoted to getting kids to play in the sat water!! either in the water or on the water.	6/2/2018 1:30 PM
98	Historical signage important!	6/2/2018 10:05 AM
99	There's enough wind power in Old Town to be able to generate plenty of power and give back to the grid. Environmentally sustainable buildings and other construction are really important to this area. While I would love to see more parking, I would not want it to take away from the Waterfront possibilities for businesses. It should not be time limits for parking anywhere in Old Town, if we want to make it a destination people must be able to park for the day and move around the area on foot.	6/2/2018 9:16 AM
100	Communicate interest in acquisition of CKSD Jennie Wright building and KRL Silverdale Branch building. This space could be used as a parking garage for easy access to Waterfront Park. The pump station should be moved.	6/2/2018 9:14 AM
101	Nothing	6/2/2018 7:54 AM
102	NO tall buldings that would obscure the view of Dyes Inlet etc And, NO buildilng taht look new age - but keep withing the hsitoric theme NO fees to use the park Need tihngs to draw more boat docking - eg.closer grocery (even grocery for park visitors good), and gas for boats. More small business' like coffee shops, grocery, ice cream shop etc. A Visitors Center - not only for Old Town and Silverdale but "gateway" to Olympic Peninsula, Poulsbo, etc.	6/2/2018 7:47 AM
103	Old towne Silverdale is outdated and needs a complete overhaul. It is a prime location to have mixed housing options which support diverse shopping options, while preserving waterfront access and enjoyment of recrrational activities.	6/2/2018 4:48 AM
104	It would be nice to see the space utilized for non-motorized watercraft. It would be nice if the pump station was removed completely.	6/1/2018 10:36 PM
105	Safety is my main concern	6/1/2018 10:31 PM
106	While facilities for all ages are important, we need to keep in mind that we have an aging population and make sure that the facilities are convenient for those with disabilities. Example: provisions for wheelchairs, plenty of benches to sit on, shaded areas, etc.	6/1/2018 10:28 PM
107	The park is nice but I feel the Port took away the History when the Old Coop and Hotel were removed.	6/1/2018 10:17 PM
108	make Oldtown Silverdale a destination place	6/1/2018 10:07 PM
109	Value volunteer and youth programs higher than economic development. Providing healthy lifestyle choices for young people increases the quality of life for all in our community. Plan for buildings and design which will inspire and engage our community to appreciate our unique place in Puget Sound. People only care about the environment they use. They can't experience Dyes Inlet if there are not programs to give them access to the water. The Port has a mission to provide access to everyone.	6/1/2018 9:04 PM
110	The port has done well with their current structure and growth. But consideration for development of land based on community and community events. Small business and services to provide an attraction that brings people and offers opportunity to keep them in the area. Whaling Days is a long standing tradition. Farmers Markets are popular. And open air attractions and shopping areas would serve as a positive attraction to the area. Sustaining the character and history of the community is key.	6/1/2018 8:24 PM

Old Town Silverdale Waterfront Public Survey

111	I am big on history. I would really like to see a historical depiction take some sort of form in the Old Town area. I would hope that any construction regarding the Port to take place would be considerate of the business community trying to survive the construction on Silverdale Way. I think there has been absolutely no regard for the long-time taxpaying businesses trying to stay alive in all the ridiculous construction which appears to me to be a waste of money that could have been spent for a more worthy project.	6/1/2018 6:47 PM
112	Needs to be very spaces efficient, and offer rainy season opportunities. Parking is a huge problem if events happen because it's already limited.	6/1/2018 6:07 PM
113	Silverdale needs a community gathering type of place. It needs to be a place where you can walk for exercise, walk to local business/food, have community events, rent and store kayaks and paddle boards, outdoor eating venues.	6/1/2018 6:00 PM
114	We NEED more playground space that include swings!! Everyone from seniors to infants enjoy the swings.	6/1/2018 5:40 PM
115	Its exciting & long overdue	6/1/2018 4:56 PM
116	Visible security/law enforcement to keep the area clean and free of vandalism substance abuse and individuals using area as protection.	6/1/2018 4:36 PM
117	Can't think of much elae	6/1/2018 4:29 PM
118	We have great beauty here that is not being brought to center stage. Let's enhance that.	6/1/2018 4:21 PM
119	Go visit Erickson Play Field in PA as an example of something new to implement downtown. E 4th St, Port Angeles, WA 98362 https://goo.gl/maps/V3FKyhckGsv	6/1/2018 4:13 PM
120	The waterfront area is the only chance for a focal point in Silverdale - please consider this as an opportunity to shape the community that exists and will grow here. I strongly believe this is and should be family focused. Open spaces and non-motorsport water activities would help foster this feeling.	6/1/2018 4:04 PM
121	the old tavern that was closed down but rented for \$400 per month - I think the organization they created to accommodate their people could be studied for a solution to homelessness and low income	6/1/2018 3:53 PM
122	Foot ferry expansion would be a fantastic addition. Parking needs an increase.	6/1/2018 3:24 PM
123	Our entire family enjoys the port. We enjoy the park, beach and businesses. We have picnics and hold birthday parties at the park. We are patrons of the restaurants and businesses. We go to the library. We use the marina in the warm months for water sports and activities. We let our kids fish off the pier. Please consider building an ADA accessible park like evergreen in bremernton.	6/1/2018 3:00 PM
124	Transportation by water is essential to a community that is surrounded by water.	6/1/2018 2:52 PM
125	I think your parks are lovely, you all are doing a great job. But Silverdale residents such as myself are enduring a lot of growth projects right now. Let's not overdue it, let the dust settle a bit. Perhaps after the new library is an event center will not be as needed as it will have space. Or perhaps it would be better at other park at that point.	6/1/2018 2:48 PM
126	My view is that the Port's job is to support and enhance the natural (plant and animal life) and human activities that relate to the water, and to help foster a vibrant, diverse, affordable neighborhood. This provides the area with character and lets it appeal to a diversity of people. The area should *not* depend on a mega-project or single-use commodity or facility. Above, I forgot to mention that it would be nice to see a little more historic preservation to balance out the new construction.	6/1/2018 2:36 PM
127	We love our waterfront lets make it a success and a refuge. Walkable, fun, festive, and a place people desire to be at. NOT SMELLY	6/1/2018 2:26 PM
128	Annex Tracyton before Bremerton, please ;)	6/1/2018 1:30 PM
129	More floating dock space for non-motorized boats would be helpful.	6/1/2018 1:27 PM
130	Hidden gem of Silverdale. Makes our "city" people friendly. Need viewing spots for people with handicaps that cannot get out of cars it want to stop for a moment to enjoy water	6/1/2018 12:57 PM
131	I now live in Olympia but often come up the coast to visit. Olympia is a vibrant aquatic-centric coastal town. I can name at least 8 other small coastal communities who have taken the plunge and leveraged their access to the sound to grow their aquatic sports community, culture and economy-Silverdale can too.	6/1/2018 12:55 PM

Old Town Silverdale Waterfront Public Survey

132	Control the number of people sleeping in the park during the day/night. Control the smell, it's not low tide all the time	6/1/2018 12:52 PM
133	Make old town Silverdale charming again. Look at how other small towns have revitalized their city center.	6/1/2018 12:26 PM
134	Restore the historical buildings and use them as a tourist attraction. Return the log house from the fairgrounds to the waterfront.	6/1/2018 12:20 PM
135	I'm a 43 year resident of Kitsap County and frequent the waterfront for kayaking and dining. I will most likely do so regardless of design or improvements. If Washington Avenue remains to the dock, I would love to see a turnaround for drivers to make parking easier. This could be a simple loop like Seaside, Oregon.	6/1/2018 12:16 PM
136	Expansion needs to consider safety and enforcement patrolling so that will be an added and continuing cost to the community, as well as sanitation, maintenance and cleaning.	6/1/2018 12:16 PM
137	Here's something we all know! Money rules! It is time we change that priority to " Nature rules in Silverdale"!!! That's what would make Silverdale special!!!	6/1/2018 12:10 PM
138	n/a	6/1/2018 11:48 AM
139	I like that there is a cohesive plan going forward to plan and grow Old Town that includes the community!	6/1/2018 11:31 AM
140	Access to WSF ferry service is critical to the future growth of both Old Town and Silverdale as a whole.	6/1/2018 11:30 AM
141	Waterfront is not currently a friendly area to visit. It's just an afterthought. Poulsbo has done a much better job of making their waterfront a vibrant area.	6/1/2018 11:14 AM
142	I object to the work sign that suggests that finding an alternative method of going to Silverdale. I live on Chico Way (about 9/10's of a mile off Newberry Hill. Going around does not make sense to me. I had to go around when the roundabout was put in, because Chico Way was closed, which I found very annoying, especially when it was delayed because the lighting fixtures were delayed.	6/1/2018 11:09 AM
143	Old town seems to be a favored hangout for lost souls who wander the library parking lot, walk down the middle of the streets of Old Town. Perhaps Kitsap Sheriff could be more visible in Old Town and try to connect these citizens with help.	6/1/2018 11:01 AM
144	Keep us informed on the progress of this project!	6/1/2018 11:00 AM
145	N/a	6/1/2018 10:59 AM
146	The County and Port should work with C-K School District to detergent future plans and use of the old Jenny Wright School and property as well as with the County Library site/building once a new location is determined for the new library. The new library should be built or located on this site and area for new parking could help provide parking to service old town.	6/1/2018 10:47 AM
147	Central Kitsap School district has about 11,000 students enrolled, plus there are many private schools with kids all living within a short distance of the Port of Silverdale. The Port can be an amazing education hub to provide water safety training, lessons and opportunities for life long skills to kids who may never have been out on the water in a boat or non-motorized craft. Programs can be expanded for students all the way through college in partnership with Olympic College as well.	6/1/2018 10:47 AM
148	It's practically perfect the way it is. Leave it alone!	6/1/2018 10:26 AM
149	We have a chance to make Silverdale a place people want to visit, live and spend time at. The waterfront is naturally beautiful, we just need to put in some work (fixing the stink, improving parking, etc) to use it to it's full potential.	6/1/2018 10:23 AM
150	Restricted access into and out of silverdale on silverdale way from Bryon to the Hwy will impede everything.	6/1/2018 10:20 AM
151	This has nothing to do with your project, but I have an issue with the landscaping maintenance around Silverdale. They over-prune everything into little boxes and balls. It looks terrible and it bad for the plants!! You would save money if you let the landscaping take a more natural form and it would enhance the beauty of Silverdale.	6/1/2018 10:19 AM
152	you're great people and we like you	6/1/2018 10:17 AM

Old Town Silverdale Waterfront Public Survey

153	Kitsap has miles of shoreline. Unfortunately, the shoreline is often privately owned and it is difficult to have long stretches of it available to the public. For example, in Hansville the shoreline area between the Point No Point WDFW Water Access Site and Point No Point County Park is privately owned, which forces the public to walk on a narrow, high traffic street to get to the lighthouse, despite the shoreline being a direct route. I would love to see shoreline access from the Silverdale waterfront that extends to Anna Smith Park, all with a connection to the Clear Creek trail. Eventually, an expansion of boardwalk and interpretive trails along the shoreline would be a wonderful addition. It would be wonderful if a trail for walkers, runners, and bicyclists could expand all the way to Sheridan Park in Bremerton. Examples of some nice urban waterfront trails are in Yakima and Wenatchee.	6/1/2018 10:16 AM
154	as I was doing this survey, I was only thinking of Old Town Silverdale for the Port - are there other areas? Old Mill Park to take into consideration or other areas? More open, flat space to run and play.	6/1/2018 10:16 AM
155	I think that Farmland has reached its point of closure. I know it has been there forever, but it is dirty, they mistreat the animals they sell there and the prices are ridiculous. Keep the building and transform it into something nicer and cleaner and more environmentally friendly.	6/1/2018 10:04 AM
156	How can I participate ongoing to help with this project? I live 2 blocks from Camp Union, so Silverdale is my town too. It is where I shop and dine, etc.	6/1/2018 10:00 AM
157	Old Town needs more parking if there is to be a unified "downtown" of businesses such as with Front Street in Poulsbo.	6/1/2018 9:52 AM
158	We appreciate the Port for letting us hold the Central Kitsap Farmers Market in Old Town.	6/1/2018 9:48 AM
159	Ideally wider sidewalks or boardwalk going all the way around to hop jacks. Turnaround parking by play area is needed, very poorly designed. Old town pub needs to be demolished and more restaurants and nightlife should be encouraged	6/1/2018 9:43 AM
160	Revitalize Old Town with updated properties, grocery, retail, housing and lots of parking. This part of town would be looked as urban living. Old town has a lot of opportunity to be a quant desirable area in Silverdale. The last thing we need is a event center that is wasted space not being used often. Not to mention the traffic mess we will have and increased crime.	6/1/2018 9:40 AM
161	Parking appears to be very low amount due to the number of slots reserved for boat trailers. Also, while there is parking across from Cash Brewing that is open? to Park visitors, this is not clear and it is kind of scary to park there because all the signs state PORT VISITOR ONLY -- and who really knows what that means?	6/1/2018 9:38 AM
162	Enjoy having the farmers market.	6/1/2018 9:37 AM
163	We love the Port and Old Town Silverdale and would love to see it become a destination like Poulsbo, Kingston, Port Orchard, LaConnor and many of the other small towns with wonderful waterfront options (stores, restaurants...ect)	6/1/2018 9:35 AM
164	yyyy	5/24/2018 4:12 PM



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

APPENDIX F: OLD TOWN PUB STRUCTURAL ANALYSIS

September 10, 2018
File No. 262018.077

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275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

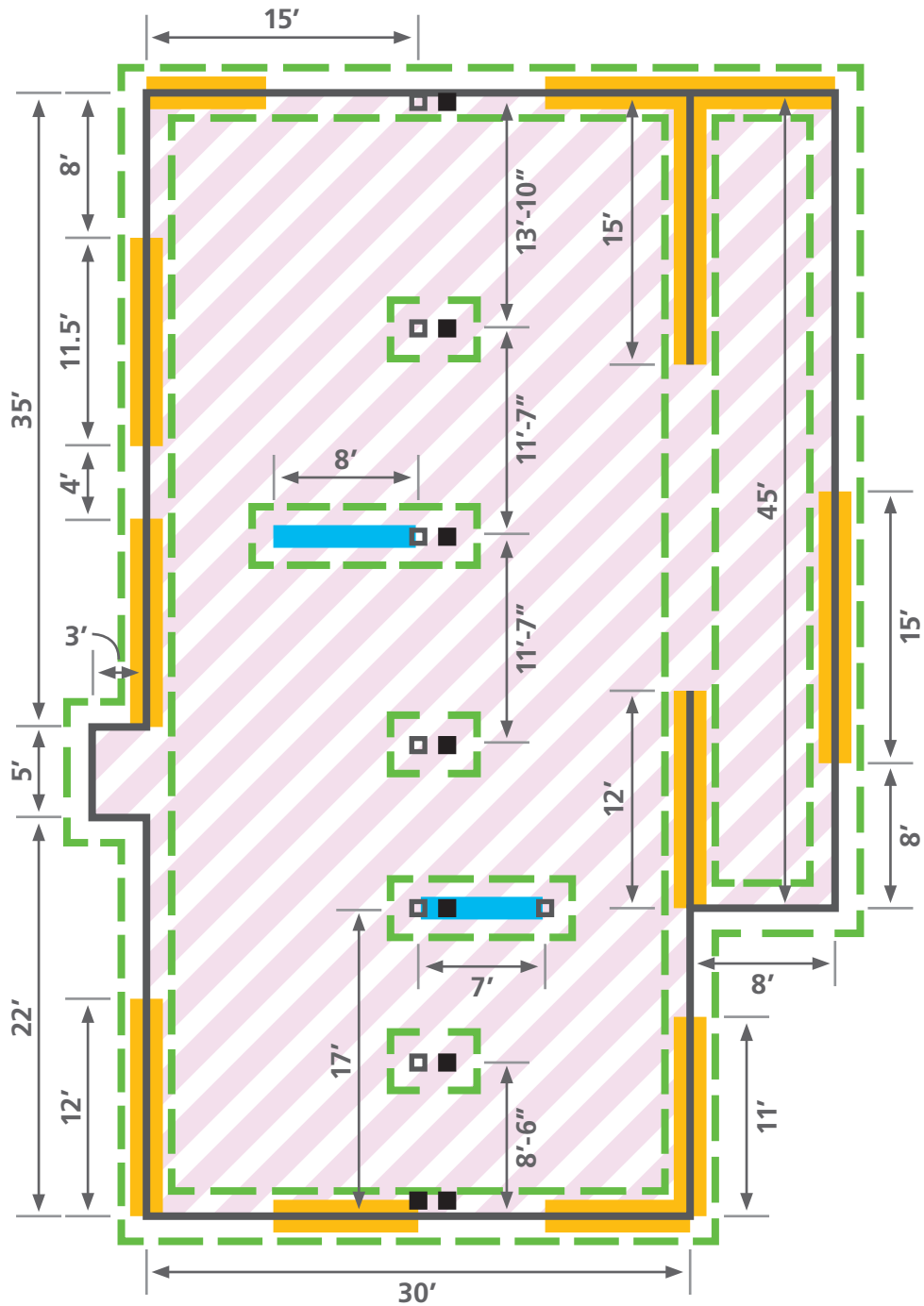


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

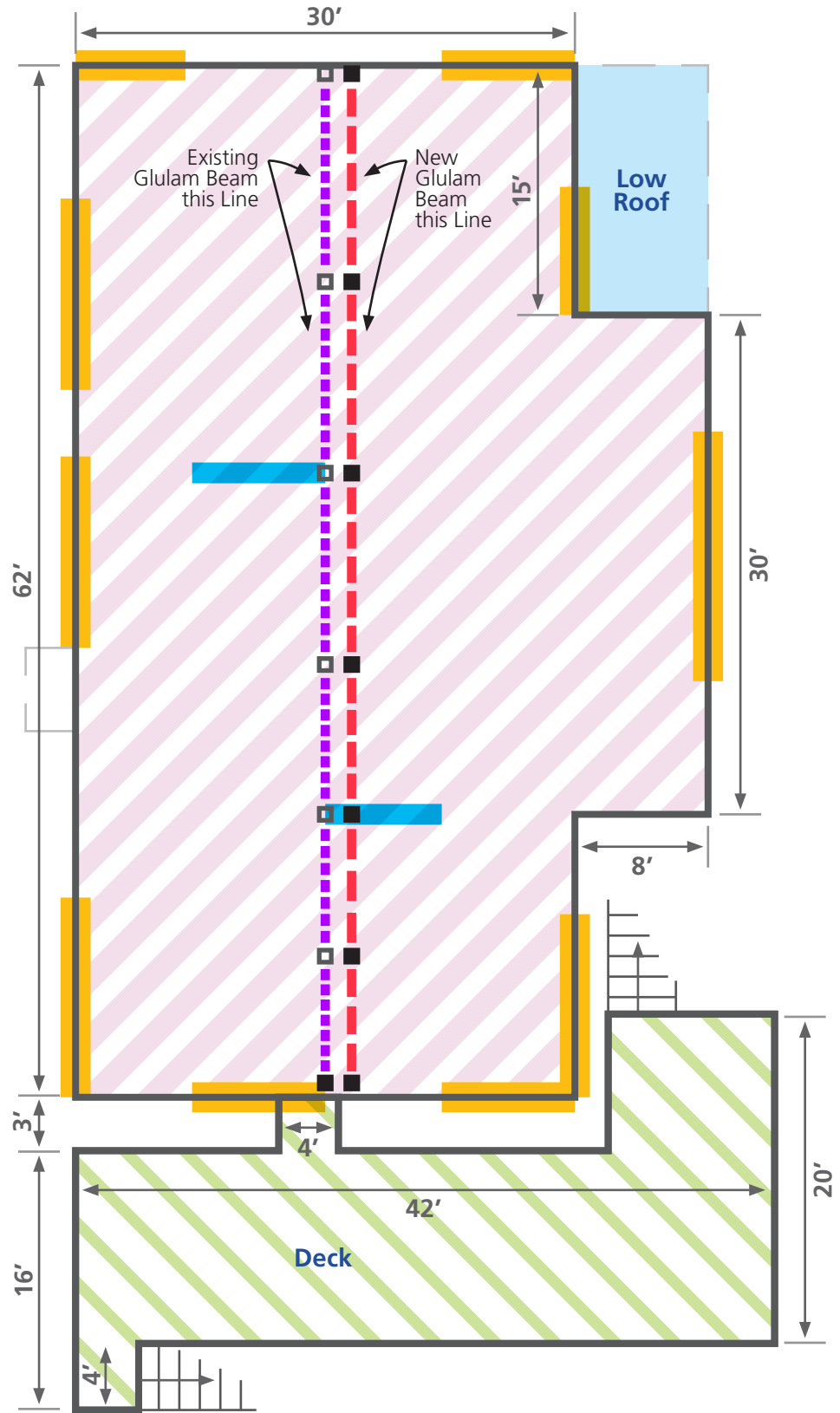




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

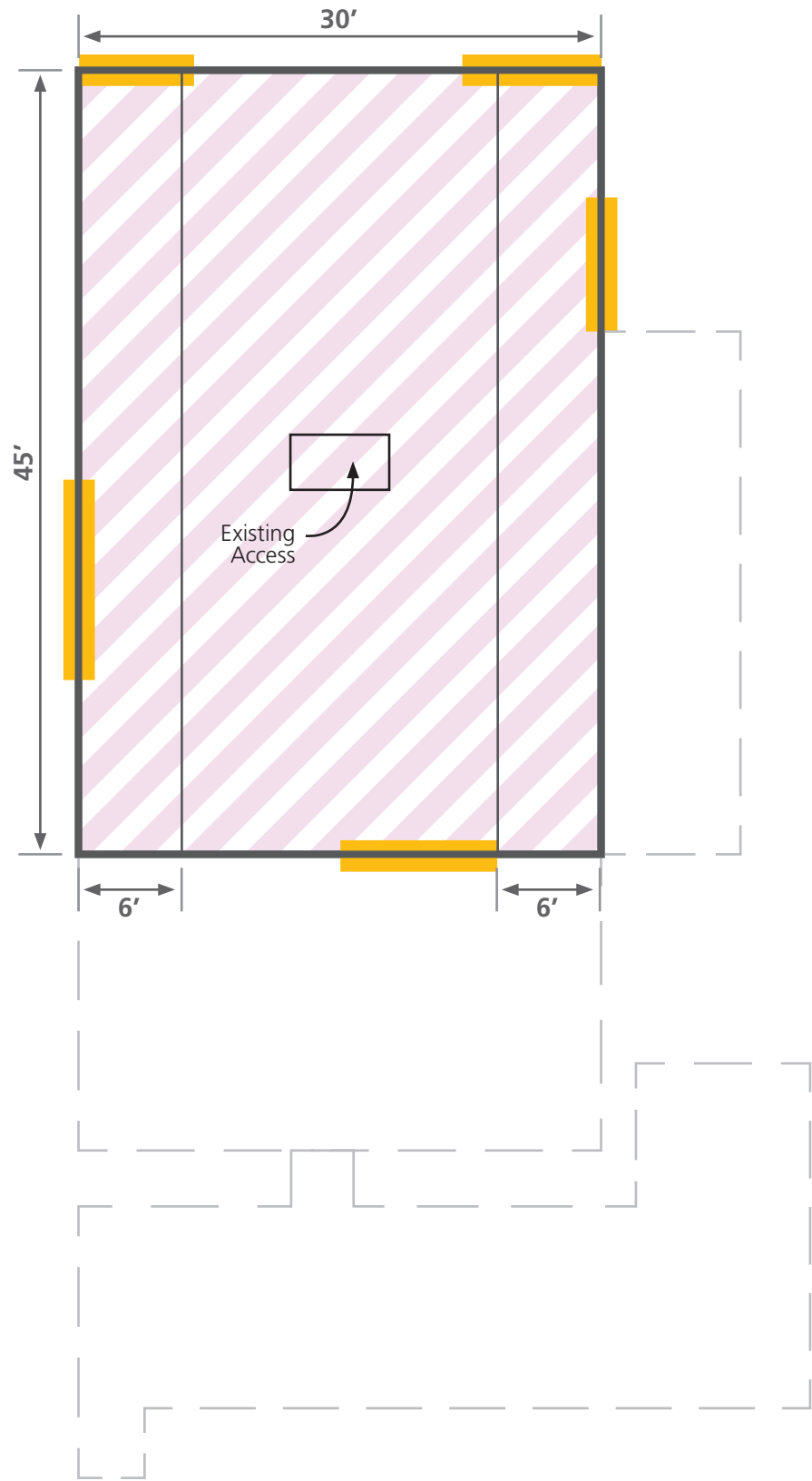


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

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File No. 262018.077

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Rice Fergus Miller Architecture & Planning
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The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

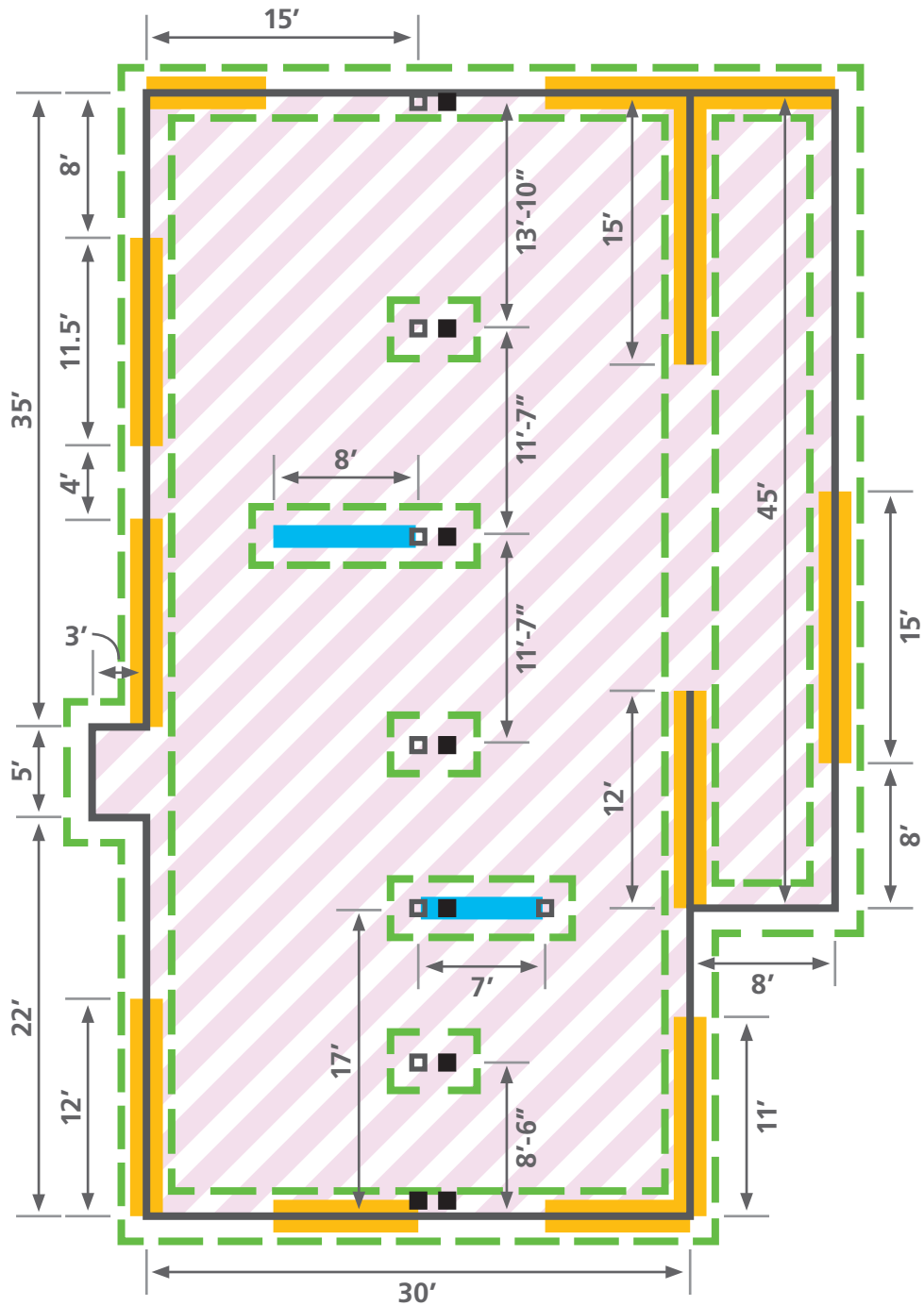


Katherine R. Brawner, P.E.
Project Engineer

Attachments

sah\26\18\077 pos old town pub structural evaluation\reports\180910 old town pub_letter report.docx\krb






-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

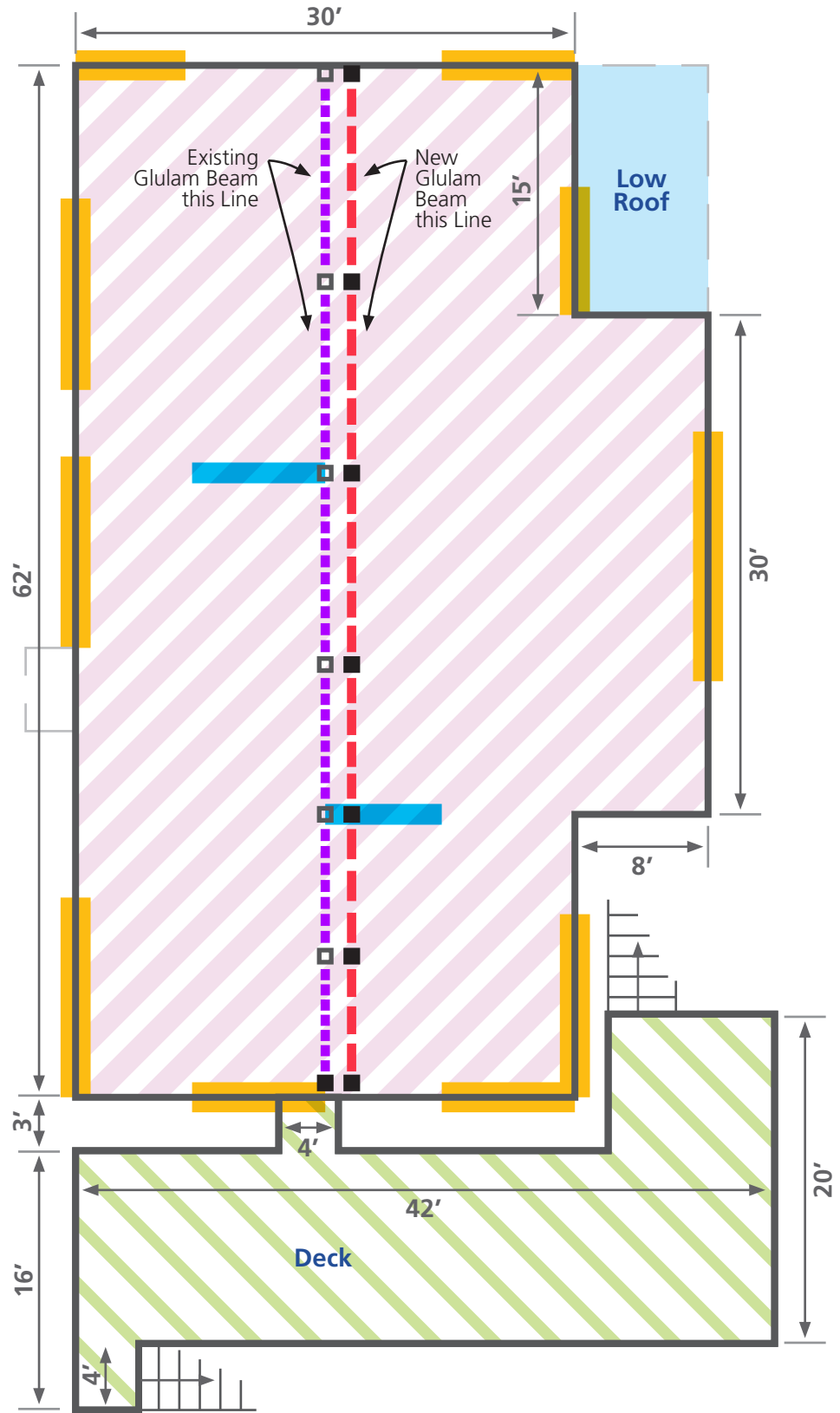


Figure 2 - Second Floor

- Upgrade (E) Shear Wall
- Attic Access Only Unless Bottom Chord of Truss is Upgraded

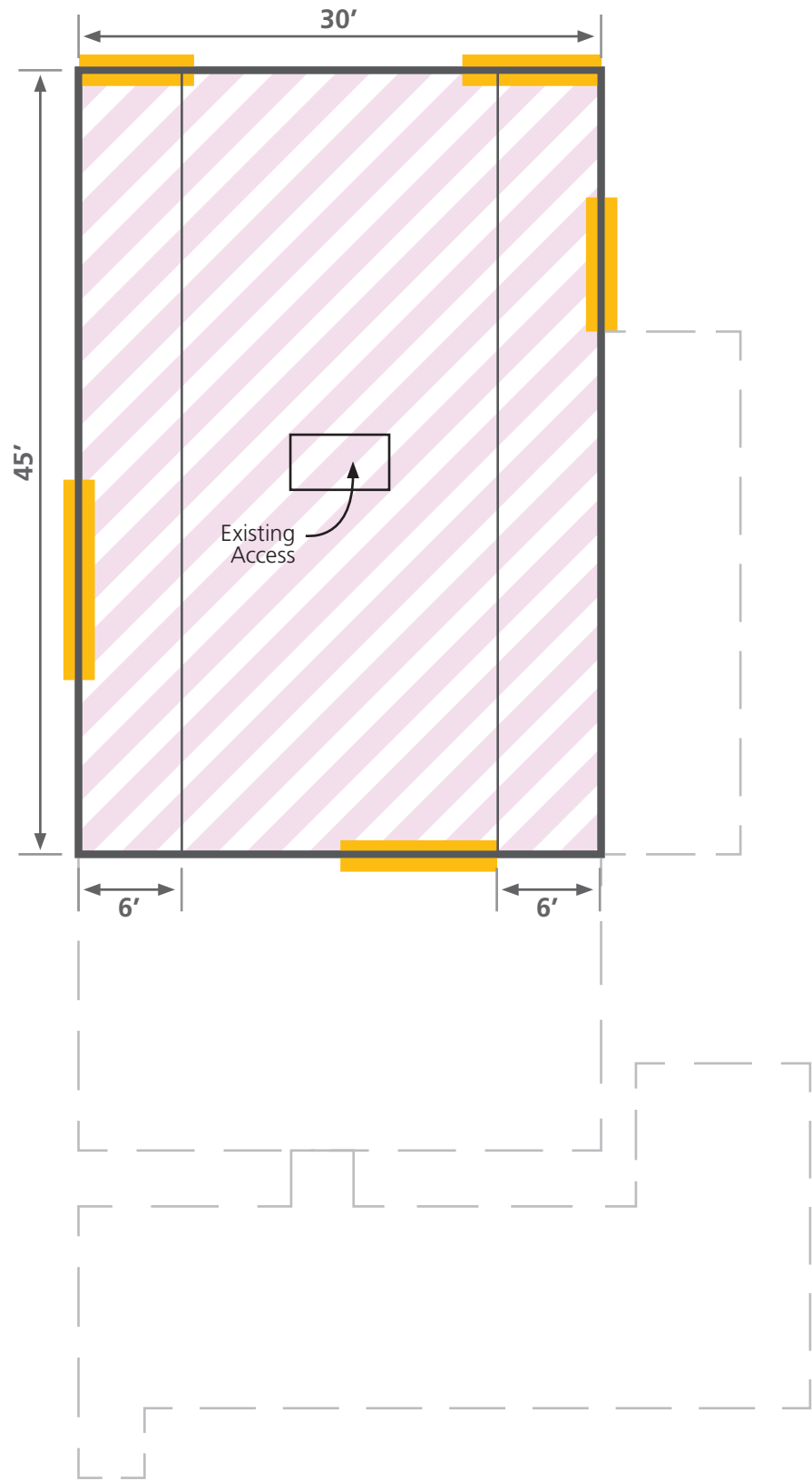


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.








Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

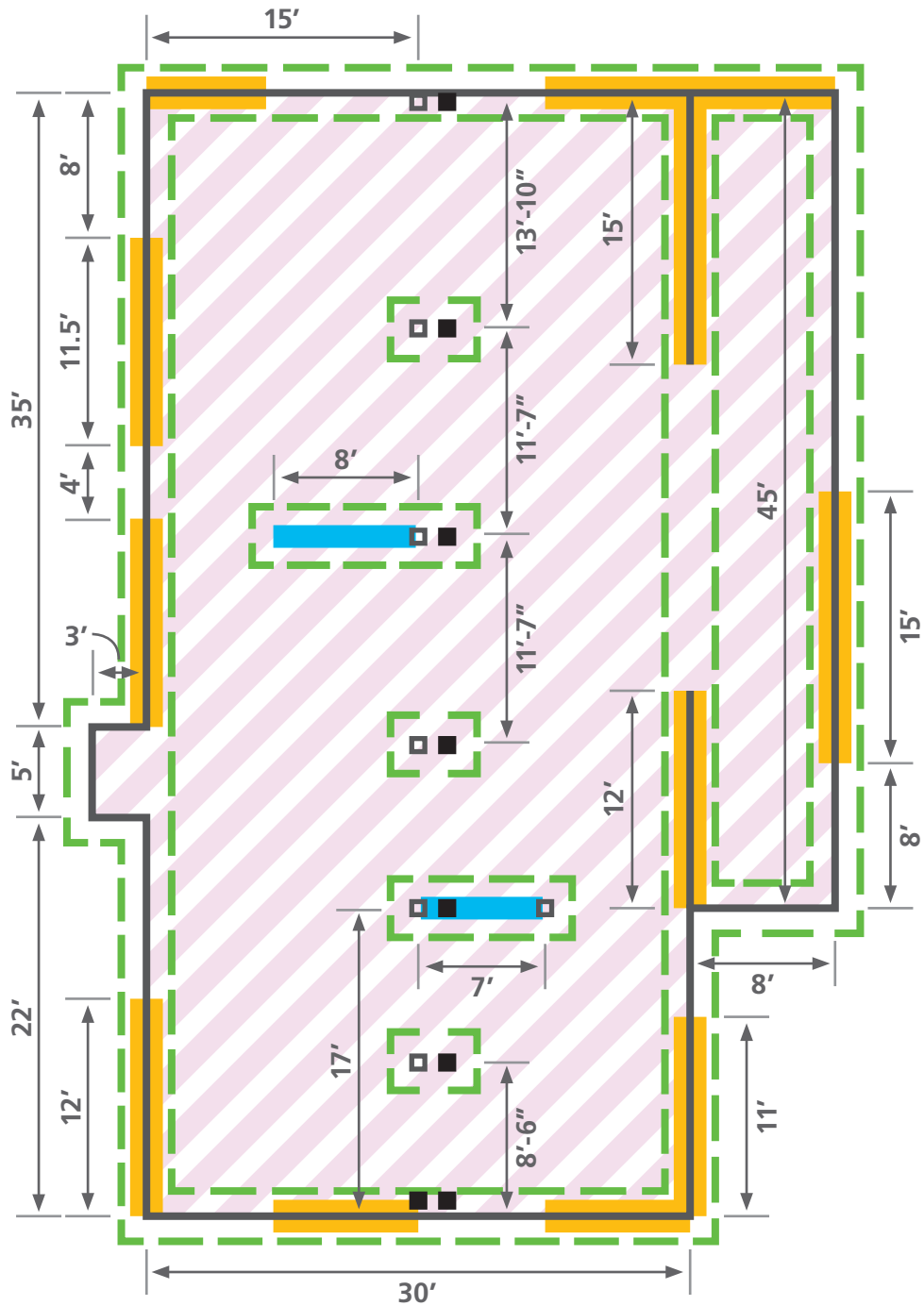


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

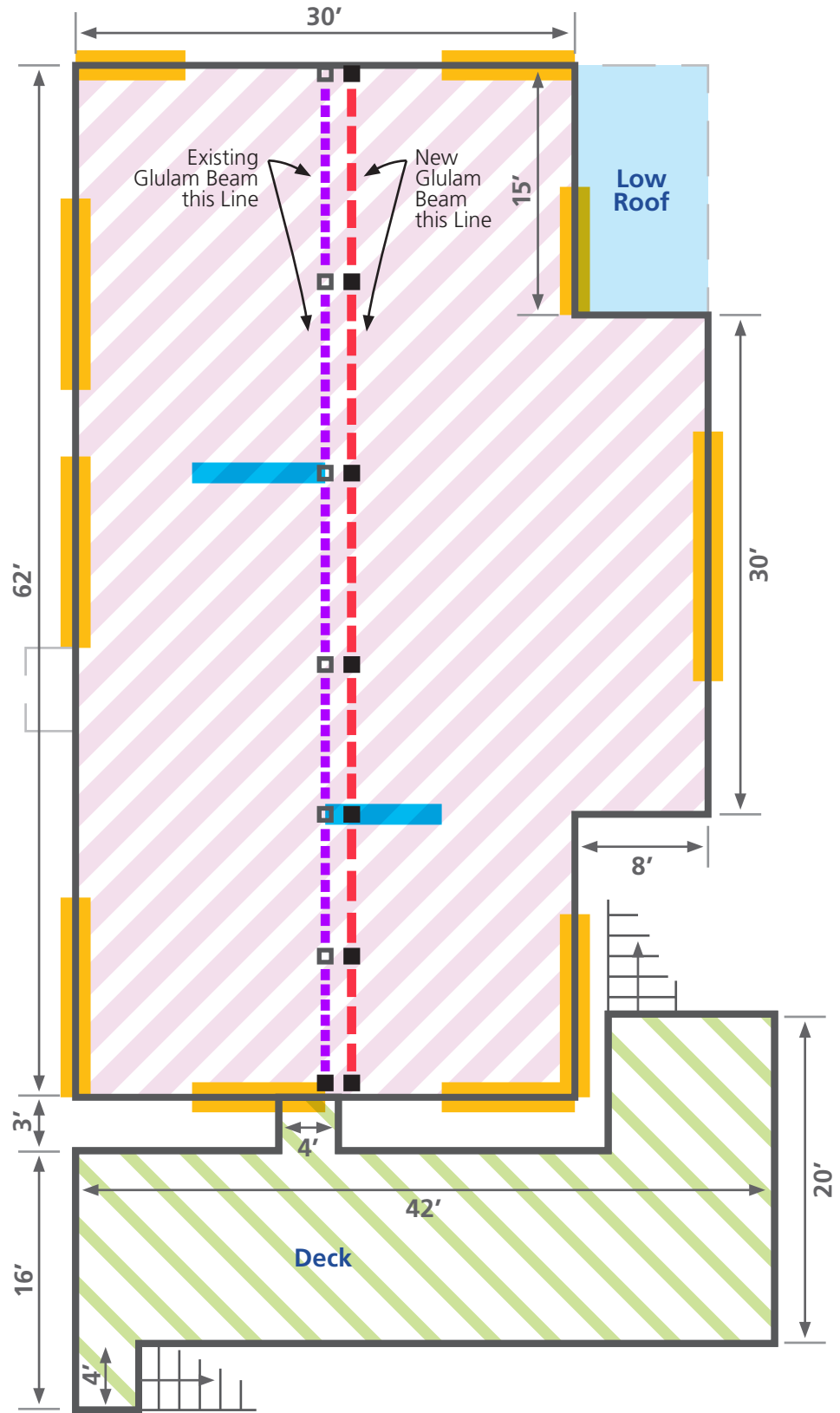


Figure 2 - Second Floor

- Upgrade (E) Shear Wall
- Attic Access Only Unless Bottom Chord of Truss is Upgraded

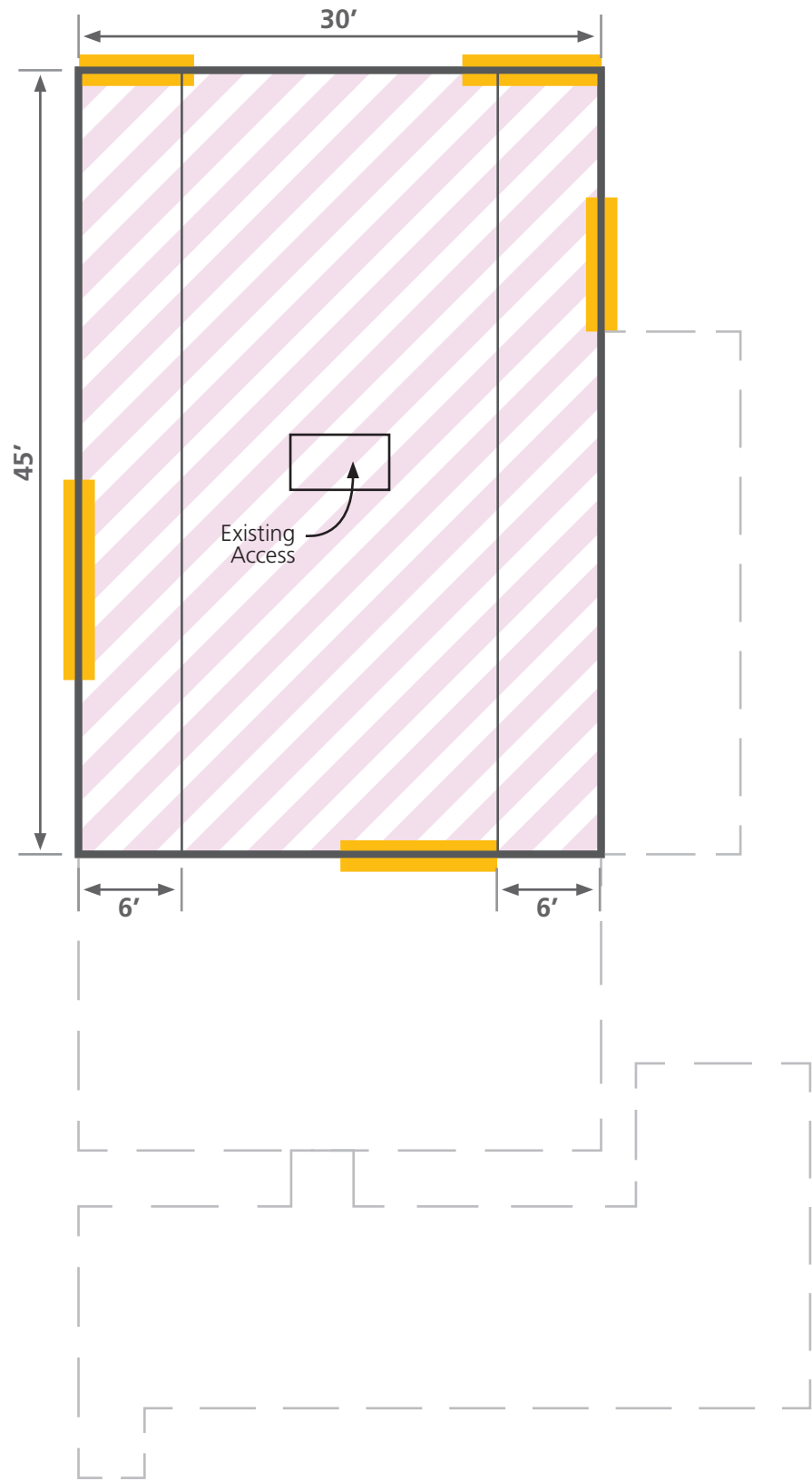


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
September 10, 2018
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Page 5

new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

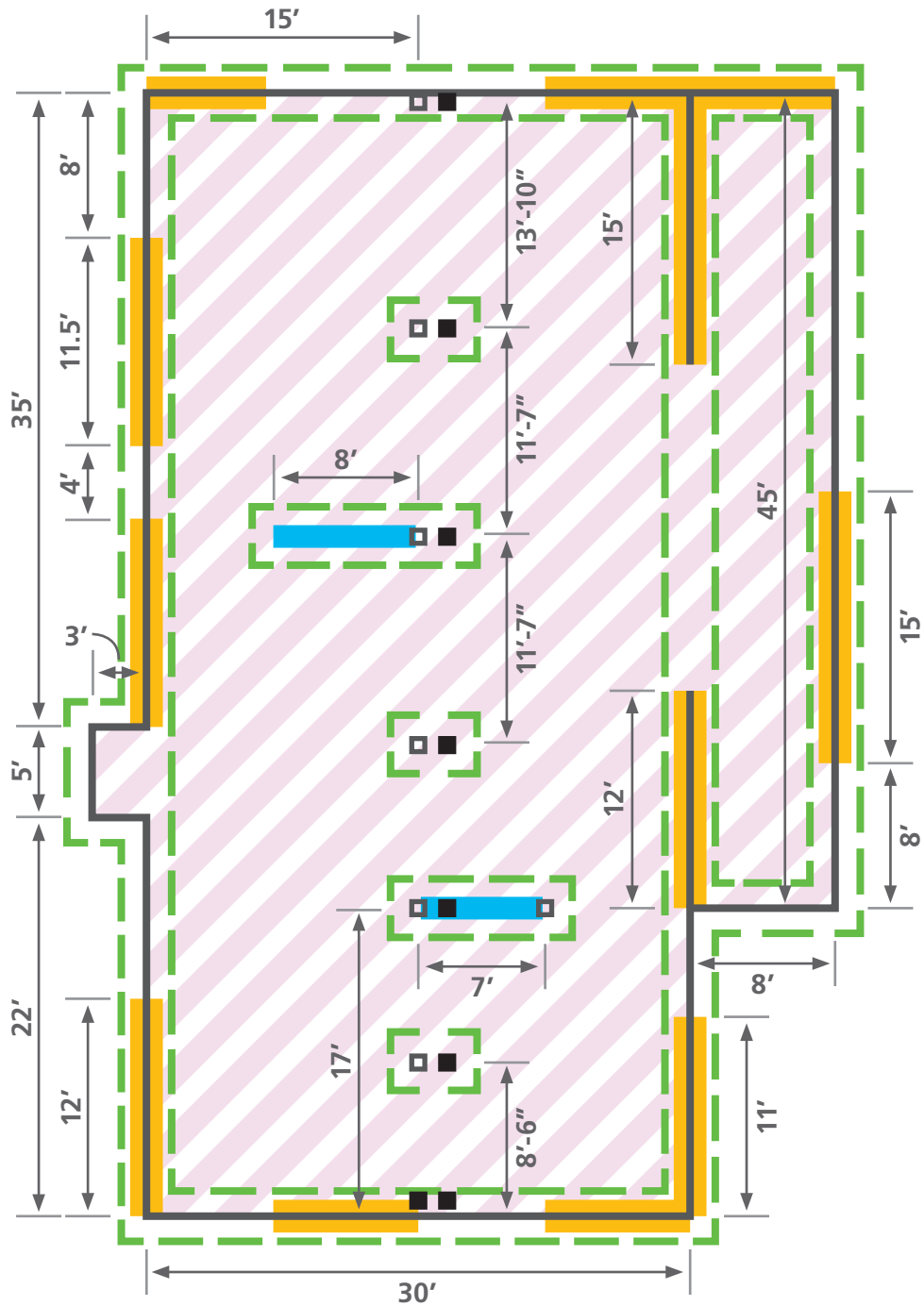


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

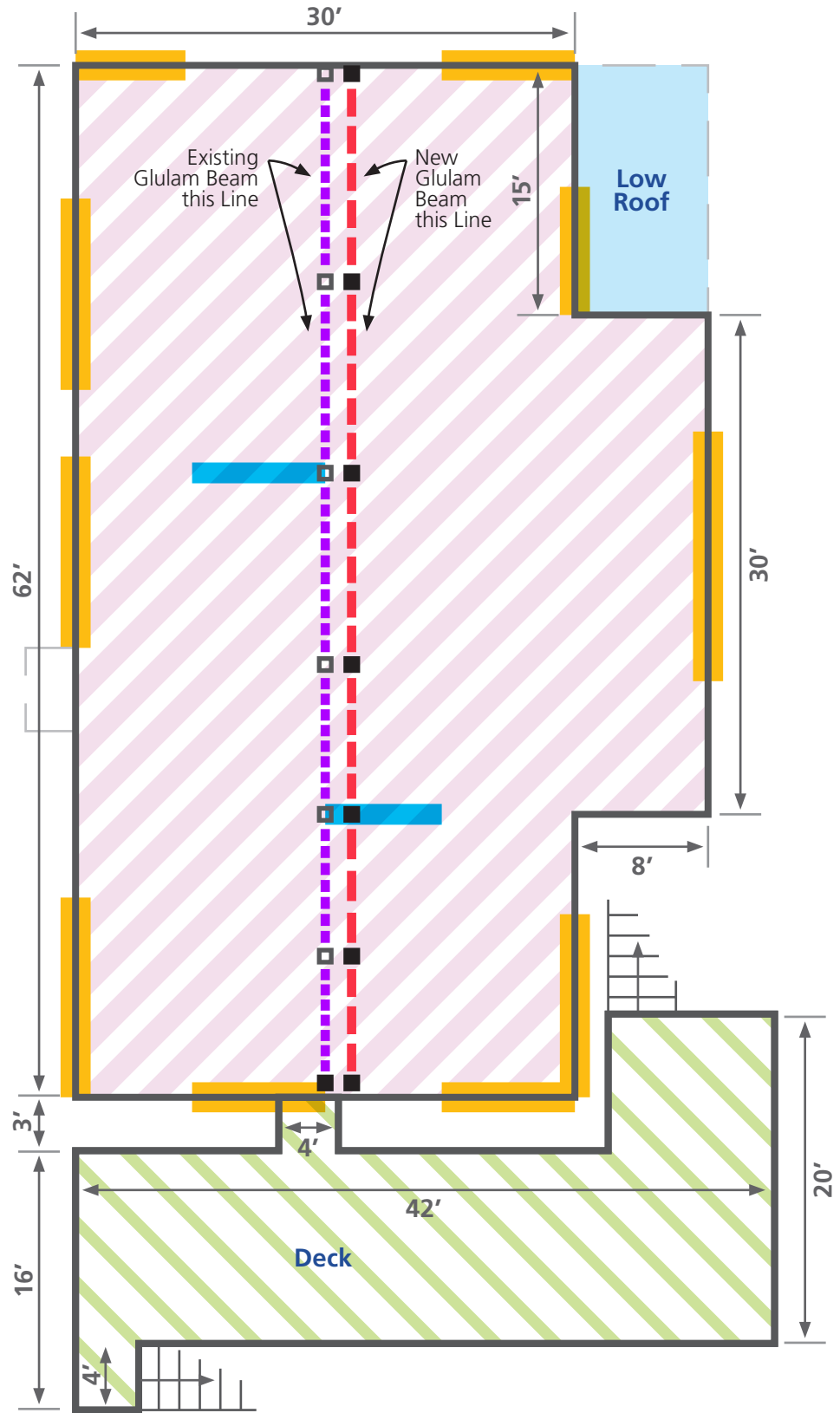




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

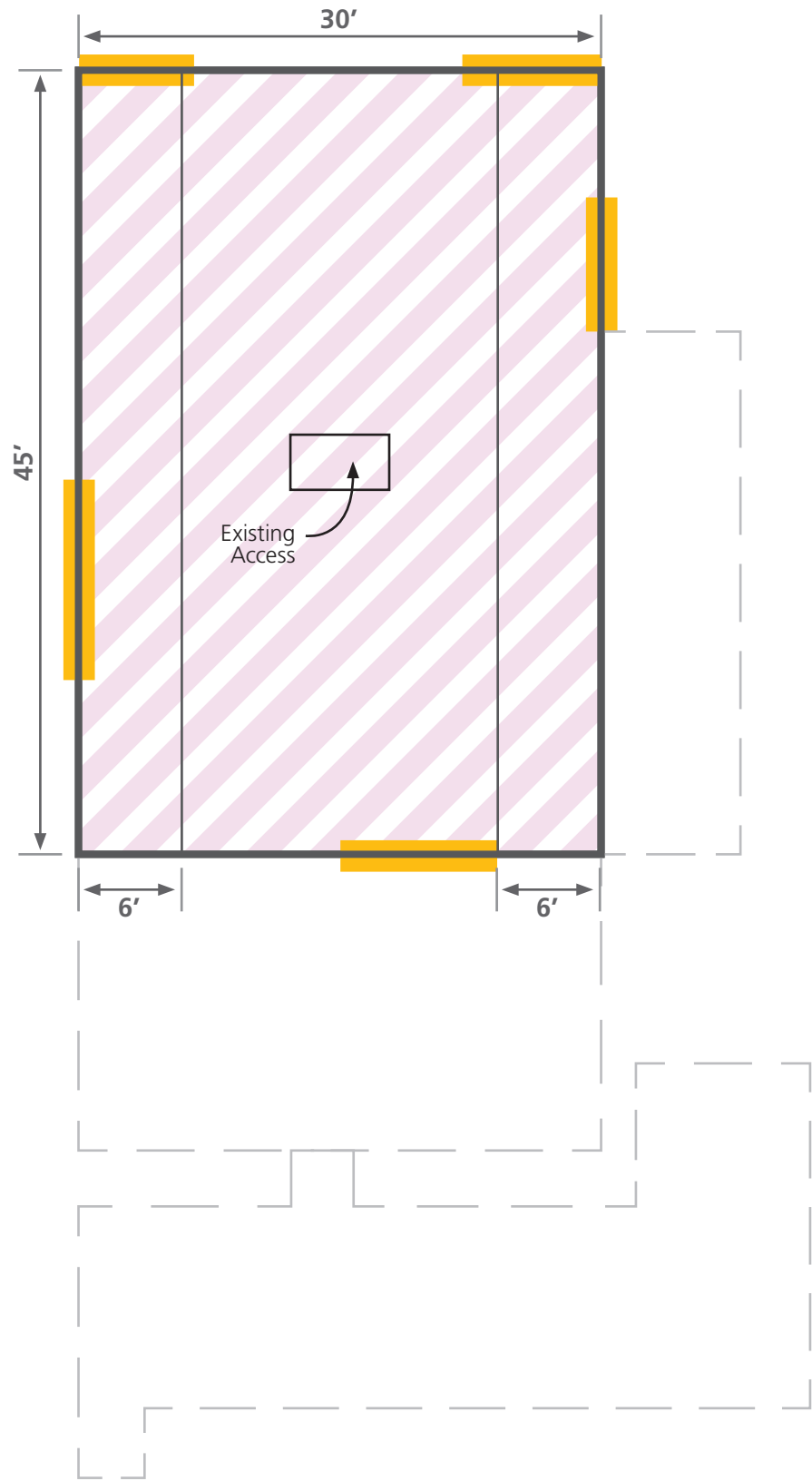


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

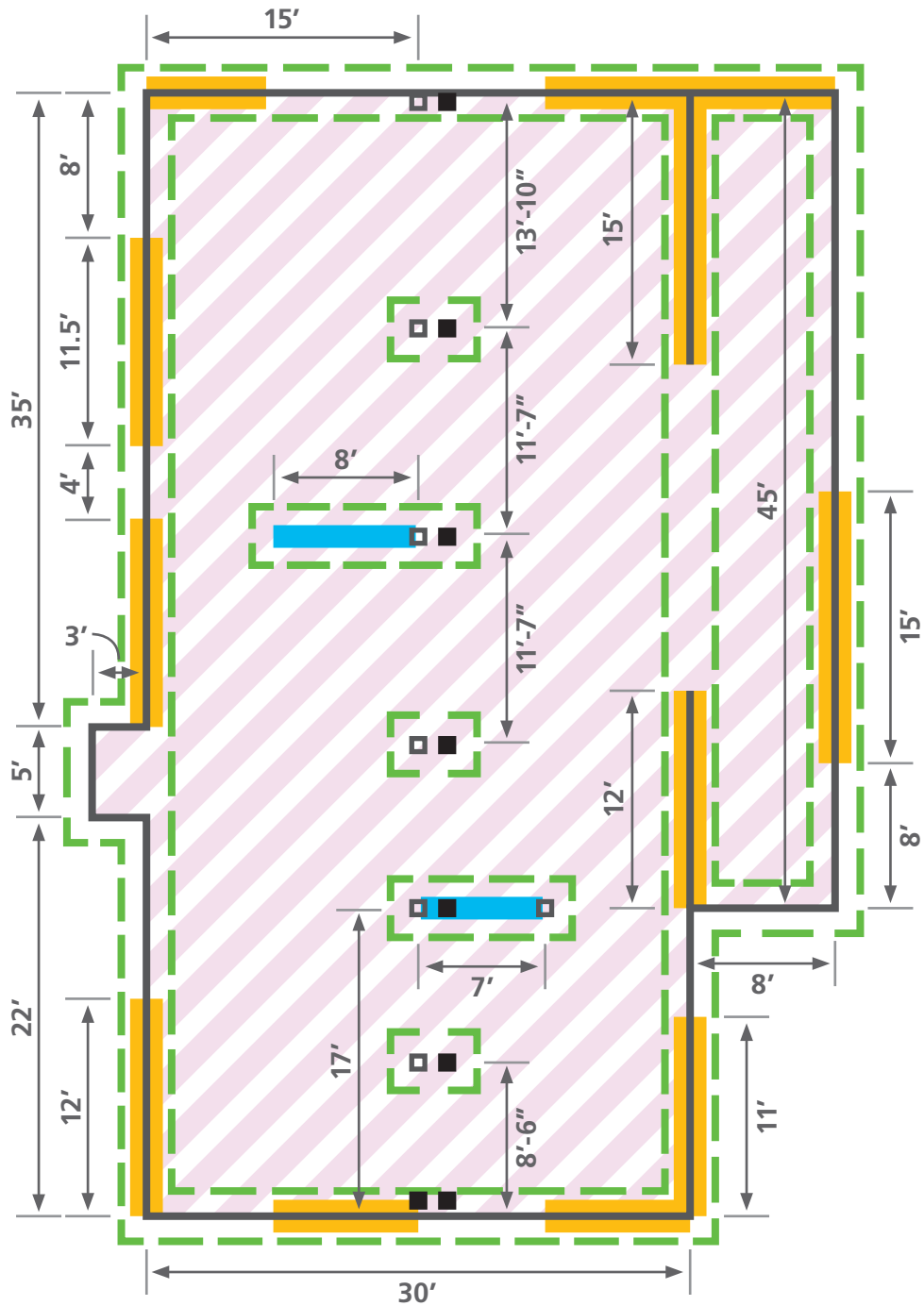


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

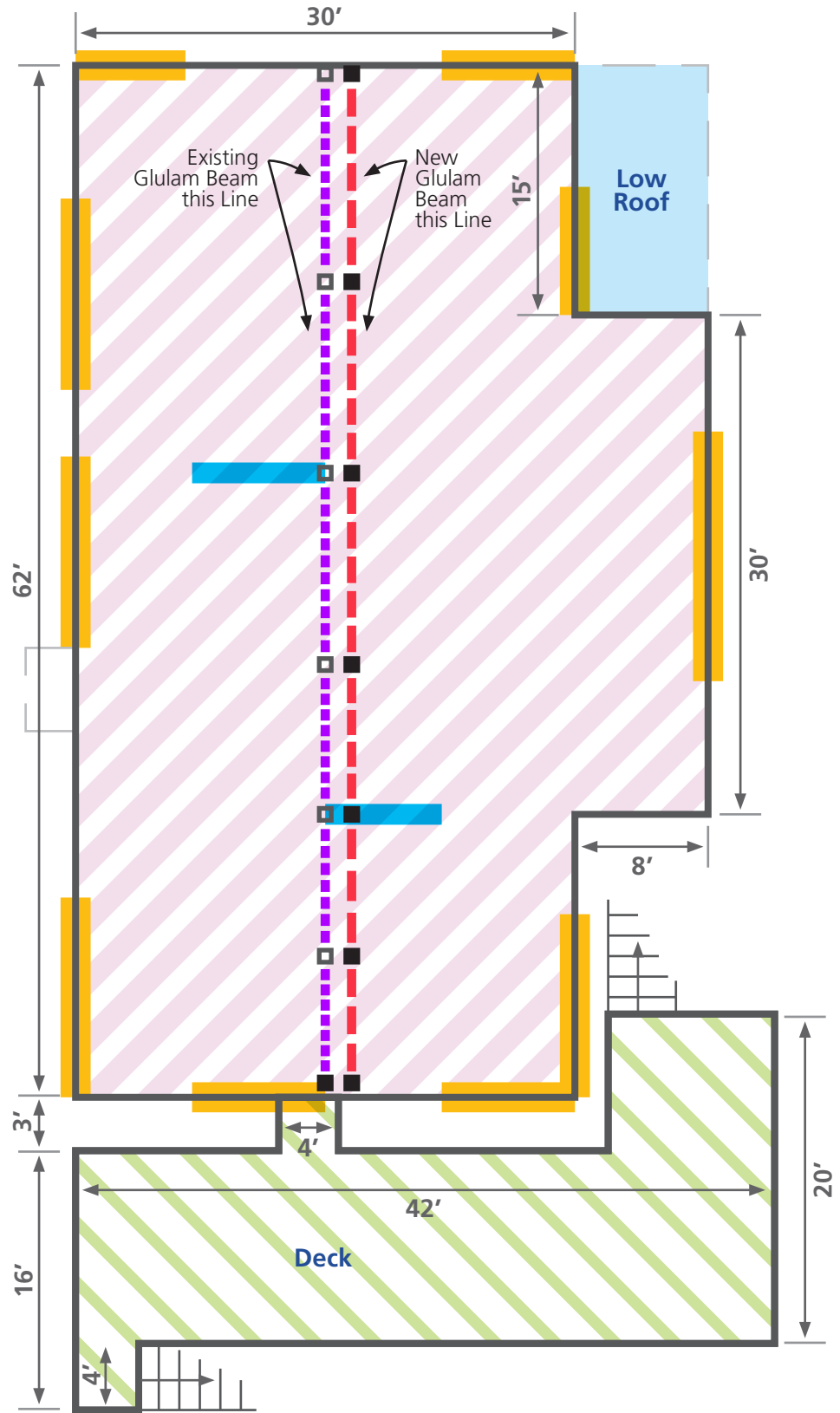




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

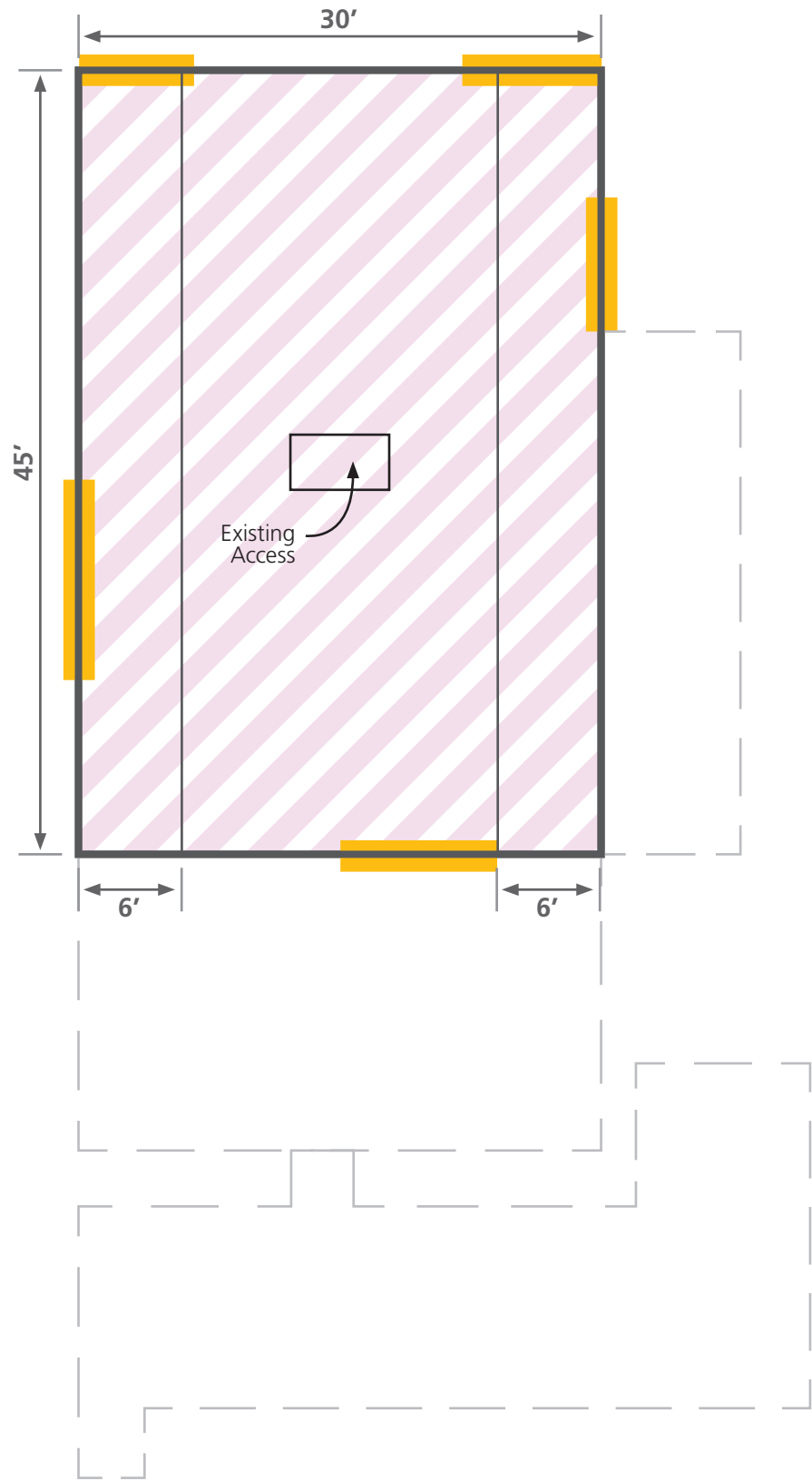


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSUM WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.








Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

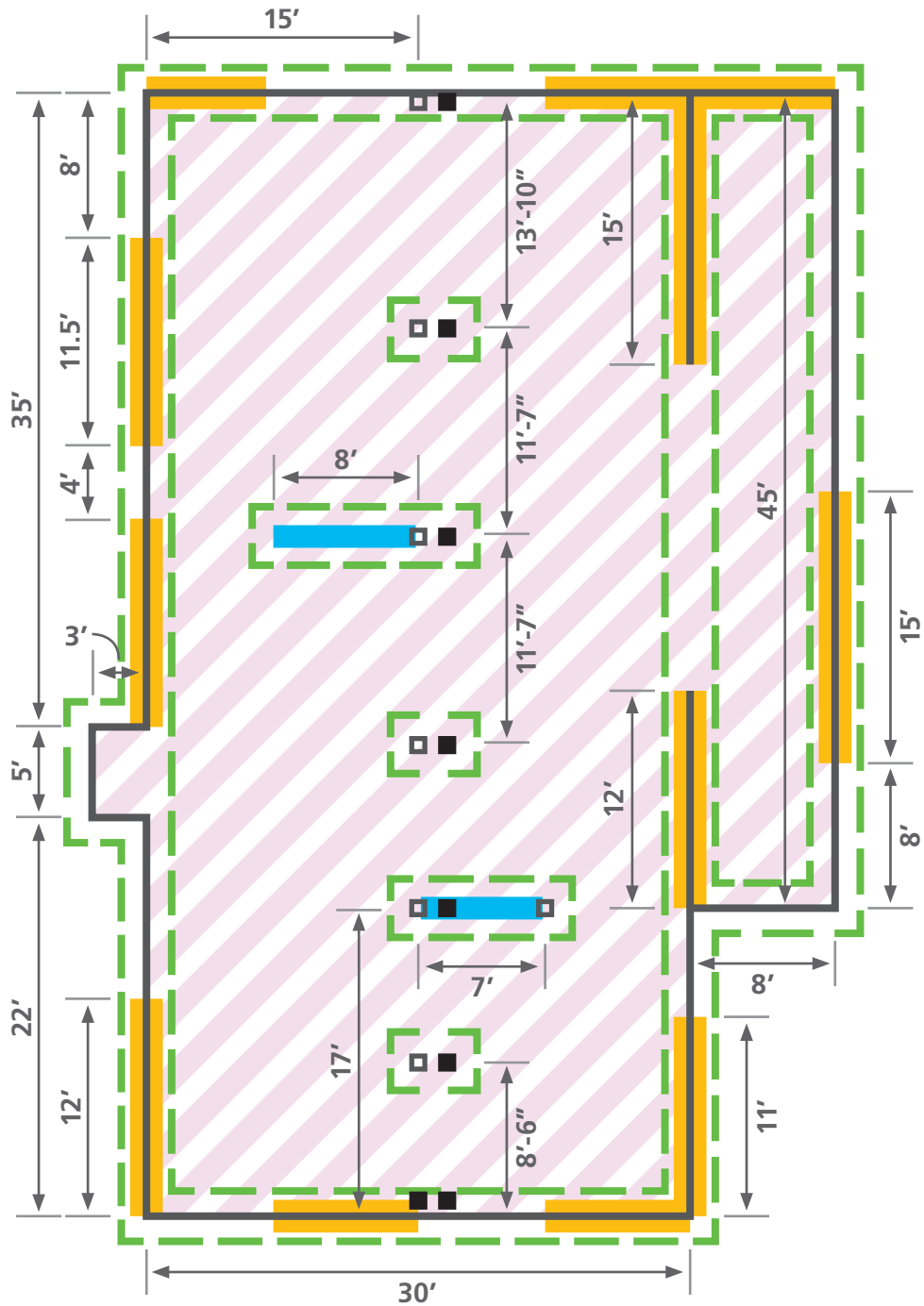


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation.
 Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

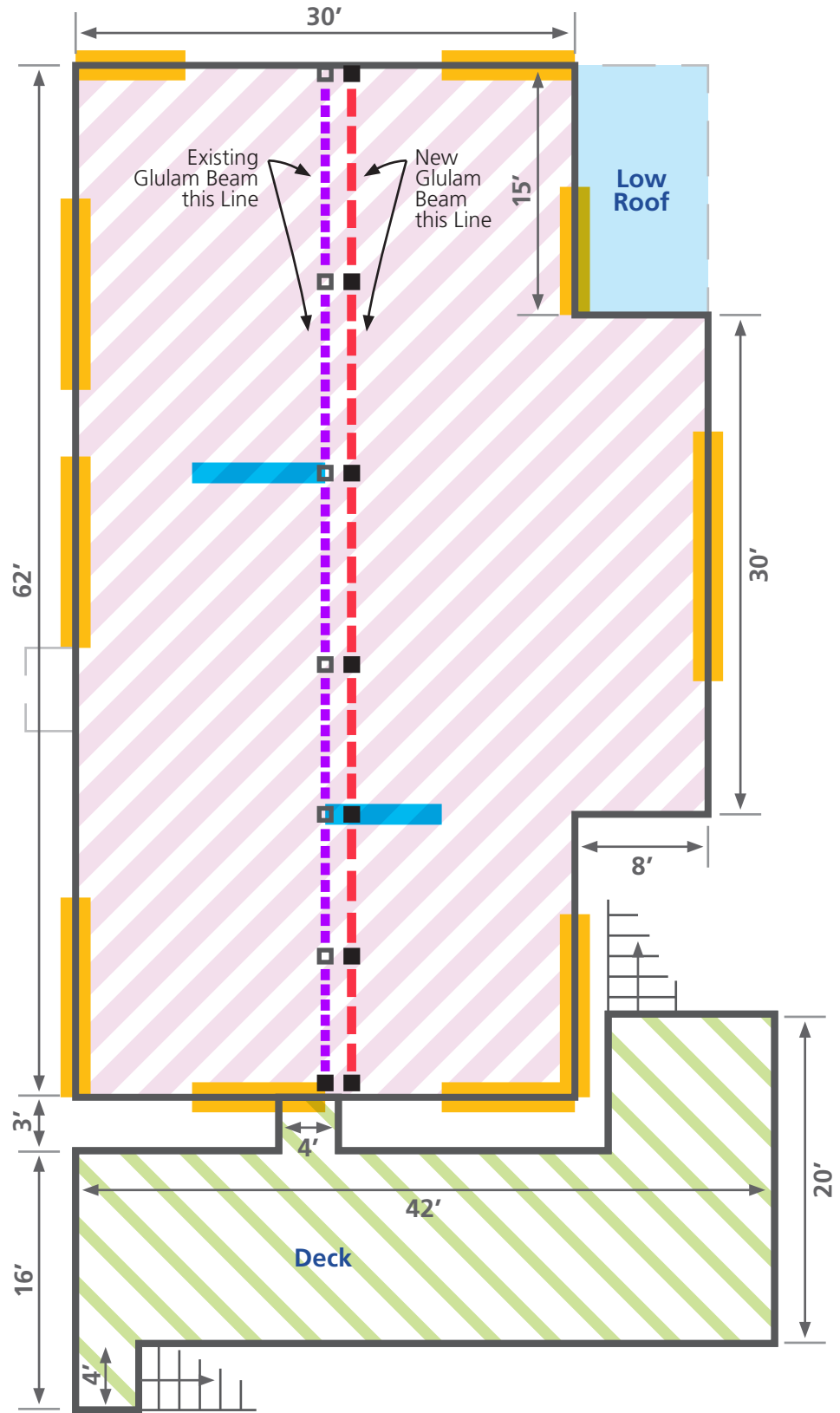


Figure 2 - Second Floor

- Upgrade (E) Shear Wall
- Attic Access Only Unless Bottom Chord of Truss is Upgraded

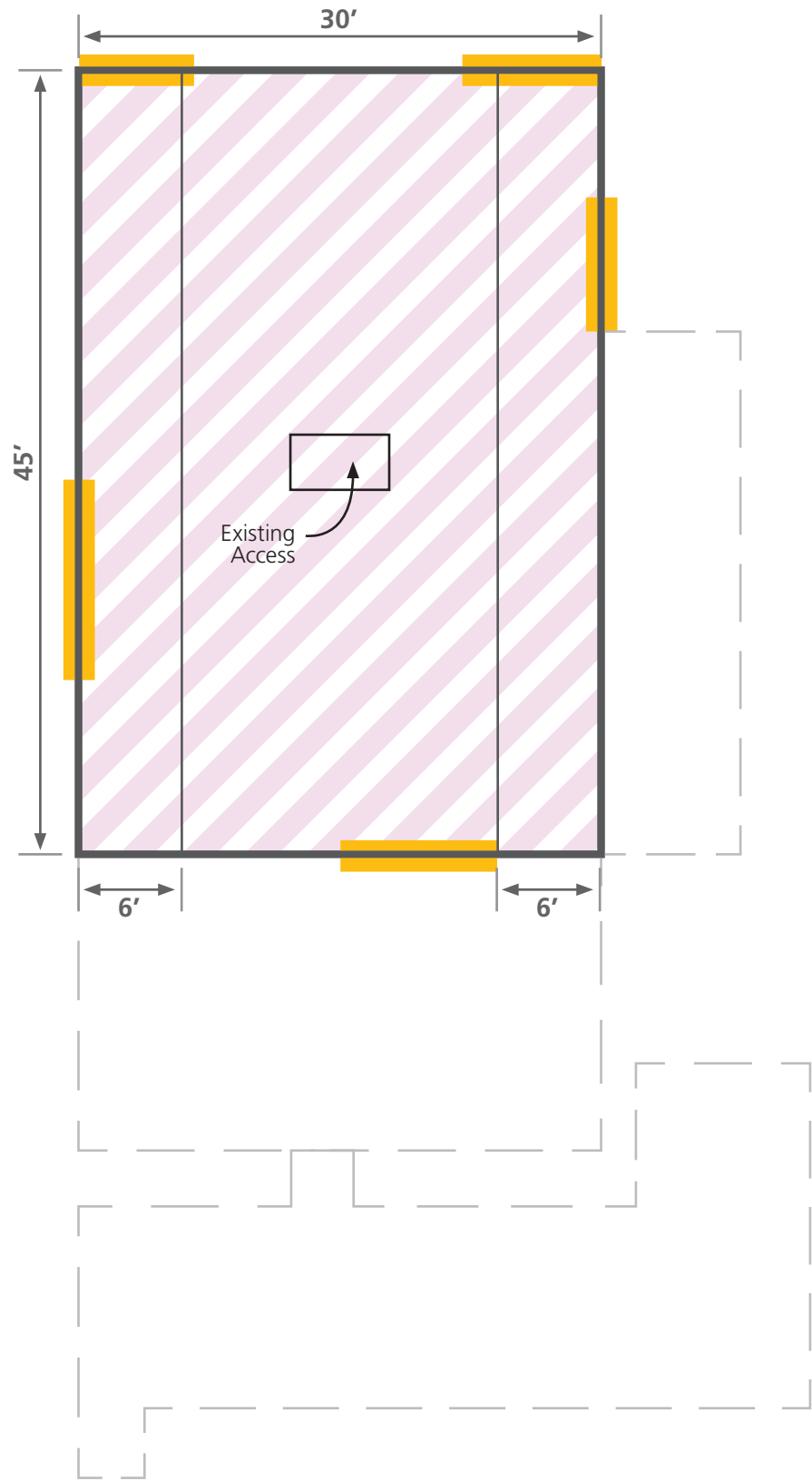


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

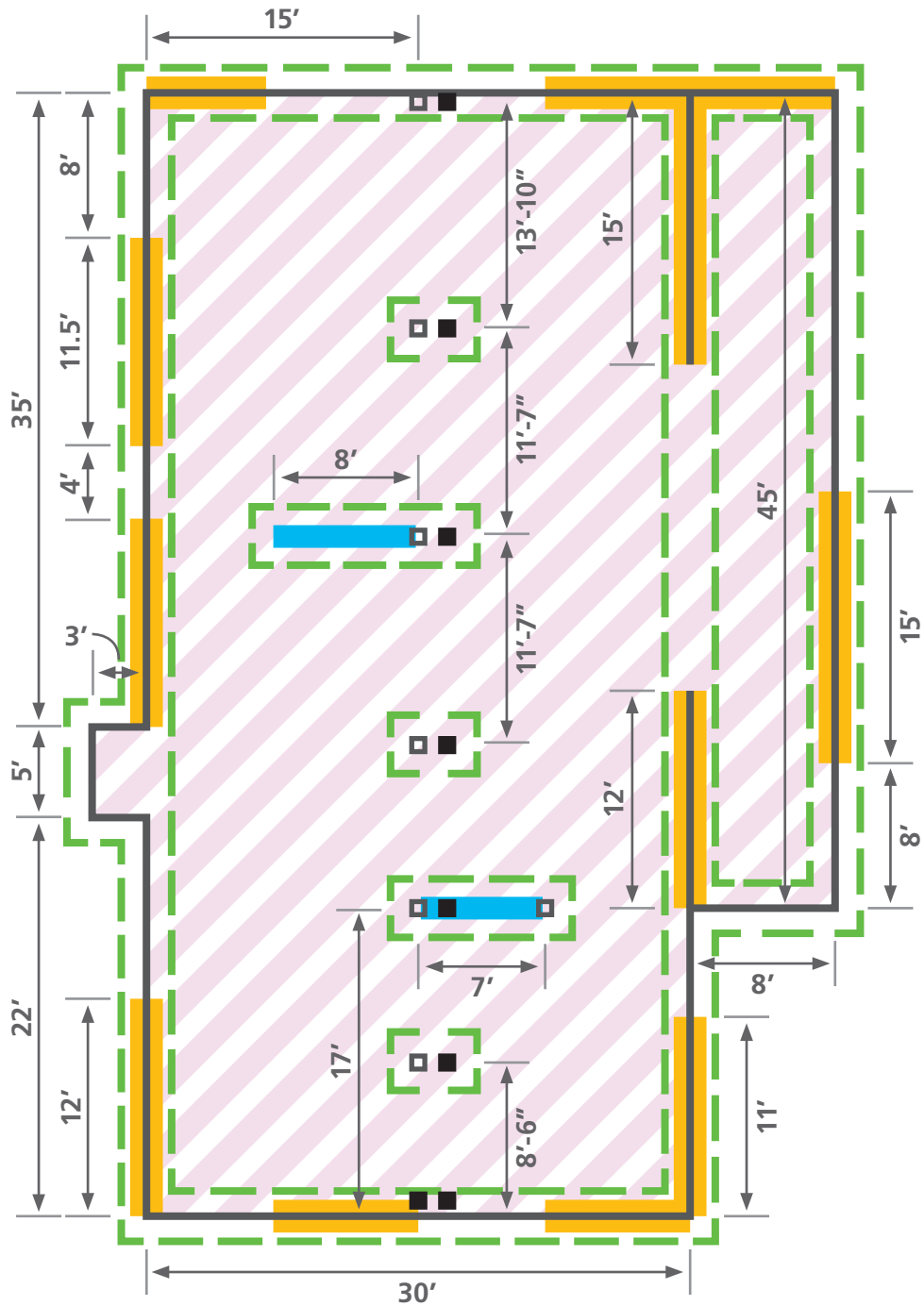


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

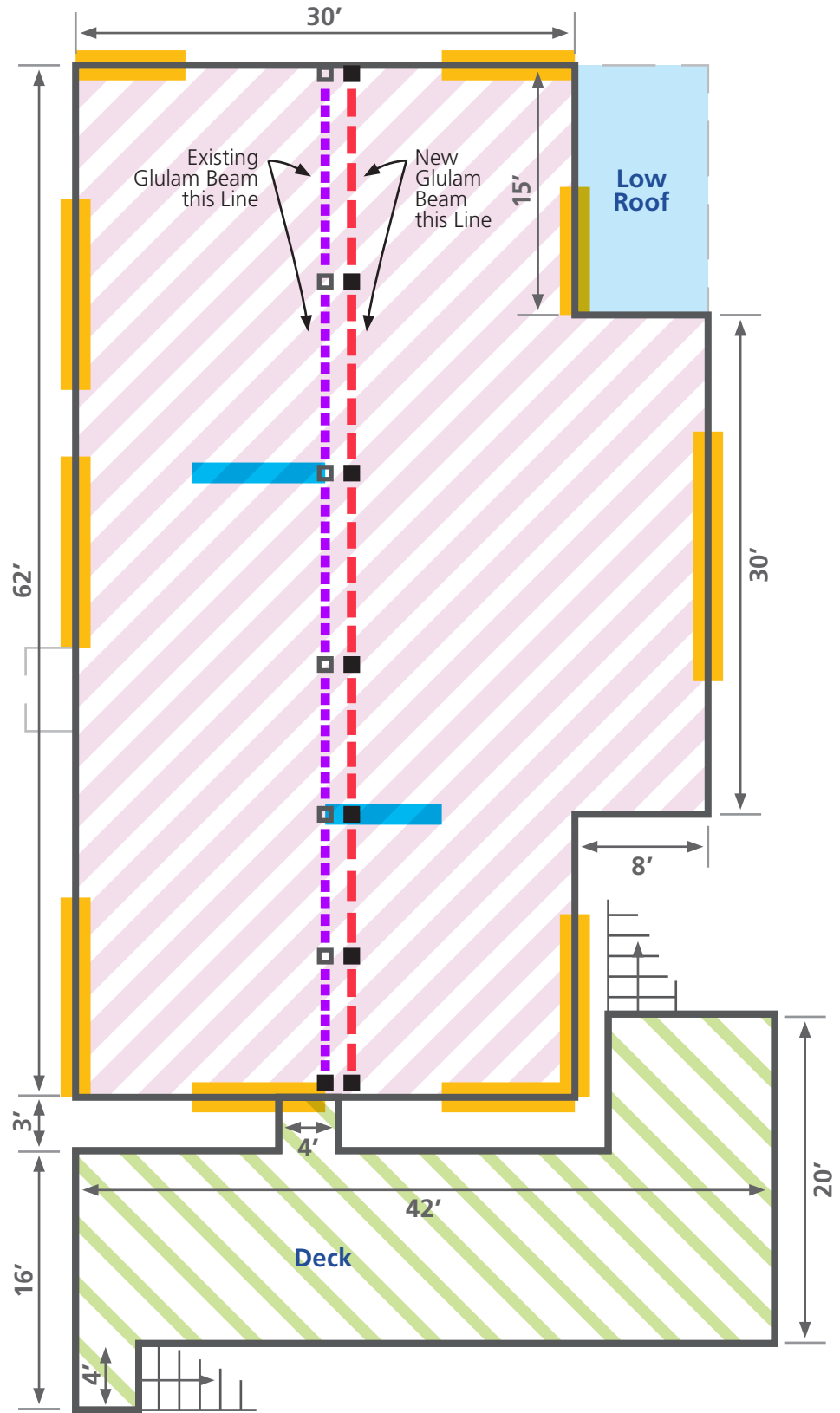




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

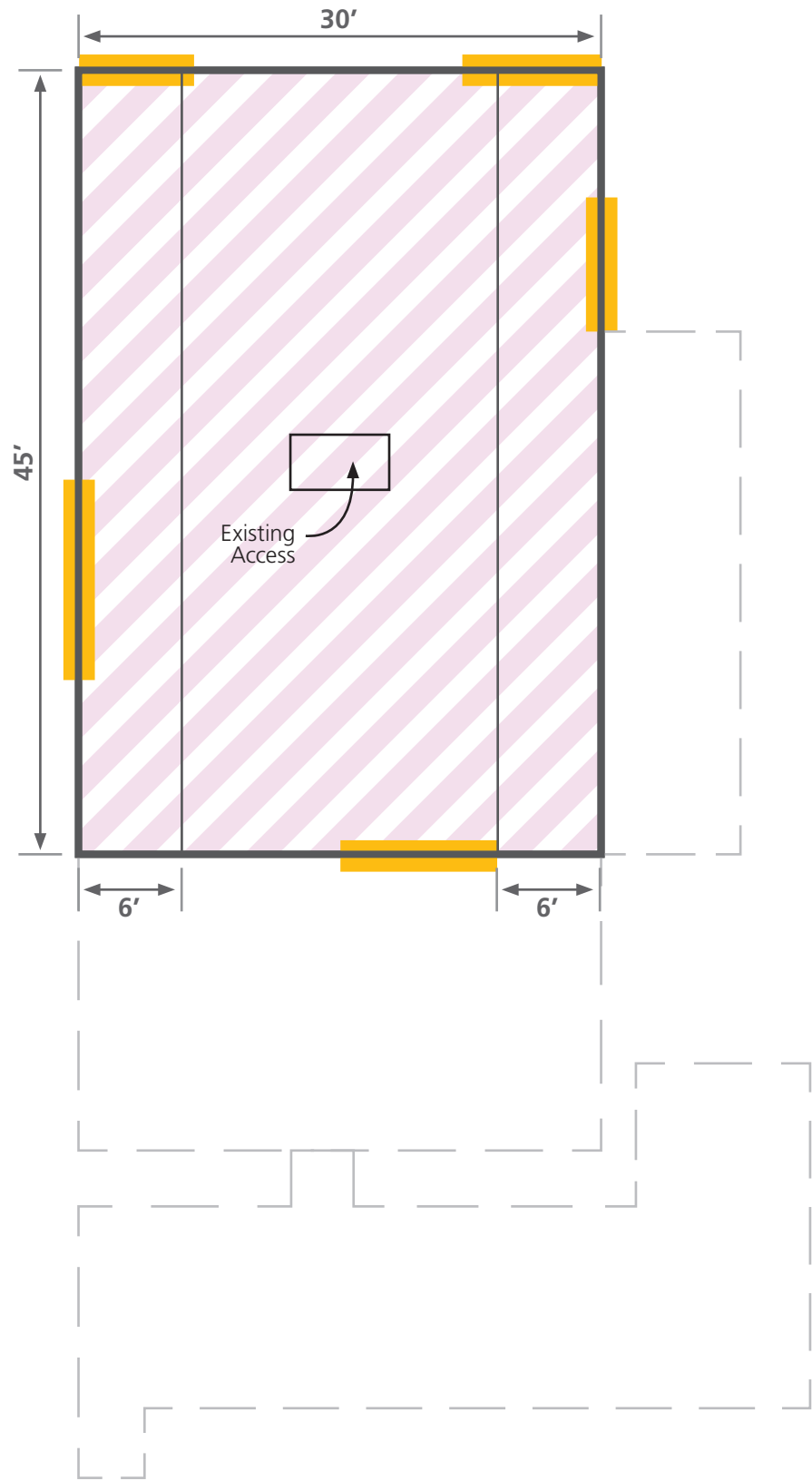


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

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Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

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Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

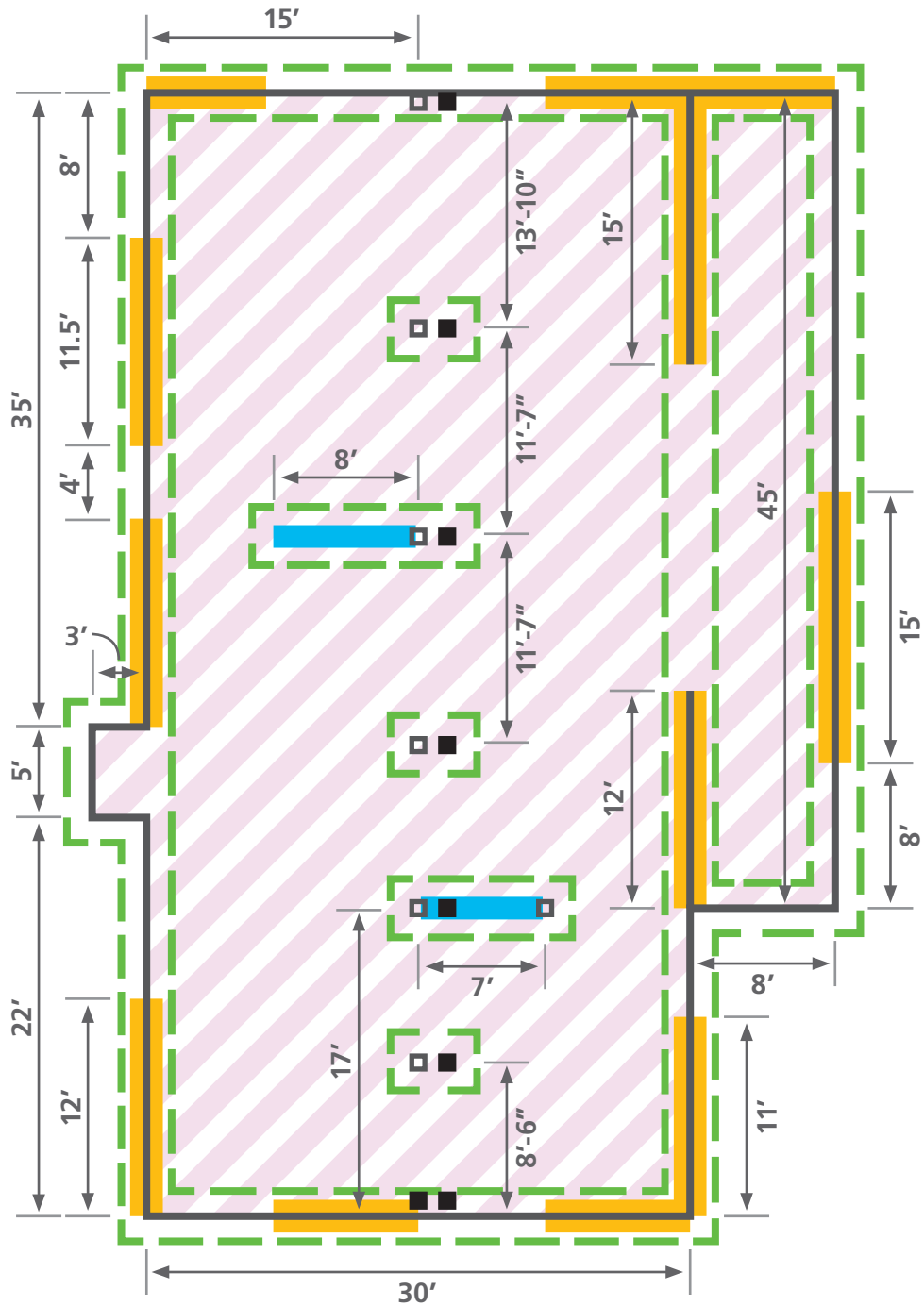


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
-  New & Existing Column Below

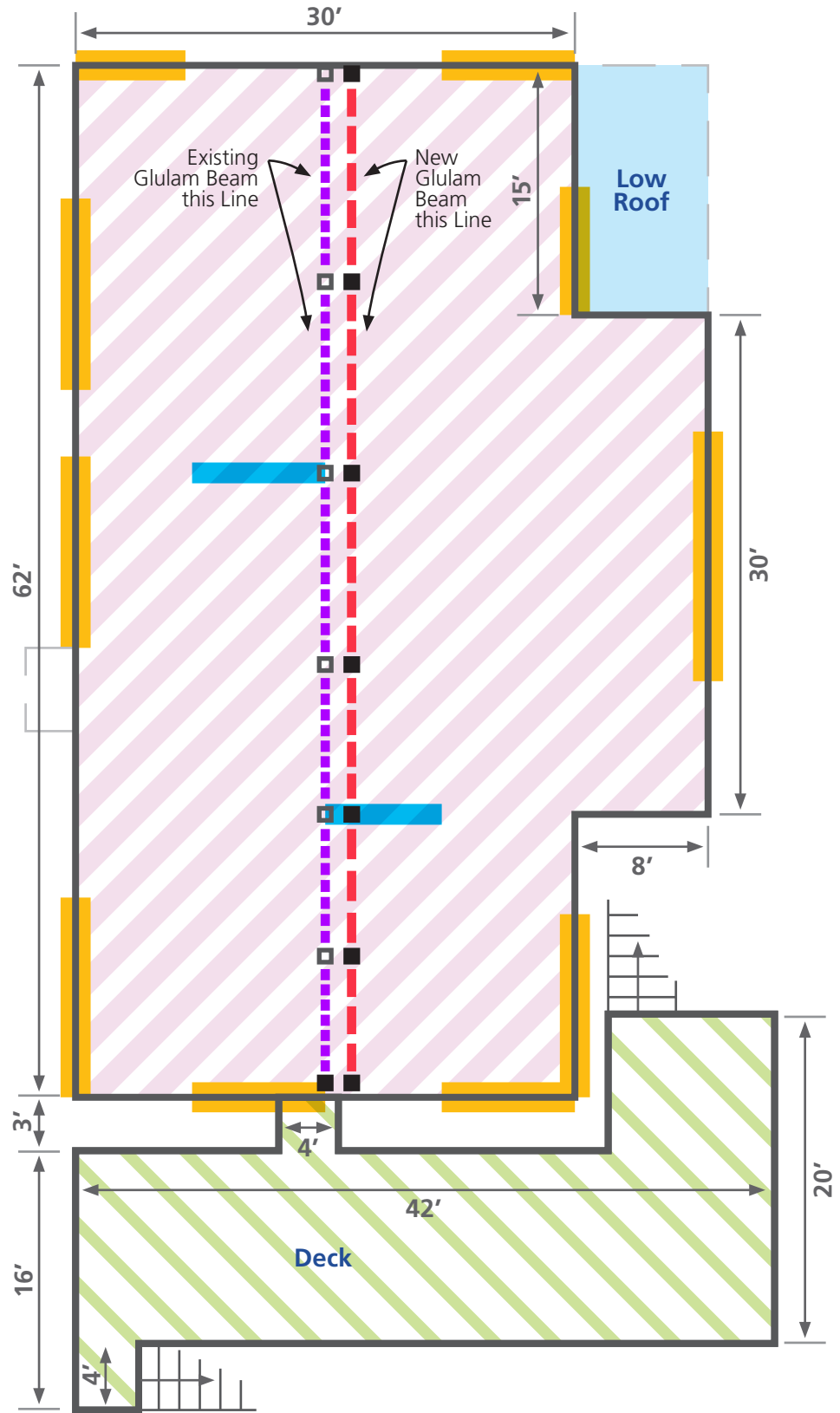




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

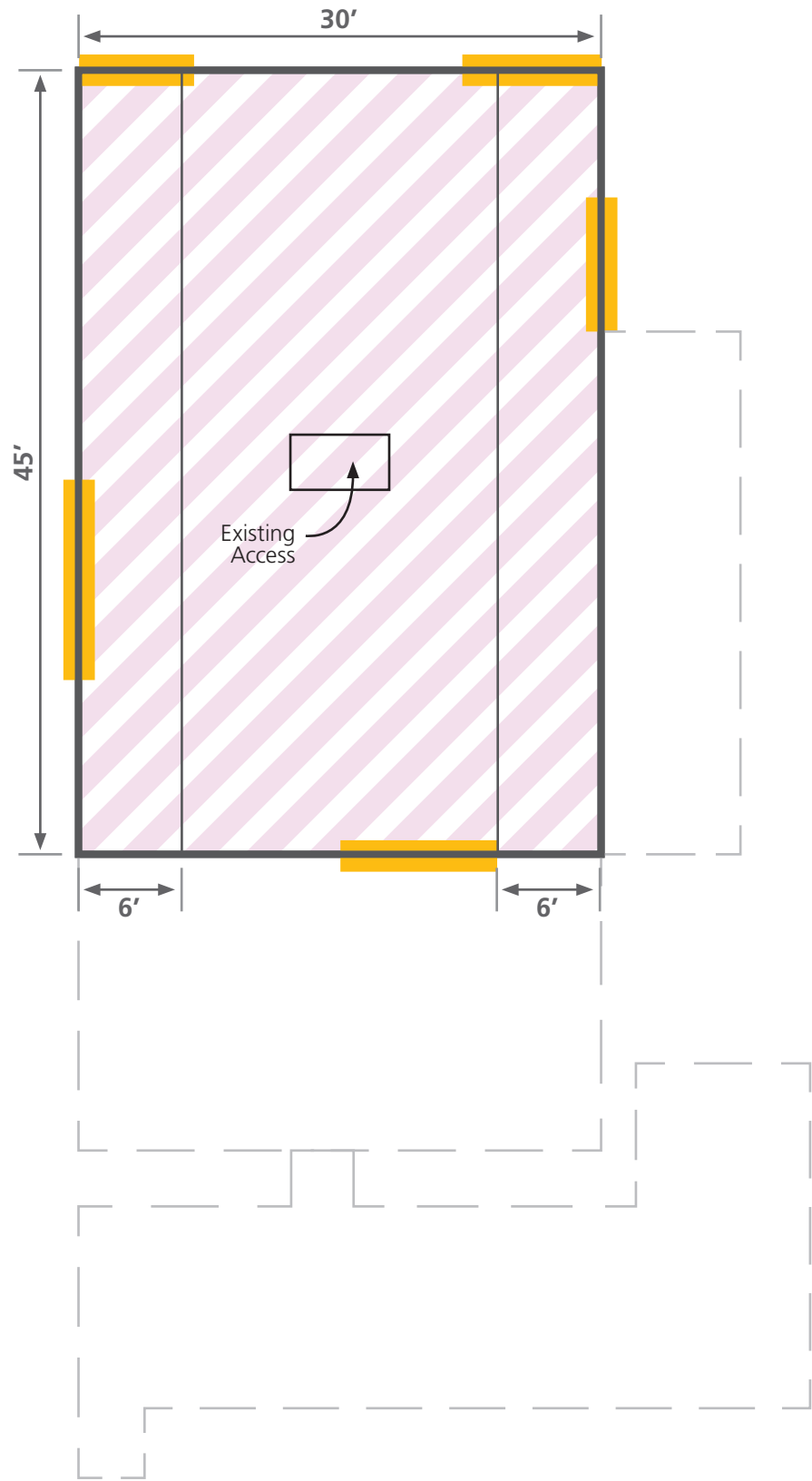


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSUM WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

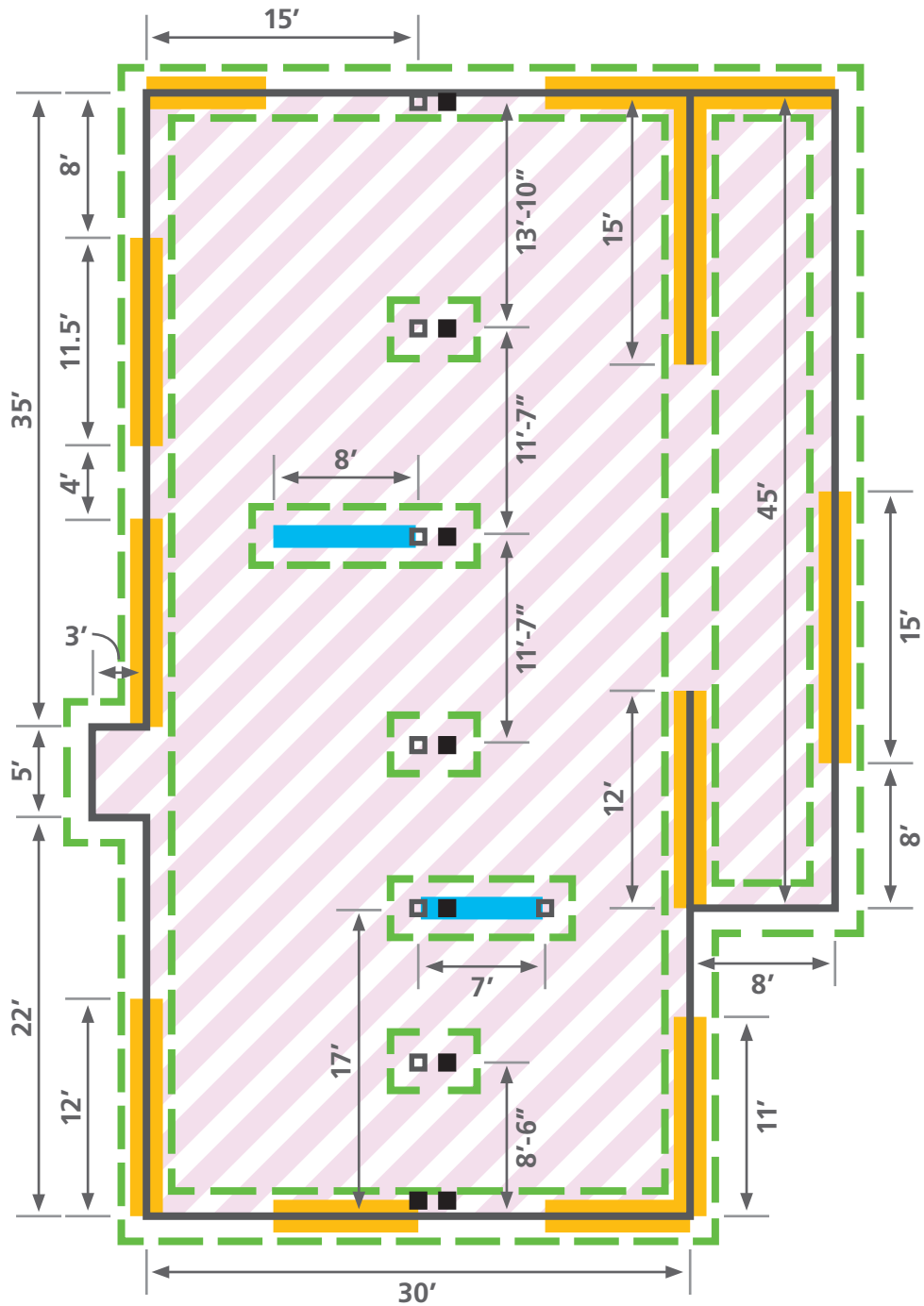


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation. Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
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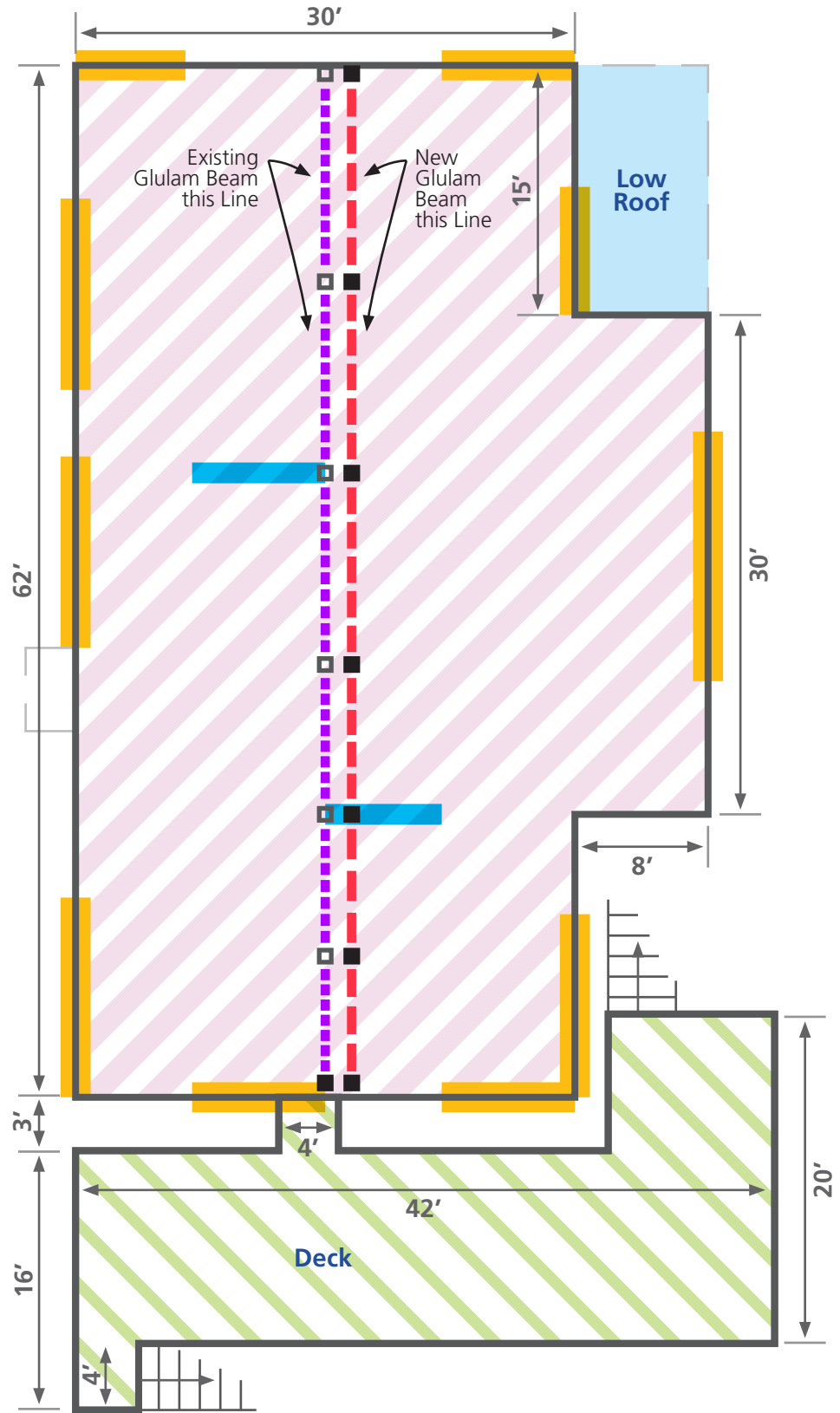




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

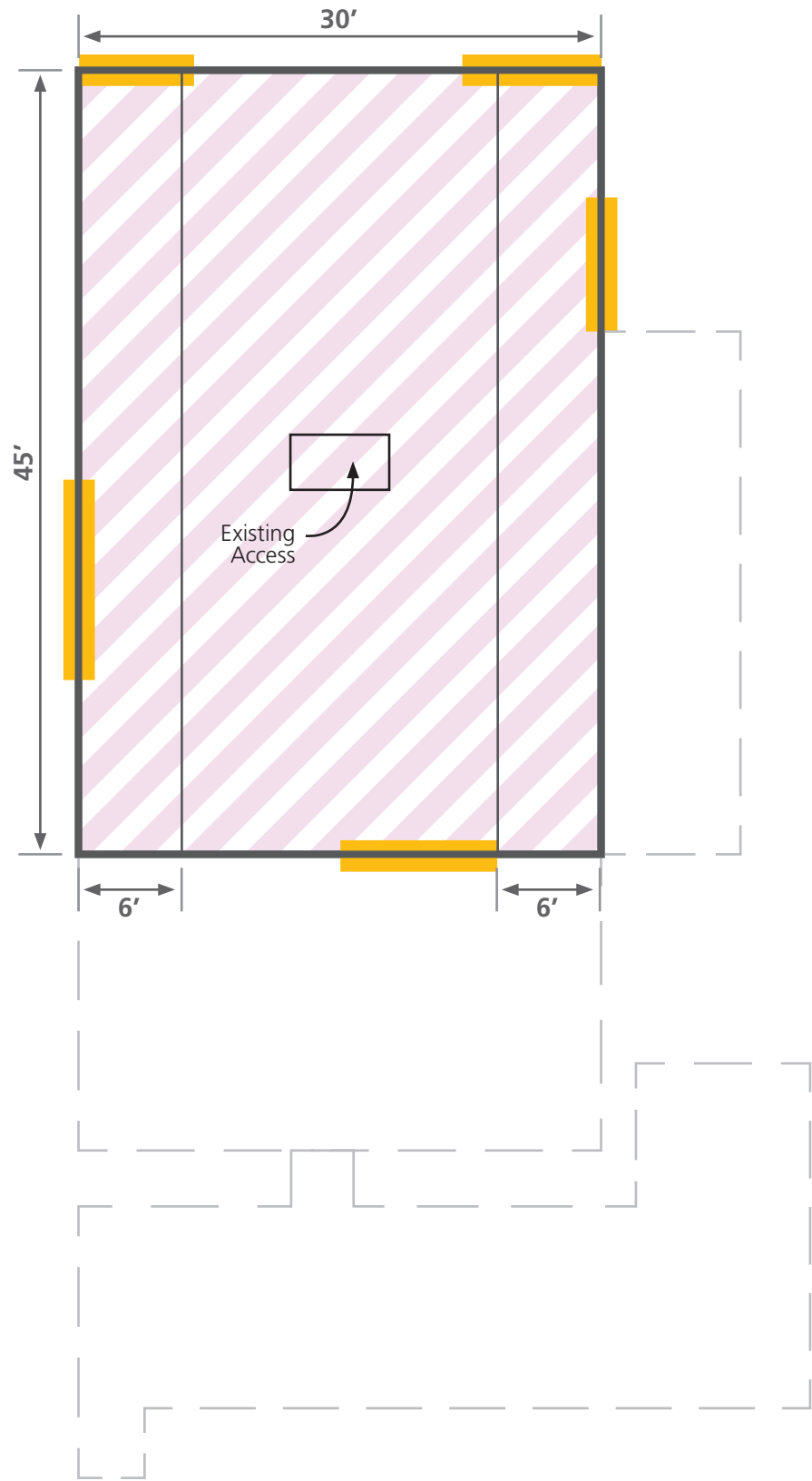


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



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Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.

September 10, 2018
File No. 262018.077

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
275 Fifth Street, Suite 100
Bremerton, WA 98337

Subject: Port of Silverdale Old Town Pub
Structural Evaluation

Dear Mr. Easterday:

We understand that the Port of Silverdale owns a building, known as the Old Town Pub, located near the City of Silverdale waterfront. The Port of Silverdale is considering renovating the Old Town Pub to return it to an occupiable condition. A limited gravity evaluation and a seismic evaluation of the existing building was performed in accordance with the ASCE 41-13 Tier 1 procedure to identify potential seismic deficiencies in the building and recommend concept-level seismic upgrades to mitigate the deficiencies.

Background

The Old Town Pub is an approximately 5,600-square-foot, three-story building located near the City of Silverdale waterfront. The building, originally constructed in the early 1900s as a Stable and Hall, has been modified several times over the life of the structure, including additions and revised layouts. Most recently, the first floor was occupied as a pub, while the second and third floors had an apartment space. The building is currently unoccupied.

As-built drawings for the building are not available. Information pertaining to the construction of the building and foundation system was obtained through on-site investigation. The wood-framed rectangular building is approximately 30 feet by 60 feet in plan, with story heights of approximately 9 feet and the roof peak creating a floor to roof height at the third level of approximately 12 feet. The exterior perimeter walls of the building are wood studs with 1x shiplap members. These compose most of the vertical- and lateral-force-resisting systems of the building.

Roof framing is composed of wood trusses that span the width of the building and a shiplap diaphragm. The bottom chords of the wood trusses compose the framing that supports the third floor. The second-floor diaphragm is composed of shiplap decking and is supported by wood exterior walls and a line of wood beams and columns at the interior. The floor is constructed of plywood over tongue-and-groove decking, supported by wood framing bearing on asphalt and dirt.

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ASCE 41-13 Seismic Evaluation Criteria

The current standard for seismic evaluation and retrofit of existing buildings is the ASCE 41-13, Seismic Evaluation and Retrofit of Existing Buildings. ASCE 41-13 provides screening and evaluation procedures used to identify potential seismic deficiencies that may require further investigation or hazard mitigation. It presents a three-tiered review process implemented by following a series of predefined checklists and “quick check” structural calculations. Each successive tier is designed to perform an increasingly refined evaluation procedure for seismic deficiencies identified in previous tiers in the process.

The Tier 1 checklists in ASCE 41-13 are specific to each common building type and contain seismic evaluation statements based on observed structural damage in past earthquakes. These checklists screen for potential seismic deficiencies by examining the lateral-force-resisting systems and details of construction that have historically caused poor seismic performance in similar buildings. Tier 1 screenings include basic “Quick Check” analyses for primary components of the lateral system: in this building’s case, the shear walls and wall anchorage. They also include prescriptive checks for proper seismic detailing of connections, diaphragm spans and continuity, and overall system configuration. Use of ASCE 41-13 for seismic evaluation requires buildings be classified from a group of common building types. The Old Town Pub is classified as a Wood Frame, Commercial and Industrial Building (W2), and was checked for Life Safety criteria. The ASCE 41-13 Tier 1 Preliminary Seismic Evaluation structural checklist was completed and is included for reference.

Findings

The findings of the structural seismic evaluation and limited gravity framing check indicate that The Old Town Pub has multiple deficiencies, including overstressed shear walls, irregularities and redundancy issues with the lateral-force-resisting system, and inadequate connections. In addition, the building has extensive settlement and insufficient support for gravity loads.

Multiple deficiencies associated with the general building system and configuration include an incomplete load path, weak story, vertical irregularities, and ratios causing overturning. Vertical irregularities and an incomplete lateral load path increase the forces in the supporting elements and require the supporting element to transfer lateral forces to surrounding systems. These deficiencies primarily occur at the northern face of the building.

A weak story is created between the first and second floors where the length of the walls of the seismic system decrease: the shear walls going east-west at the first floor have approximately 50 percent less capacity than those at the second floor. A weak story may result in partial collapse of the structure. In addition, the deck on the southern side of the

building does not have an approved lateral system, adequate detailing, and had limited connections at gravity system.

The wood walls that compose both the gravity system and lateral-force-resisting system have multiple seismic deficiencies, including redundancy, shear stress, and narrow walls. There should be a minimum of two shear walls in each direction. However, the walls at the northern side of the first floor do not meet the length-to-height ratios to be considered shear walls; therefore, a line of walls is not present at the northern face of the building at the first floor. Redundancy is typically desired for seismic performance to provide additional support in case another element of the lateral system fails. In addition, multiple walls on the northern face of the building are narrow, meaning they have an aspect ratio greater than 2-to-1. Narrow wood shear walls have high stresses, which impact the ability to provide adequate seismic support. A majority of the wood walls do not meet the shear stress check, which means that the overall strength of the building to resist seismic forces may be compromised.

There are inadequate connections throughout the building, including the connection of walls through floors, wood sills and sill bolts, girder and column connections, connections at wood posts, and ties between foundation elements. With the exception of toe nailing, no connections were observed. The connection between the walls of the first and second floors could not be observed. Based on the building's age, it is anticipated that straps and hold-downs are not present to allow for a complete load path. Connections were not observed at the wood sills of the shear walls; sill bolts are required to transfer lateral loads to foundation elements.

The span of all the floor diaphragms exceeds the 24-foot recommended span due to the open floor space with no interior shear walls. The diaphragms at the roof and all of the floors of the three-story building are composed of decking and appear to be unblocked. Unblocked diaphragms and diaphragms composed of decking have limited capacities. Diaphragms with sheathing and those that are blocked at panel edges have more strength to transfer lateral forces than those that are composed of decking and are unblocked at panel edges.

Extensive settling of the building was observed through visual observation and the measurement of sloping floors. Most settling appears to be occurring at the exterior sides. At both the first and second floors, the floor sloped away from the center of the building to all four exterior walls. The maximum slope measurement taken on site was approximately 1/2-inch per foot. This occurred at both the first and second floor in the northwest portion of the building. The settling appears to be due to an inadequate foundation system. The building does not appear to have an adequate concrete foundation system, and it is anticipated that the building does not sit on piling, as is typical near most waterfront areas in the Pacific Northwest.

Limited gravity framing calculations were performed on the roof trusses, floor framing, and first floor interior columns and beams that run down the center of the building.

Calculations for these gravity framing elements indicate that they do not have sufficient capacity for current code-prescribed loads (2015 International Building Code). With current snow load requirements, the existing roof trusses can support a third-floor live load on the bottom chord ranging from 10 to 20 pounds-per-square foot (psf), depending on the grade of the wood. The capacity of the bottom chord controls the truss capacity. As a reference, a 40 psf live load is typical for residential dwellings. The floor framing also has limited capacity, with a similar capacity as the existing roof trusses. While the first floor columns have adequate capacity, the glulam beams running down the center of the first floor do not meet deflection requirements by five percent, assuming a live load of 40 psf.

The Old Town Pub does not meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, extensive damage of the lateral-force-resisting elements may occur. It is recommended that the building be upgraded to meet the Life Safety performance objective.

Conclusions and Recommendations

The attached figures display concept-level upgrade concepts to improve the gravity and lateral systems to meet the Life Safety performance objective. The upgrade concepts involve adding lateral strength to the building, strengthening the roof and floor diaphragms and connections, upgrading the foundation system, upgrading gravity framing, and limiting live loads and uses at portions of the building.

Specified exterior walls should be resheathed with structural sheathing, which will strengthen the existing shear walls and increase shear capacity of the LFRS. Interior wood shear walls with structural sheathings should be added to decrease the diaphragm span. Hold-downs should be added to all walls and between walls at floor levels to strengthen and stiffen the building. By stacking shear walls, multiple deficiencies noted for the general building system and configuration will be alleviated.

The live load capacity of the third floor should be limited unless the trusses are upgraded. A second line of column and beams should be added at the first floor to decrease the load on the existing beams. The load on the second floor should be limited until the floor system is upgraded. Upgrades include adding additional floor framing members and adding additional columns and glulam beams down the center. As an alternate option to additional glulam beams and columns, the framing could be replaced with steel, which would allow for longer beam spans and thus fewer interior columns. The deck at the southern side of the building should be demolished. A new means of egress, or a deck with an adequate lateral system, should be provided.

The floor systems should be resheathed with structural sheathing and nailing, and blocking should be added to increase the diaphragms' capacity and strength. The connection between the diaphragms and shear walls should be improved using wall ties to ensure that forces are transferred to the shear walls. Connections should be added at a

Mr. Ron Easterday
Rice Fergus Miller Architecture & Planning
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new foundation system. A concrete foundation system, consisting of spread footings under all gravity and lateral systems and a concrete slab, should be installed. It is also anticipated that pile foundations may be required. Prior to installation of a foundation system, the existing gravity and lateral systems should be leveled.

With both seismic and lateral upgrades, the Old Town Pub may be upgraded to meet the Life Safety performance level as determined by the ASCE 41-13 Tier 1 evaluation. During a design-level earthquake, upgrades would limit the damage of the lateral-force-resisting elements and gravity system.

Thank you for allowing us the opportunity to assist you with this project. If you need any clarification or additional information, please call.

Sincerely,

Reid Middleton, Inc.









Corbin M. Hammer, P.E., S.E.
Principal Structural Engineer

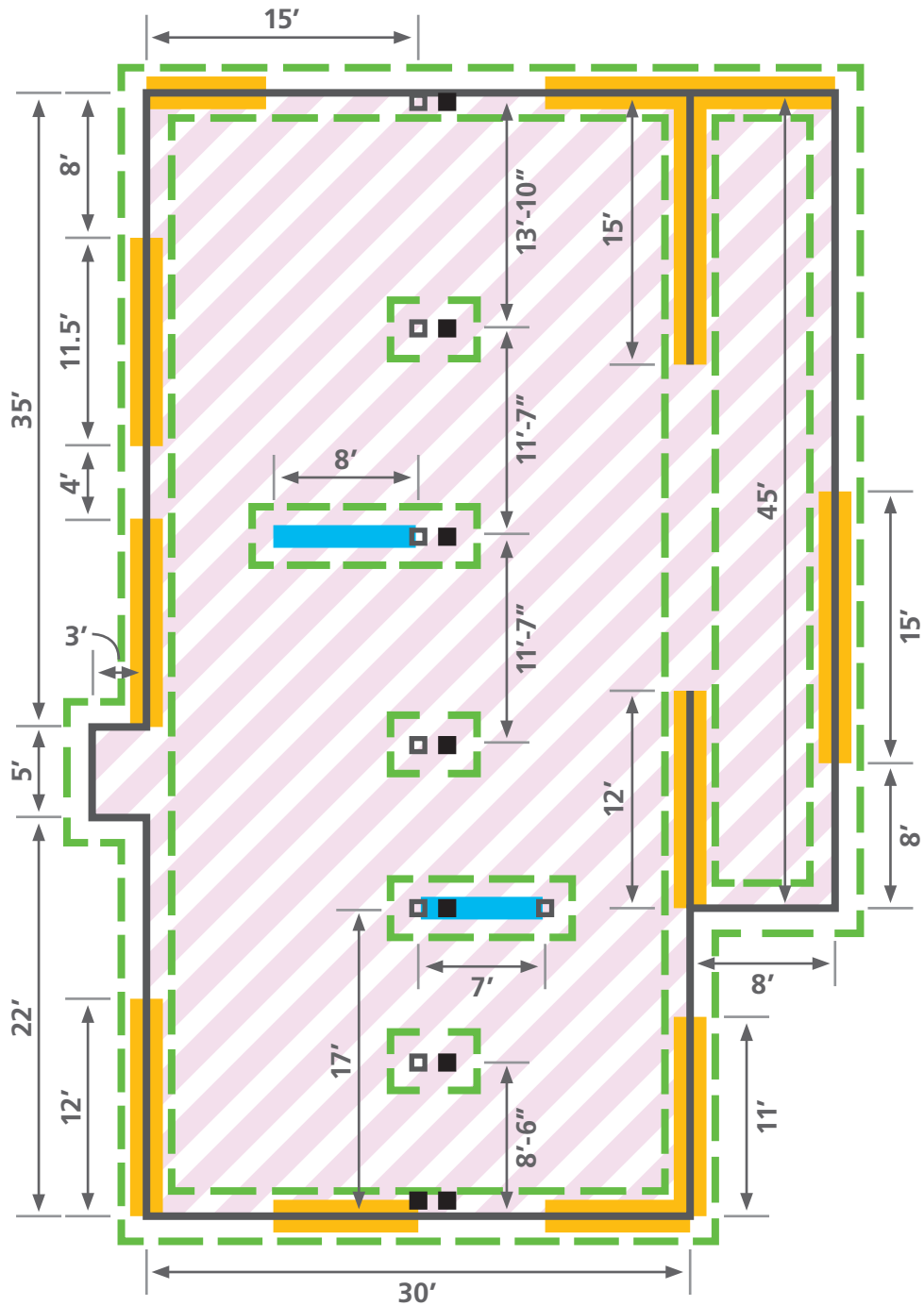


Katherine R. Brawner, P.E.
Project Engineer

Attachments

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-  Upgrade (E) Shear Wall
-  New Wood Shear Wall
-  Remove (E) Wood Floor to Allow for Foundation Improvements, Replace w/ Concrete Slab on Grade of Floor Framing at Owner's Option
-  Replace (E) Foundation System, Jack / Re-Level (E) Exterior Walls & Interior Columns Add Sill Bolts, Holdowns. Note Piles May Be Required. Additional Geotechnical Investigation is Required
-  (E) Wood Column, TYP.
-  New Wood Column, TYP.



NOTES

Dimensions are approximations and based upon site investigation.
 Dimensions are provided for informational purposes

Figure 1 - First Floor

-  Upgrade (E) Shear Wall. Provide Hold Down Connections Between Floors
-  New Wood Shear Wall
-  Upgrade Floor Diaphragm & Upgrade Floor Framing
-  Demolish Deck & Provide New Means of Egress as Required
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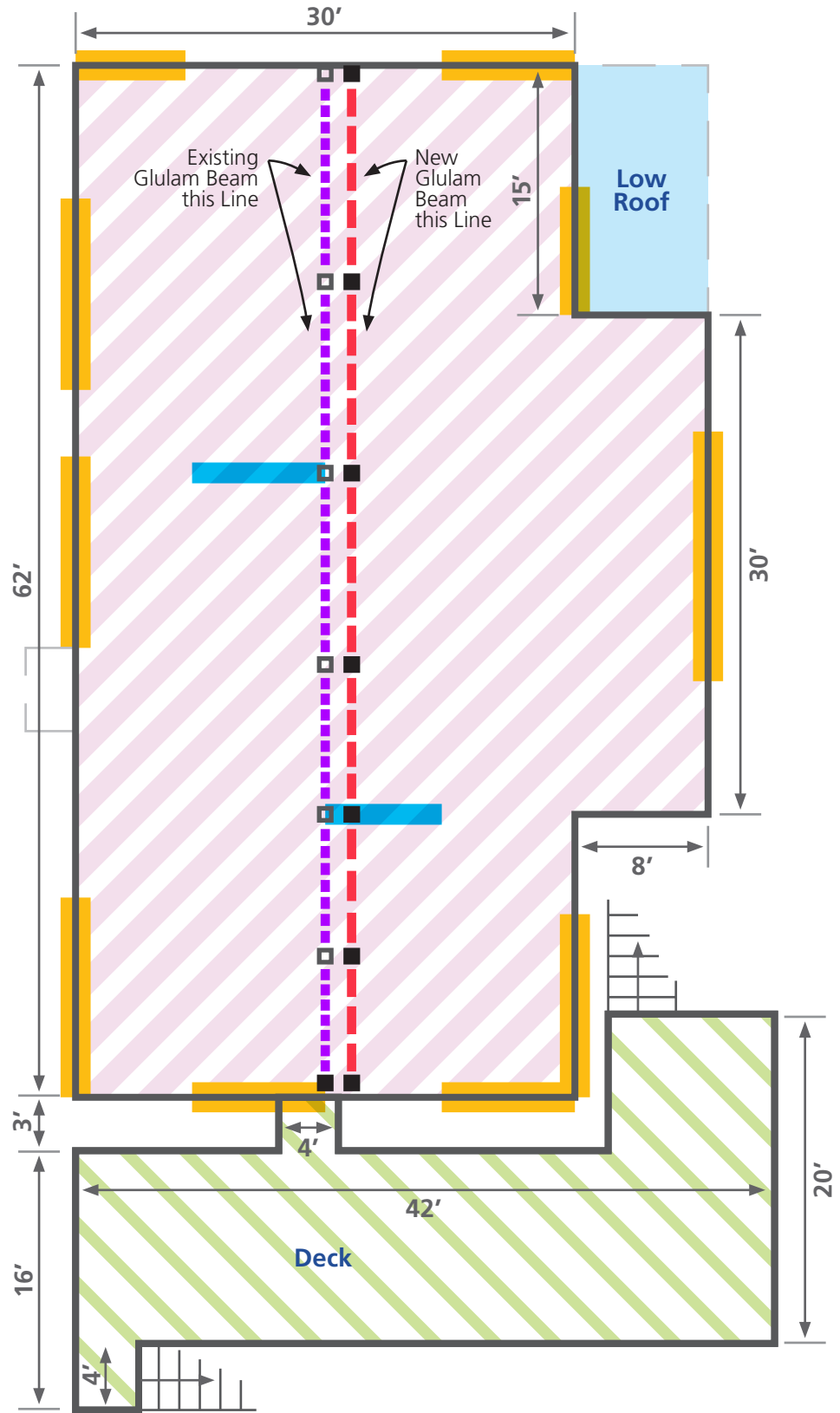




Figure 2 - Second Floor

-  Upgrade (E) Shear Wall
-  Attic Access Only Unless Bottom Chord of Truss is Upgraded

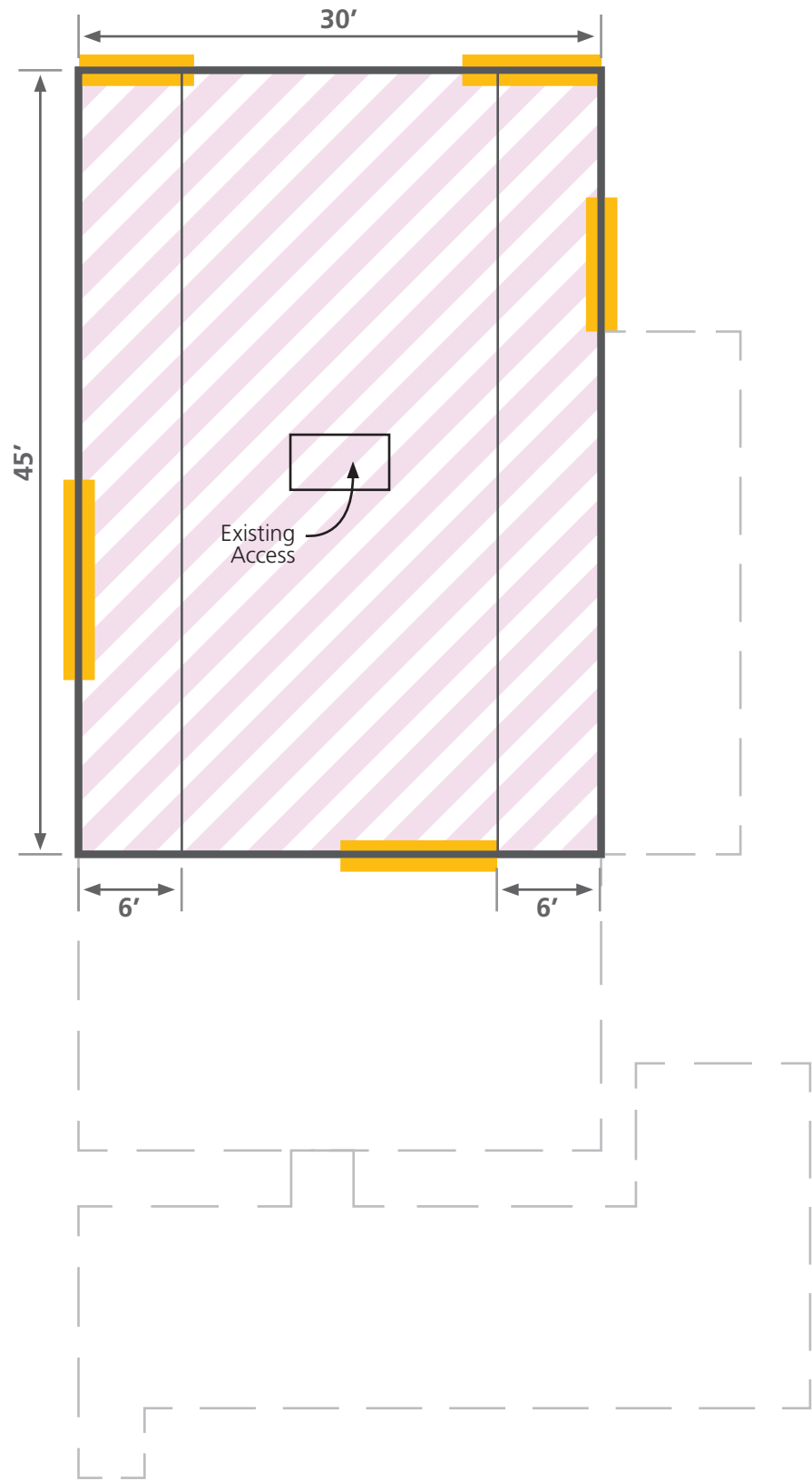


Figure 3 - Third Floor

Photographs



Photograph 1: Northern exterior of building.



Photograph 2: Northwestern exterior corner of building.



Photograph 3: Southwestern exterior corner of building.



Photograph 4: Deck at southern exterior of building.



Photograph 5: Deck at southeastern exterior corner of building.



Photograph 6: Northeastern exterior corner of building.



Photograph 7: Typical exterior shiplap.



Photograph 8: Underside of deck at southern side of building.



Photograph 9: Interior line of columns at first floor.



Photograph 10: Hole cut at column base at first floor.



Photograph 11: Underside of second floor with slope toward exterior of 12-inch per 1-foot.



Photograph 11: Typical floor and walls (second floor shown).



Photograph 12: Second floor at building center with high point (slope east and west).



Photograph 13: Typical floor and walls (third floor shown).



Photograph 14: Western interior with exposed truss top and bottom chords.



Photograph 15: Underside of first floor.



Photograph 16: Typical foundation system and underside of first floor.

16.1.2LS Life Safety Basic Configuration Checklist

The evaluation statements represent general configuration issues applicable for most buildings based on observed earthquake structural damage during actual earthquakes. This checklist should be completed for all buildings in Very Low, Low, Moderate, and High Seismicity for Life Safety Performance Levels.

Each of the evaluation statements in this checklist shall be marked Compliant (C), Noncompliant (NC), Unknown (U), or Not Applicable (N/A) for a Tier 1 screening. Compliant statements identify issues that are acceptable according to the criteria of this standard, whereas noncompliant and unknown statements identify issues that require further investigation. Certain statements may not apply to the building being evaluated. For noncompliant and unknown evaluation statements, the design professional may choose to conduct further investigation using the corresponding Tier 2 evaluation procedure; corresponding section numbers are in parentheses after each evaluation statement.

Very Low Seismicity

Building System

General

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			LOAD PATH: The structure shall contain a complete, well-defined load path, including structural elements and connections, that serves to transfer the inertial forces associated with the mass of all elements of the building to the foundation. (Commentary: Sec. A.2.1.1. Tier 2: Sec. 5.4.1.1)	
X				ADJACENT BUILDINGS: The clear distance between the building being evaluated and any adjacent building is greater than 4% of the height of the shorter building. This statement need not apply for the following building types: W1, W1a, and W2. (Commentary: Sec. A.2.1.2. Tier 2: Sec. 5.4.1.2)	
		X		MEZZANINES: Interior mezzanine levels are braced independently from the main structure or are anchored to the seismic-force-resisting elements of the main structure. (Commentary: Sec. A.2.1.3. Tier 2: Sec. 5.4.1.3)	

Building Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WEAK STORY: The sum of the shear strengths of the seismic-force-resisting system in any story in each direction shall not be less than 80% of the strength in the adjacent story above. (Commentary: Sec. A.2.2.2. Tier 2: Sec. 5.4.2.1)	The shear walls going east-west at the first floor have approximately 50% less capacity than those at the second floor.
X				SOFT STORY: The stiffness of the seismic-force-resisting system in any story shall not be less than 70% of the seismic-force-resisting system stiffness in an adjacent story above or less than 80% of the average seismic-force-resisting system stiffness of the three stories above. (Commentary: Sec. A.2.2.3. Tier 2: Sec. 5.4.2.2)	
	X			VERTICAL IRREGULARITIES: All vertical elements in the seismic-force-resisting system are continuous to the foundation. (Commentary: Sec. A.2.2.4. Tier 2: Sec. 5.4.2.3)	

16.1.2LS Life Safety Basic Configuration Checklist

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				GEOMETRY: There are no changes in the net horizontal dimension of the seismic-force-resisting system of more than 30% in a story relative to adjacent stories, excluding one-story penthouses and mezzanines. (Commentary: Sec. A.2.2.5. Tier 2: Sec. 5.4.2.4)	
X				MASS: There is no change in effective mass more than 50% from one story to the next. Light roofs, penthouses, and mezzanines need not be considered. (Commentary: Sec. A.2.2.6. Tier 2: Sec. 5.4.2.5)	
X				TORSION: The estimated distance between the story center of mass and the story center of rigidity is less than 20% of the building width in either plan dimension. (Commentary: Sec. A.2.2.7. Tier 2: Sec. 5.4.2.6)	

Low Seismicity: Complete the Following Items in Addition to the Items for Very Low Seismicity.

Geologic Site Hazards

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
			X	LIQUEFACTION: Liquefaction-susceptible, saturated, loose granular soils that could jeopardize the building's seismic performance shall not exist in the foundation soils at depths within 50 ft under the building. (Commentary: Sec. A.6.1.1. Tier 2: 5.4.3.1)	
X				SLOPE FAILURE: The building site is sufficiently remote from potential earthquake-induced slope failures or rockfalls to be unaffected by such failures or is capable of accommodating any predicted movements without failure. (Commentary: Sec. A.6.1.2. Tier 2: 5.4.3.1)	
			X	SURFACE FAULT RUPTURE: Surface fault rupture and surface displacement at the building site are not anticipated. (Commentary: Sec. A.6.1.3. Tier 2: 5.4.3.1)	

16.1.2LS Life Safety Basic Configuration Checklist

Moderate and High Seismicity: Complete the Following Items in Addition to the Items for Low Seismicity.

Foundation Configuration

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			OVERTURNING: The ratio of the least horizontal dimension of the seismic-force-resisting system at the foundation level to the building height (base/height) is greater than $0.6S_a$. (Commentary: Sec. A.6.2.1. Tier 2: Sec. 5.4.3.3)	
	X			TIES BETWEEN FOUNDATION ELEMENTS: The foundation has ties adequate to resist seismic forces where footings, piles, and piers are not restrained by beams, slabs, or soils classified as Site Class A, B, or C. (Commentary: Sec. A.6.2.2. Tier 2: Sec. 5.4.3.4)	No connections observed at the foundation.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

Low and Moderate Seismicity

Lateral Seismic-Force-Resisting System

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			REDUNDANCY: The number of lines of shear walls in each principal direction is greater than or equal to 2. (Commentary: Sec. A.3.2.1.1. Tier 2: Sec. 5.5.1.1)	Walls at the northern side of the first floor do not meet length/height ratios to be considered shear walls therefore not having a line of walls.
	X			SHEAR STRESS CHECK: The shear stress in the shear walls, calculated using the Quick Check procedure of Section 4.5.3.3, is less than the following values (Commentary: Sec. A.3.2.7.1. Tier 2: Sec. 5.5.3.1.1): Structural panel sheathing 1,000 lb/ft Diagonal sheathing 700 lb/ft Straight sheathing 100 lb/ft All other conditions 100 lb/ft	
		X		STUCCO (EXTERIOR PLASTER) SHEAR WALLS: Multi-story buildings do not rely on exterior stucco walls as the primary seismic-force-resisting system. (Commentary: Sec. A.3.2.7.2. Tier 2: Sec. 5.5.3.6.1)	
		X		GYPSON WALLBOARD OR PLASTER SHEAR WALLS: Interior plaster or gypsum wallboard is not used as shear walls on buildings more than one story high with the exception of the uppermost level of a multi-story building. (Commentary: Sec. A.3.2.7.3. Tier 2: Sec. 5.5.3.6.1)	Interior walls are not part of the lateral system
	X			NARROW WOOD SHEAR WALLS: Narrow wood shear walls with an aspect ratio greater than 2-to-1 are not used to resist seismic forces. (Commentary: Sec. A.3.2.7.4. Tier 2: Sec. 5.5.3.6.1)	
	X			WALLS CONNECTED THROUGH FLOORS: Shear walls have an interconnection between stories to transfer overturning and shear forces through the floor. (Commentary: Sec. A.3.2.7.5. Tier 2: Sec. 5.5.3.6.2)	No connection observed on site.
		X		HILLSIDE SITE: For structures that are taller on at least one side by more than one-half story because of a sloping site, all shear walls on the downhill slope have an aspect ratio less than 1-to-2. (Commentary: Sec. A.3.2.7.6. Tier 2: Sec. 5.5.3.6.3)	
		X		CRIPPLE WALLS: Cripple walls below first-floor-level shear walls are braced to the foundation with wood structural panels. (Commentary: Sec. A.3.2.7.7. Tier 2: Sec. 5.5.3.6.4)	

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
		X		OPENINGS: Walls with openings greater than 80% of the length are braced with wood structural panel shear walls with aspect ratios of not more than 1.5-to-1 or are supported by adjacent construction through positive ties capable of transferring the seismic forces. (Commentary: Sec. A.3.2.7.8. Tier 2: Sec. 5.5.3.6.5)	

Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD POSTS: There is a positive connection of wood posts to the foundation. (Commentary: Sec. A.5.3.3. Tier 2: Sec. 5.7.3.3)	No connection observed on site.
	X			WOOD SILLS: All wood sills are bolted to the foundation. (Commentary: Sec. A.5.3.4. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.
	X			GIRDER/COLUMN CONNECTION: There is a positive connection using plates, connection hardware, or straps between the girder and the column support. (Commentary: Sec. A.5.4.1. Tier 2: Sec. 5.7.4.1)	No connections beyond toe nailing observed on site.

16.3LS LIFE SAFETY STRUCTURAL CHECKLIST FOR BUILDING TYPE W2: WOOD FRAMES, COMMERCIAL AND INDUSTRIAL

High Seismicity: Complete the Following Items in Addition to the Items for Low and Moderate Seismicity.

Diaphragms

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
X				DIAPHRAGM CONTINUITY: The diaphragms are not composed of split-level floors and do not have expansion joints. (Commentary: Sec. A.4.1.1. Tier 2: Sec. 5.6.1.1)	
X				ROOF CHORD CONTINUITY: All chord elements are continuous, regardless of changes in roof elevation. (Commentary: Sec. A.4.1.3. Tier 2: Sec. 5.6.1.1)	
		X		DIAPHRAGM REINFORCEMENT AT OPENINGS: There is reinforcing around all diaphragm openings larger than 50% of the building width in either major plan dimension. (Commentary: Sec. A.4.1.8. Tier 2: Sec. 5.6.1.5)	No diaphragm openings larger than 50% of width.
		X		STRAIGHT SHEATHING: All straight sheathed diaphragms have aspect ratios less than 2-to-1 in the direction being considered. (Commentary: Sec. A.4.2.1. Tier 2: Sec. 5.6.2)	Diaphragms do not have sheathing.
	X			SPANS: All wood diaphragms with spans greater than 24 ft consist of wood structural panels or diagonal sheathing. Wood commercial and industrial buildings may have rod-braced systems. (Commentary: Sec. A.4.2.2. Tier 2: Sec. 5.6.2)	Span exceeds 24 feet in both directions.
		X		DIAGONALLY SHEATHED AND UNBLOCKED DIAPHRAGMS: All diagonally sheathed or unblocked wood structural panel diaphragms have horizontal spans less than 40 ft and aspect ratios less than or equal to 4-to-1. (Commentary: Sec. A.4.2.3. Tier 2: Sec. 5.6.2)	Diaphragms are not composed of sheathing of structural panels.
X				OTHER DIAPHRAGMS: The diaphragm does not consist of a system other than wood, metal deck, concrete, or horizontal bracing. (Commentary: Sec. A.4.7.1. Tier 2: Sec. 5.6.5)	

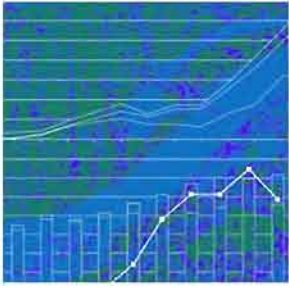
Connections

C	NC	N/A	U	EVALUATION STATEMENT	COMMENT
	X			WOOD SILL BOLTS: Sill bolts are spaced at 6 ft or less, with proper edge and end distance provided for wood and concrete. (Commentary: Sec. A.5.3.7. Tier 2: Sec. 5.7.3.3)	No sill bolts observed on site.



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

APPENDIX G: SOCIO-ECONOMIC ANALYSIS



Port of Silverdale Comprehensive Plan

Socio-Economic Analysis

Draft

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Port of
Silverdale

Chapter One - Overview

Introduction

BST Associates was retained by the Port of Silverdale to assist in the development of a comprehensive plan. One of the primary roles of BST in this effort was to develop background information on the economy and demographics of the Silverdale area, and then to help determine the role that the Port of Silverdale can play in the community. The following document presents the results of this analysis.

The document is arranged into three sections:

- Chapter 2 provides background into Silverdale economy, including population trends and forecasts, and economic trends and forecasts.
- Chapter 3 provides analysis of the supply of and demand for different types of commercial property in the Silverdale area, including industrial land and industrial building space, commercial office space, and retail space.
- Chapter 4 provides an analysis of the recreational boating market in Kitsap County, focusing on the potential for the Port of Silverdale to invest in expanded boating facilities.

Initial Findings

Demographics

Between 1980 and 2000 the population of the Silverdale area more than doubled, climbing from 21,000 to nearly 49,000 and by 2020 the population is expected to exceed 62,000.

Government is currently the biggest source of jobs in Kitsap County, due mainly to the presence of the Navy. However, forecasts indicate that employment in the retail and services sectors is expected to grow much faster.

- In 1980 government and education accounted for 38.8% of all jobs in Kitsap County, while retail and services accounted for a total 34.4% of jobs. By 2000 government/education employment had fallen to 33.4%, while retail and services grew to 48.2%, or nearly half of all jobs.
- The number of people employed in government jobs is projected to continue growing through 2020 at an annual average rate of 0.5% to 1.0% per year, but these jobs will account for a declining share of total county employment. In contrast, retail employment is projected to grow between 1.0% and 1.4% per year on average, and service sector employment between 0.6% and 1.8% per year on average.
- The share of Kitsap County retail and service jobs that are located in the Silverdale area is expected to grow from 24.3% in 2000 to 25.2% by 2020.
- Between 1998 and 2005 total taxable retail sales in Kitsap County jumped from approximately \$1.0 billion to approximately \$1.7 billion, with most of the increase occurring in unincorporated parts of the county (note: Silverdale is unincorporated).
- Manufacturing is an important source of jobs in Kitsap County, but not in the Silverdale area. The sector is also expected to see little growth by the year 2020.

Unemployment in Kitsap County has tended to be less severe than in the rest of the state due to the high percentage of government (i.e. Navy) jobs in the county. Over the past 17 years the highest rates of unemployment in Kitsap County occurred between 1993 through 1995 (i.e. 6.2%), and between 2002 and 2003 (i.e. 7.0%). However, in 2006 the unemployment rate in Kitsap County was at its lowest point in 17 years, 4.7%

Per capita income in Kitsap County has lagged behind the state for much of the past three decades, but in recent years the gap has narrowed significantly. In both Kitsap County and in Washington the share of income that comes from wages and salaries has decreased while an increasing share has been generated by transfer payments (e.g. Navy pensions, Social Security) and dividends, interest, and rent. This underscores the importance of the growth in the senior population in both Kitsap County and Washington.

Commercial & Industrial Development

Vacancy rates for office space in Kitsap County, especially in the Silverdale area, have been declining for most of the past decade, although there has recently been an uptick in office vacancy. The demand for commercial space in Silverdale is strong, especially for office space to serve the fast-growing services industry. Employment growth projections for the Silverdale area indicate that as much as 575,000 square feet of additional office space may be needed between 2007 and 2020. The office market presents an attractive opportunity for the Port of Silverdale's Old Town properties.

Through most of the last two decades the vacancy rate for retail space in the Silverdale area has been very low. Construction of the Kitsap Mall in 1985 has led to Silverdale becoming the retail hub of Kitsap County. Retail employment is one of the fastest growing sectors of the Silverdale economy, and the projected job growth will lead to demand for an estimated 265,000 to 600,000 square feet of additional retail space between 2007 and 2020. Mixed use redevelopment of Port property in the Old Town area is likely to see demand from the retail sector as well as from the services sector.

Industrial development is a lower priority for the Port of Silverdale. Kitsap County currently has a large supply of vacant and underutilized industrial land, including 433 acres in unincorporated areas. Of this vacant industrial land, 159 acres is in the Silverdale UGA, along with 22 acres of underutilized industrial space. One factor that may impact the supply are a lack of sewers in the area, limiting the types of industrial uses on industrial property.

Despite the large supply of land, vacancy rates for developed industrial buildings in North Kitsap generally average between 4.5% and 7.5%. Based on historical patterns, the PSRC forecasts little growth in manufacturing employment. While there might be a role for the port to try to attract industrial jobs by developing industrial buildings because the private sector is not willing to, the reluctance of the private developers indicates that this type of development may present a higher risk to the Port than commercial development in Old Town.

Recreational Boating

The Port of Silverdale operates a short-stay recreational dock but no long-term marina or marine fuel facility. This dock is popular for day trips and overnight stays during the boating season, which generally runs from Memorial Day through Labor Day. This dock is an important asset, and may be useful in attracting customers to existing and new business in a redeveloped Old Town. The Port should continue to maintain and promote this facility.

On the other hand, there is little need for the Port to pursue the construction of a permanent-moorage marina. While the demand for moorage in Kitsap County may grow by 50 slips per year over the next decade, the new downtown Bremerton marina and the reconstructed Seabeck marina will likely provide enough new moorage to satisfy this demand. There area also a number of other marina projects in various stages of feasibility analysis in the area.

In addition to the amount of competition in the moorage market, a new marina in Silverdale might prove to be financially risky for the Port of Silverdale. Moorage rates in Kitsap County are relatively low, while the construction costs for new marinas have been escalating rapidly.

A marine fueling facility is also likely to prove financially risky. The short boating season (Memorial Day to Labor Day) means that a marine fuel facility must generate sufficient sales over a four month period to cover 12 months worth of operation & maintenance costs as well as financing costs. In

addition, much of the boating activity in the area involves trailerable boats, which are able to fuel at gas stations that can charge a lower price than a marine fuel facility.

Port of
Silverdale.

Chapter Two – Socio-Economic Trends

An understanding of the development trends and characteristics of Kitsap County and the Silverdale area is critical to determining the context and opportunities for development at the Port of Silverdale. The following chapter evaluates several key variables that identify the development trends in the area, including:

- Population
- Employment
- Unemployment
- Income
- Retail Sales

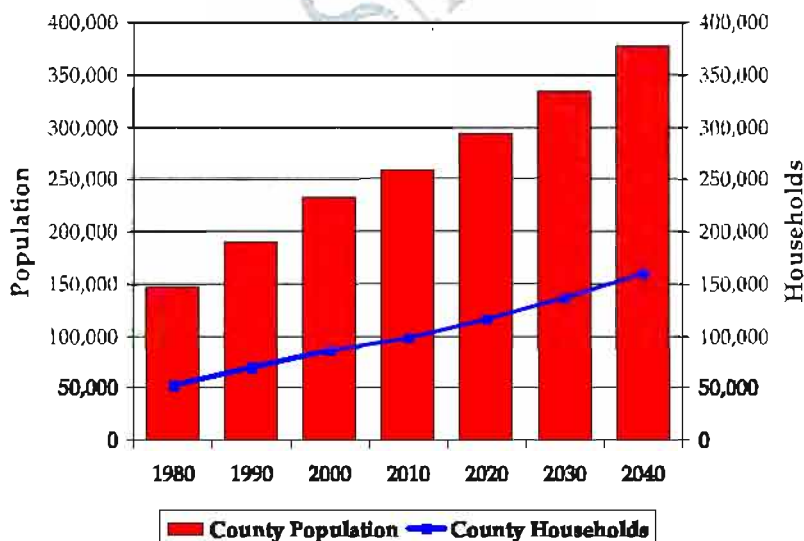
Population and Housing Trends

County Total

According to forecasts from the Puget Sound Regional Council (PSRC), between 1980 and 2000 the population of Kitsap County grew by an average of more than 4,100 residents per year. After 2000 the rate of growth slowed, with an average of slightly less than 2,600 new residents per year, but from 2010 through 2020 growth is expected to accelerate.

The number of households in Kitsap County has grown slightly faster than the population, a trend which is expected to continue through 2020. The result of the difference in the growth rates of housing and population means that the average household size has dropped from 2.68 persons per household in 1980 to approximately 2.57 in 2007. The decline is projected to continue, with average household size dropping to 2.55 in 2010 and to 2.46 in 2020.

Figure 1 – Population and Households Forecast for Kitsap County



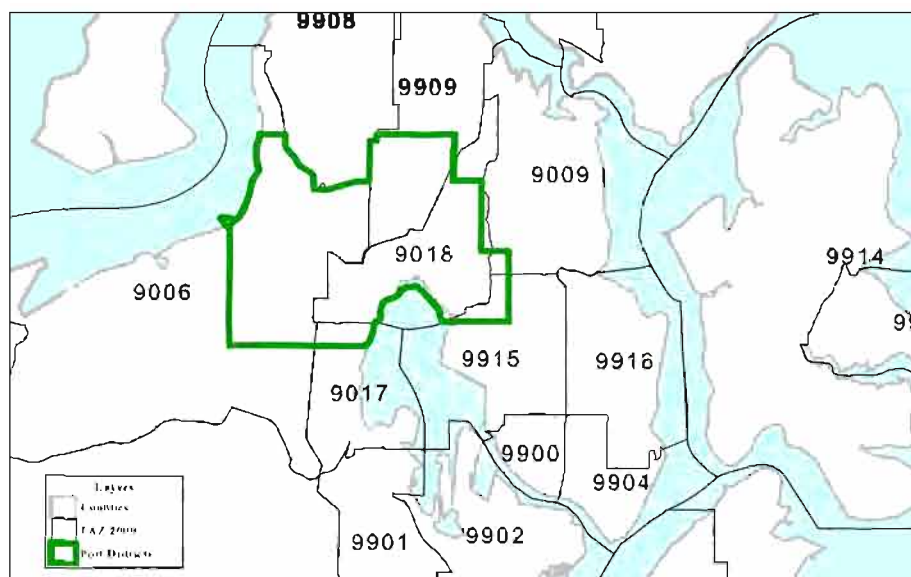
Source: PSRC

County Sub-Area

The Puget Sound Regional Council (PSRC) produces economic and demographic forecasts at the sub-county level. These forecasts divide counties into Forecast Analysis Zones, or “FAZs”, and provide the best source of information on the Silverdale area.

As shown in Figure 2, the Port of Silverdale, represented by the area shaded in green, encompasses most of FAZ 9018 as well as parts of several others.

Figure 2 – FAZ and Port District Map of Silverdale



Source: PSRC

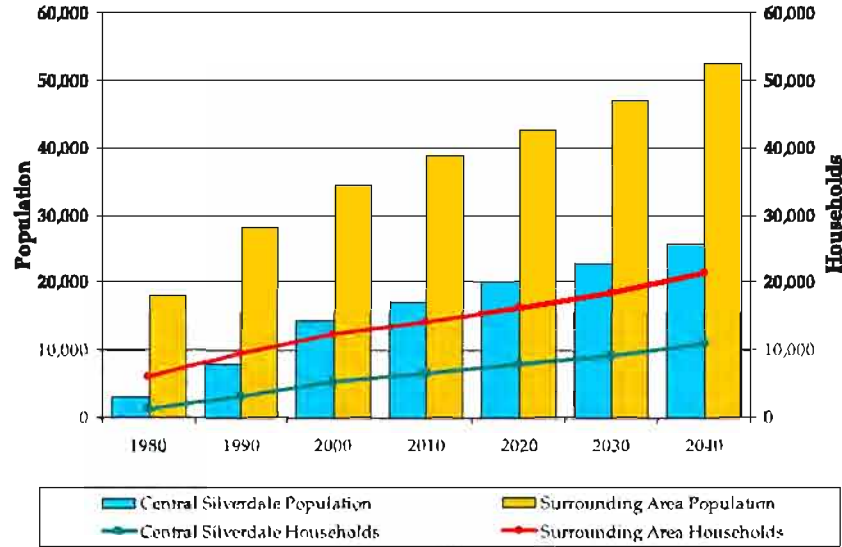
According to historical trends and forecasts from the PSRC, the central Silverdale area has experienced faster population growth than the surrounding area for more than two decades, and this pattern is expected to continue through the year 2020.

The combined population of central Silverdale (FAZ 9018) and the other FAZs that are partially in the Port of Silverdale district grew from slightly more than 21,000 in 1980 to nearly 49,000 in 2000. Between 1980 and 1990 the population grew by an average of 5.5% per year, and between 1990 and 2000 the population grew by an average of 3.1% per year.

The population of these surrounding FAZs is projected to continue growing through 2020, albeit at a much slower rate. Between 2000 and 2010 the annual growth rate is projected to average 1.3% per year, and from 2010 through 2020 is projected to average between 1.1% and 1.2% per year. The total population is projected to reach more than 62,500 in 2020.

In central Silverdale the population grew at an average annual rate of 10.6% between 1980 and 1990, as the number of residents jumped from fewer than 3,000 to more than 8,100. Between 1990 and 2000 the growth rate slowed to 6.0% per year. However, the number of residents grew by more in that decade (6,400) than during the previous decade (5,100).

**Figure 3 – Population and Households
Forecast for Silverdale & Surrounding FAZs**



Source: PSRC

From 2000 through 2020 the PSRC forecast average annual growth of 1.6% in central Silverdale, with the population growing to approximately 17,000 in 2010, and 20,000 in 2020, or by approximately 300 new residents per year in central Silverdale.

**Table 1 – Growth Rates and Central Silverdale Share of
Population and Households**

Year	Population				Households			
	Central Silverdale (FAZ 9018)	Surrounding Area	Total	Silverdale Share	Central Silverdale (FAZ 9018)	Surrounding Area	Total	Silverdale Share
1980	2,954	18,131	21,085	14.0%	1,111	6,100	7,211	15.4%
1990	8,111	28,036	36,147	22.4%	3,184	9,618	12,802	24.9%
2000	14,500	34,371	48,871	29.7%	5,451	12,354	17,805	30.6%
2010	17,024	38,686	55,710	30.6%	6,531	14,198	20,729	31.5%
2020	20,039	42,498	62,537	32.0%	7,968	16,194	24,162	33.0%
2030	22,642	46,963	69,605	32.5%	9,326	18,560	27,886	33.4%
2040	25,702	52,419	78,121	32.9%	10,959	21,467	32,426	33.8%
Average Annual Growth Rate								
1980-1990	10.6%	4.5%	5.5%		11.1%	4.7%	5.9%	
1990-2000	6.0%	2.1%	3.1%		5.5%	2.5%	3.4%	
2000-2010	1.6%	1.2%	1.3%		1.8%	1.4%	1.5%	
2010-2020	1.6%	0.9%	1.2%		2.0%	1.3%	1.5%	
2020-2030	1.2%	1.0%	1.1%		1.6%	1.4%	1.4%	
2030-2040	1.3%	1.1%	1.2%		1.6%	1.5%	1.5%	

Source: BST Associates, PSRC

Because of the faster growth in FAZ 9018 than in the surrounding FAZs, central Silverdale's share of the population grew from 14.0% in 1980 to nearly 30% in 2000, and is projected to reach 32% by 2020.

The growth in households has shown a pattern similar to that of population: central Silverdale has grown from a smaller base but at a much faster rate. As a result, the share of households in central Silverdale jumped from 15.4% in 1980 to 30.6% in 2000. Continued stronger growth in this FAZ when compared to the surround area will cause this share to continue to grow, reaching 33.0% by 2020.

Employment

Kitsap County

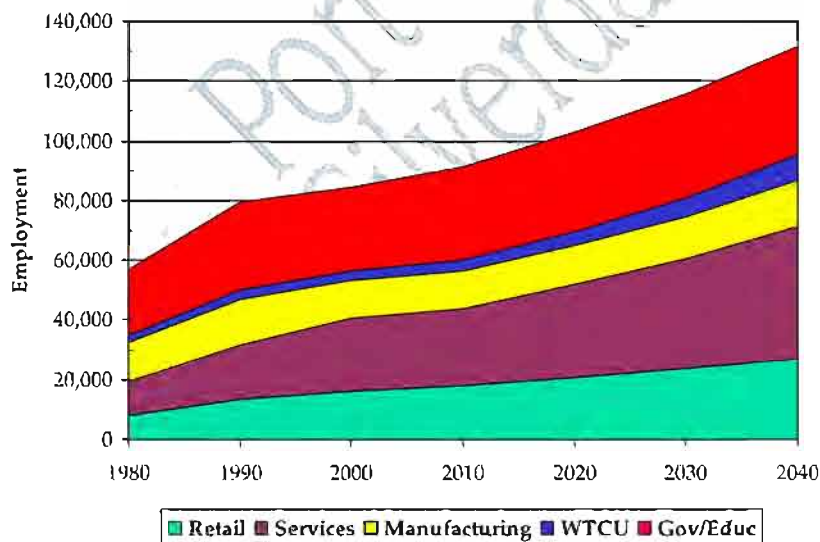
Government is currently the biggest source of jobs in Kitsap County, due mainly to the presence of the Navy. However, forecasts from the Puget Sound Regional Council indicate that employment in the retail and services sectors is expected to grow much faster.

In 1980 there were an estimated 22,000 jobs in government and education in Kitsap County, which accounted for 38.8% of all jobs. Retail accounted for 8,200 jobs and services 11,300 jobs; combined, these two sectors accounted for 34.4% of employment.

By 2000 government/education employment had grown to 28,300 jobs, while retail jumped to 16,500 and services to 24,300. In that year government's share of employment fell to 33.4%, while the share of jobs accounted for by retail and services grew to 48.2%, or nearly half of all jobs.

Government employment is projected to continue growing through the year 2020 at an annual average rate of 0.8% per year. In contrast, employment in both the retail sector and services sector are projected to grow by 1.2% per year on average through 2020.

Figure 4 – Kitsap County Employment Growth Forecast



Note: "WTCU" is wholesale trade, transportation, communications, and utilities
Source: PSRC

In terms of jobs, government employment is expected to grow by approximately 250 jobs per year through 2020, retail by 220, and services by 330 jobs per year. Most of the government job growth is expected to occur early in the forecast period, while employment in retail and services is expected to accelerate as time goes on.

Another sector that has seen strong growth is WTCU (wholesale trade, transportation, communications, and utilities), although total employment in the sector is low relative to the three major sectors. Employment in the WTCU sector increased from 2,000 jobs in 1980 to more than 3,200 jobs in 2000, representing average growth of 2.4% per year. Continued strong growth is projected by the PSRC,

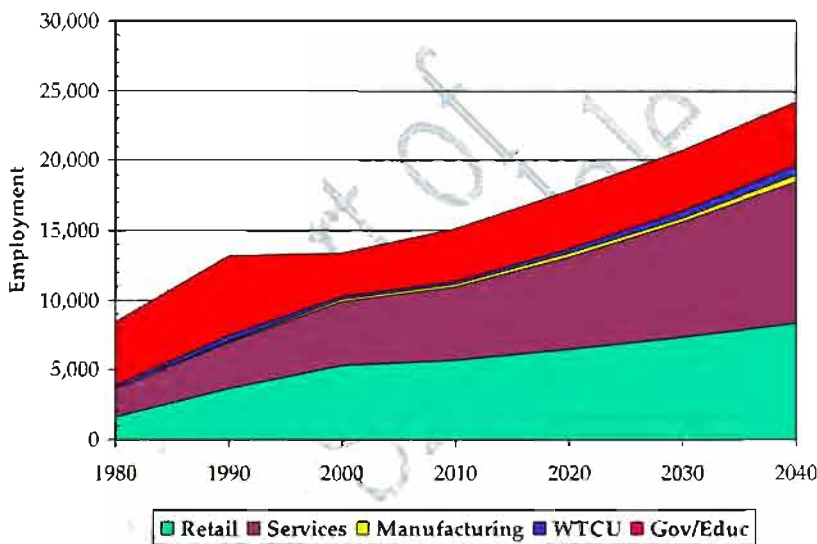
averaging nearly 2.0% per year through 2020, and with the number of jobs growing approximately 75 per year.

Manufacturing is an important source of jobs in Kitsap County, but does not appear to have much prospect for growth. According to figures from the PSRC, manufacturing employment growth has been non-existent in Kitsap County in recent years, although some recovery is projected. Between 1980 and 1990 the number of manufacturing jobs grew by 2,200, but between 1990 and 2000 they fell by 3,100. Growth in manufacturing in Kitsap County is projected to grow slowly through the current decade, and then accelerate somewhat through 2020.

Silverdale Area

The retail sector in the Silverdale area experienced tremendous growth between 1980 and 2000, largely due to the construction of Kitsap Mall and other nearby stores. The number of jobs in retail grew from less than 1,700 in 1980 to more than 5,300 in 2000. Total job growth for the Silverdale area was slightly more than 4,800 jobs during that period, and retail accounted for more than 3,600 of these, or more than 75%.

Figure 5 – Silverdale Area Employment Growth Forecast



Source: PSRC

The services sector also grew strongly between 1980 and 2000, adding more than 2,500 jobs. Employment in manufacturing and in wholesale trade, communications, and utilities remained essentially unchanged, while the number of jobs in government and education actually declined by 1,500.

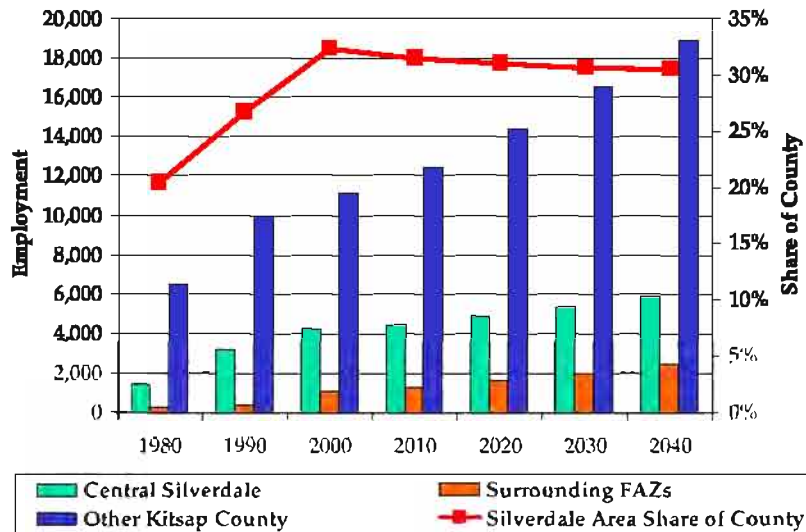
Each of these sectors is discussed in more detail below.

Employment in Retail

As described above, retail employment tripled between 1980 and 2000 due to the construction of the Kitsap Mall and other retail space. The share of total jobs accounted for by the retail sector jumped from 20% in 1980 to 32% in 2000

Most of the Silverdale-area retail jobs are currently located in the central Silverdale FAZ (FAZ 9018). PSRC forecasts indicate that from 2000 through 2020 retail employment growth will be split relatively evenly between central Silverdale and the surrounding FAZs, but central Silverdale is expected to continue to account for most of these jobs in 2020.

Figure 6 –Employment Growth Forecast - Retail



Source: PSRC

Retail job growth is not expected to increase at the rates seen between 1980 and 2000. An **average of 60 new retail jobs** area projected each year for the Silverdale area 2020, split relatively evenly **between central Silverdale and the surrounding FAZs**.

Other parts of Kitsap County are expected to grow at a slightly faster pace than the Silverdale area, which will cause Silverdale’s share of countywide retail employment to fall slowly through the **forecast period, from a high of 32.4% in 2000 to 31.0% in 2020**.

Growth in the retail sector is important to note in planning the future of the Port of Silverdale.

Employment in Service – (Finance, Insurance, Real Estate & Other Services)

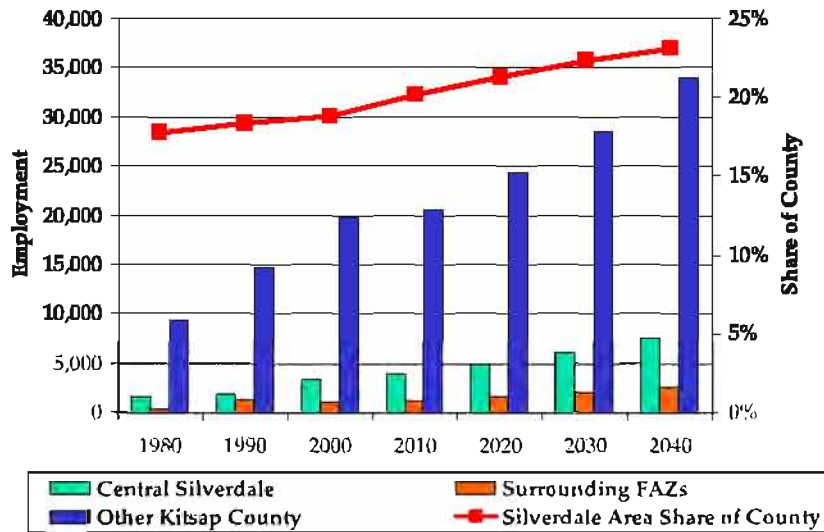
The **services sector (finance, insurance, real estate & other services)** has been the other major **source of job growth** in the Silverdale area. The share of all jobs in the area accounted for by the **retail and services sector** grew from 38.3% in 1980 to 51.1% in 2000, with retail accounting for 3,600 new **jobs and services 2,500**. From 2000 through 2020, however, employment in services is projected to grow **much faster than retail**.

PSRC projections show that between 2000 and 2020 the services sector in the Silverdale area **will add more than 2,000 new jobs**, in contrast to the 1,200 new retail jobs. Over the long run an **additional 100 service jobs** are projected to be added each year.

Most of the growth in the services sector is forecast to occur in FAZ 9018, central Silverdale. This FAZ had service employment of approximately 3,400 in 2000, while the surround FAZ’s had 1,100. Between 2000 and 2020 job growth is projected to average approximately 80 per year in central Silverdale and 24 in surrounding FAZs.

The share of Kitsap County service sector jobs that are located in the Silverdale area grew slowly between 1980 and 2000, but this growth in market share is projected to accelerate between 2000 and 2040. In 1980 the Silverdale area had less than 18% of county service jobs. This share climbed to nearly 19% in 2000, and is projected to exceed 21% by 2040.

Figure 7 –Employment Growth Forecast - Services



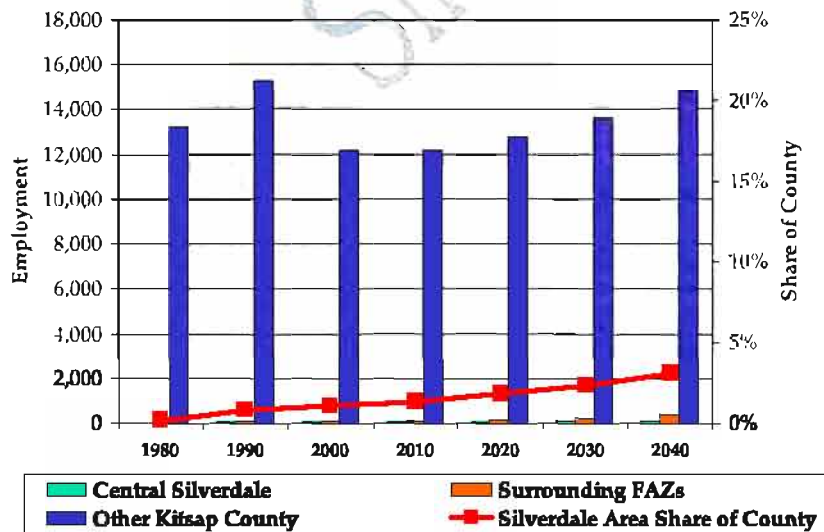
Source: PSRC

Increasing demand for office space created by the growth in the service sector has important implications for the Port of Silverdale, especially in regard to the Old Town property.

Employment in Manufacturing

Manufacturing accounts for very little employment in the Silverdale area, and this is not expected to change substantially between 2000 and 2020. In 2000 there were only 141 manufacturing jobs in the Silverdale area, accounting for only 1.1% of all manufacturing employment in the county.

Figure 8 –Employment Growth Forecast - Manufacturing



Source: PSRC

By 2020 employment in manufacturing in the Silverdale area is projected to grow to just 230 jobs. While this increase in jobs may represent a substantial rate of growth in the manufacturing sector, the additional 88 jobs account for only 3% of projected job growth in the area. By 2020 Silverdale will account for less than 2.0% of all manufacturing employment in the county.

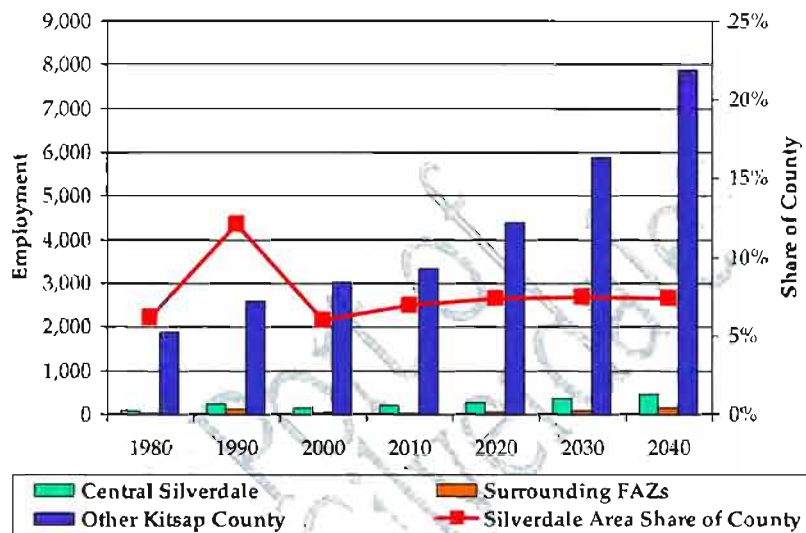
The weak growth in manufacturing indicates that this market presents a poor prospect for the Port of Silverdale.

Employment in Wholesale Trade, Communications, Utilities (WTCU)

Employment in wholesale trade, communications, and utilities (WTCU) is expected to grow steadily in Kitsap County through the year 2020, and the Silverdale share of this growth should remain steady. However, any growth in employment that occurs will be from a very low base, so the total number of jobs added will be small when compared with retail and services.

The Silverdale area had an estimated 193 WTCU jobs in 2000, and accounted for 7.0% of the county total. By 2020 the Silverdale area is projected to account for 7.4% of total county employment, with a total of 350 jobs.

Figure 9 –Employment Growth Forecast - WTCU



Note: "WTCU" is wholesale trade, transportation, communications, and utilities
Source: PSRC

While the Silverdale area has relatively little employment in the WTCU sector, to the extent that these jobs are located in office space similar to that of the service sector they may contribute to the demand for office space. This additional demand for office space is relevant to the Port of Silverdale.

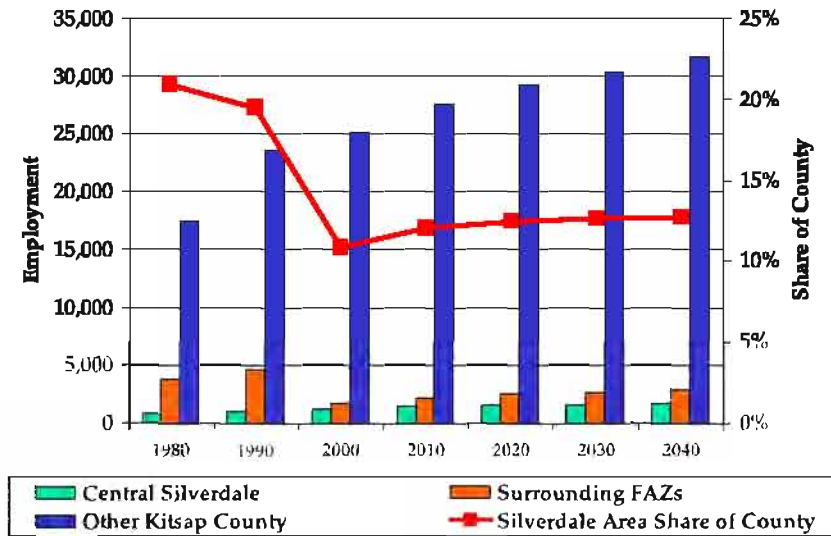
Employment in Government & Education

The Silverdale area experienced a sharp drop in government jobs between 1980 and 2000. This drop was especially pronounced between 1990 and 2000, when the FAZs surrounding central Silverdale lost nearly 3,000 jobs. At the same time government employment in central Silverdale (FAZ 9018) actually grew, from 783 jobs in 1980 to 1,314 jobs in 2000.

From 2000 through 2020 government employment in both central Silverdale and the surround FAZs is projected to grow slowly. Central Silverdale is expected to see an average of 14 new government/education jobs each, while the surrounding area is projected to add 40 new jobs per year.

As with jobs in WTCU, these new government jobs may create demand for additional office space, which in turn may provide additional market for the Port of Silverdale development in

Figure 10 –Employment Growth Forecast - Government & Education



Source: PSRC

Unemployment

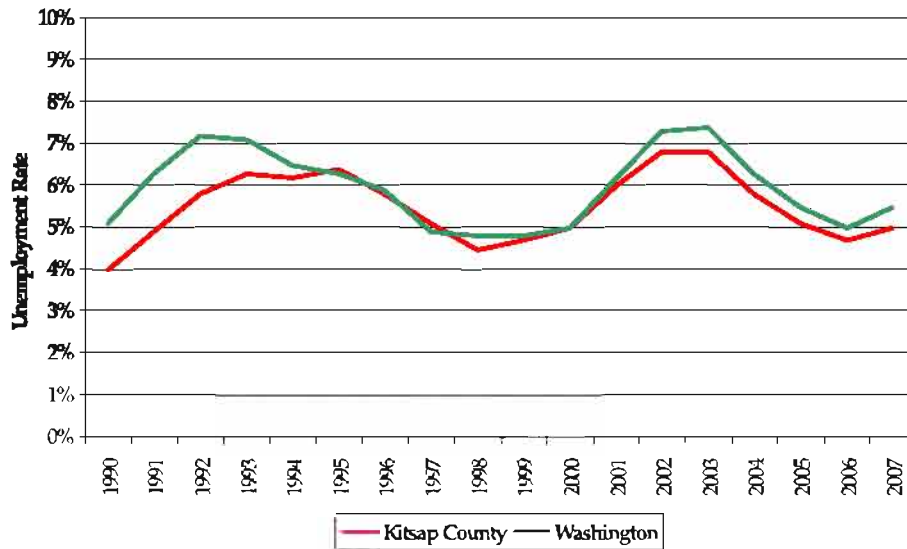
Unemployment in Kitsap County has tended to be less severe than in the rest of the state. Although the unemployment rate in both the state and Kitsap County tends to follow the same pattern, peak unemployment rates in Kitsap County tend to be 0.5 to 1.0% lower than in the rest of the state.

The lower unemployment rates in Kitsap County are likely due to the high percentage of government jobs in the county. These jobs, especially Navy jobs, tend to be insulated from economic downturns more than private-sector jobs.

Between 1990 and early 2007 the unemployment rate in Kitsap County peaked between 1993 through 1995 at approximately 6.2%, and between 2002 and 2003 at nearly 7.0%. In Washington state the peaks occurred from 1992 through 1993 (7.1%) and from 2002 through 2003 (7.4%).

In 2006 unemployment rates in both Kitsap County and Washington state were nearly at their lowest points in the last 17 years, with Kitsap County's rate at 4.7% and Washington's at 5.5%. Rates were last this low in 1990 and from 1997 through 2000.

Figure 11 –Unemployment History in Kitsap County and Washington State



Source: WA State Employment Security Dept

Income

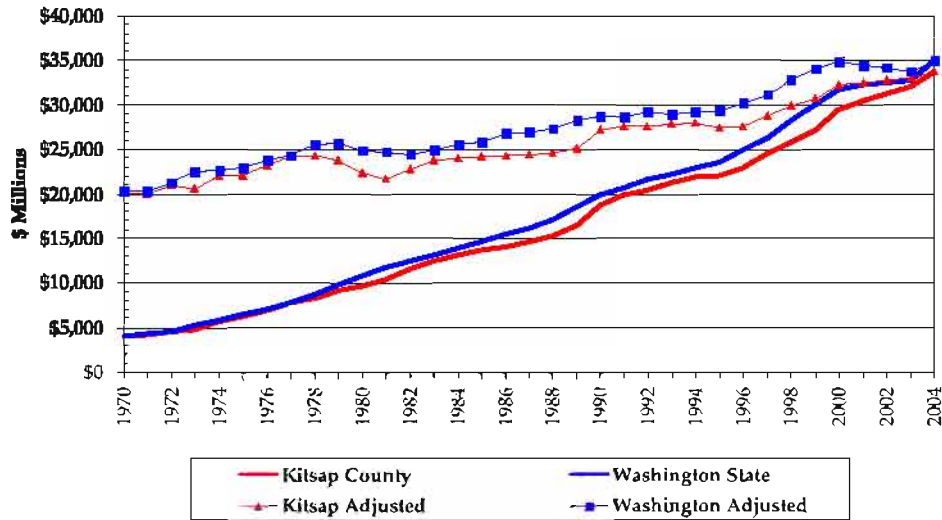
Kitsap County per capita income was the same as the state average in 1970 but lagged behind the state for much of the time since then. However, in recent years the gap narrowed significantly.

When adjusted for inflation (using a 2004 base year) both Kitsap County and state personal income grew steadily over the long term, with Kitsap County's income growing an average of 1.5% per year and Washington's by 1.6% per year. Both areas experienced declines in inflation-adjusted income in the late 1970's. Kitsap County also saw a small drop in 1995 and 1996 that did not also occur statewide, while the statewide average dropped in 2001, 2002 and 2003, a drop that was not seen in Kitsap County.

Kitsap County average personal income was an inflation-adjusted \$20,234 in 1970, nearly identical to the state average of \$20,404. By 1980 the statewide average was 10% higher than in Kitsap County, \$24,832 vs. \$22,466. By 1990 the difference had dropped to slightly more than 5% (\$28,711 vs. \$27,263, but by 2000 the difference had grown again, to nearly 8% (\$34,861 vs. \$32,353). However, the difference dropped quickly between 2000 and 2004, to just 3.5%. Average personal income in 2004 was \$22,865 in Kitsap County and \$35,041 in Washington.

Personal income comes primarily from three sources, namely wage and salary income, transfer payments (such as Navy pensions and Social Security), and dividends/interest/ rent. Kitsap County and the rest of the state have witnessed a decline in the share of income that comes from wages and salaries. In 1970, this source contributed nearly 78% to total personal income in Kitsap County and nearly 76% in Washington State. By 2004, however, wages and salaries generated slightly less than 71% of total personal income in Kitsap County and 69% in the state. The share of income contributed by dividends, interest, and rent climbed from 14.0% to 16.6% in Kitsap County, and from 14.1% to 17.9% in Washington. This is primarily a function of the growing number of retired residents in the county.

Figure 12 – Personal Income Trends



Source: Bureau of Economic Analysis

The increase in the share of personal income contributed by non-wage sources underscores the importance of the growth of the senior population both in Kitsap County and in Washington.

Table 2 – Source of Personal Income

Item	1970	1980	1990	2000	2004
Kitsap County					
Wage & Salary	77.8%	74.1%	71.6%	68.8%	70.7%
Dividends, interest, and rent	14.0%	16.3%	19.0%	19.8%	16.6%
Transfer payments	8.2%	9.7%	9.5%	11.4%	12.7%
Washington State					
Wage & Salary	75.8%	73.1%	68.3%	69.7%	68.9%
Dividends, interest, and rent	14.1%	16.0%	19.8%	18.4%	17.9%
Transfer payments	10.0%	10.9%	11.9%	11.9%	13.1%

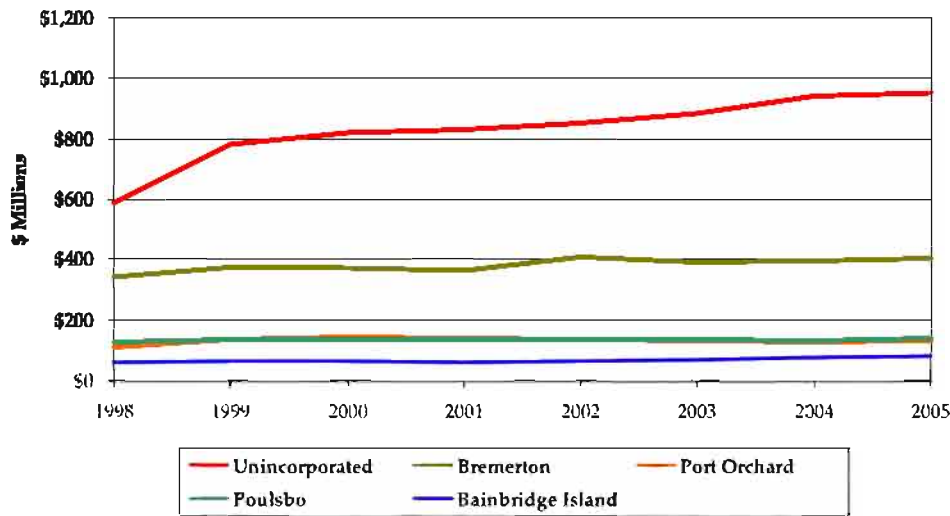
Source: BST Associates, Bureau of Economic Analysis

Retail Sales

Taxable retail sales (which include all retail items with the exception of non-taxable sales of food and drugs) are also a good indicator of the vitality of an area. In Kitsap County most taxable retail sales occur in unincorporated parts of the county, as illustrated in Figure 13.

When adjusted to 2005 dollars, between 1998 and 2005 total taxable retail sales in Kitsap County jumped from approximately \$1.2 billion to approximately \$1.7 billion. More than two-thirds of the increase in sales occurred in unincorporated parts of the county, including Silverdale.

Figure 13 – Taxable Retail Sales Trends



Source: WA State Department of Revenue

In Kitsap County taxable retail sales grew by an annual average rate of 4.7% from 1998 through 2005, which is a very strong growth rate and much faster than the population growth rate. In contrast, taxable retail sales in Washington state grew by an average of 2.0% per year during the same period.

The fastest growth in retail sales occurred in unincorporated parts of the county, where annual growth was 7.2% per year. Growth was slowest in Poulsbo, averaging 1.0% per year, while in both Bremerton and Port Orchard growth averaged 2.2% to 2.3% per year.

Table 3 – Taxable Retail Sales Trends
(\$ millions, adjusted to 2005 dollars)

Region	1998	2005	AAGR
Unincorporated	\$588	\$1,071	8.9%
Bremerton	\$402	\$451	1.7%
Port Orchard	\$134	\$154	2.0%
Poulsbo	\$150	\$157	0.7%
Bainbridge Island	\$76	\$96	3.4%
Kitsap County	\$1,418	\$1,891	4.2%
Washington	\$46,777	\$51,932	1.5%

Source: WA State Department of Revenue

General merchandise stores such as Wal-Mart account for the largest share of taxable retail sales in Kitsap County, but the rate of growth of these stores was less than the overall county average between 1998 and 2005. Total county taxable retail sales grew at an average annual rate of 10.0% per year, while sales at general merchandise stores grew by 7.7% per year. Because of the slower rate of growth for general merchandise stores, their share of total county taxable retail sales dropped from 47% in 1998 to slightly more than 40% in 2005.

In contrast, building material & gardening stores, such as Home Depot, accounted for the second largest share of sales, but these sales grew much faster than the county average. Sales growth at these stores averaged 16.1% per year from 1998 through 2005, and their share of county taxable retail sales jumped from 8.8% in 1998 to 15.4% in 2005.

**Table 4 – Taxable Retail Sales in Unincorporated Kitsap County
By Category of Business (\$ millions, adjusted to 2005 dollars)**

Category	1998	Share of Total	2005	Share of Total	AAGR
Motor Vehicle and Parts Dealers	\$34.4	5.3%	\$55.7	5.2%	7.1%
Furniture and Home Furnishings Stores	\$21.3	3.2%	\$20.4	1.9%	-0.6%
Electronics and Appliance Stores	\$32.1	4.9%	\$65.3	6.1%	10.7%
Building Material and Garden Equipment and Supplies Dealers	\$57.7	8.8%	\$164.4	15.4%	16.1%
Food and Beverage Stores	\$31.9	4.9%	\$40.8	3.8%	3.6%
Health and Personal Care Stores	\$12.1	1.8%	\$15.4	1.4%	3.6%
Gasoline Stations	\$10.9	1.7%	\$14.5	1.4%	4.1%
Clothing and Clothing Accessories Stores	\$36.8	5.6%	\$55.4	5.2%	6.0%
Sporting Goods, Hobby, Book, and Music Stores	\$35.1	5.3%	\$55.7	5.2%	6.8%
General Merchandise Stores	\$273.4	41.7%	\$383.3	35.8%	4.9%
Miscellaneous Store Retailers	\$34.3	5.2%	\$64.2	6.0%	9.4%
Nonstore Retailers	\$6.1	0.9%	\$18.7	1.7%	17.3%
Food Services and Drinking Places	\$69.7	10.6%	\$116.7	10.9%	7.6%
Total	\$655.7	100.0%	\$1,070.5	100.0%	7.3%

Source: BST Associates, WA State Department of Revenue data

Three other types of retailers saw faster than average rates of growth during this period, albeit from lower starting points. These included: electronics and appliance store (13.6% AAGR), miscellaneous store retailer (12.2%), and nonstore retailers (20.3%).

Electronics retailers tend to be out of scale for the Old Town, and typically have larger stores with large parking lots, or are located in malls. According to data from Dun & Bradstreet, electronics retailers currently located in Silverdale include:

- Radio Shack (Radioshack Corporation)
- Nuts About Hi-Fi (U2 Inc)
- Magnolia Hi-Fi (Magnolia Audio Video)
- Printer Specialist (Lasertech Northwest)
- Software Design
- Computer Renaissance (Marine Services Inc)
- Eb Games 397 (Gamestop Inc)
- Software Etc 1013 (Gamestop Inc)

Miscellaneous store retailers include include stores with unique characteristics like florists, used merchandise stores, and pet and pet supply stores as well as other store retailers. Some of the stores in this category typically have big-box stores or are located in malls, and are not appropriate for Old Town. Examples in Silverdale include:

- Office Depot (Office Depot Inc)
- Petsmart (Petsmart Inc)
- Bath & Body Works 151 (Bath & Body Works Inc)
- Disney (Hoop Retail Stores LLC)
- AT&T (New Cingular Wireless Svcs Inc)
- McBride Hallmark Shop (Mc Brides Cards & Gifts Inc)

However, others in this category might be of the appropriate scale for Old Town. Examples of these include:

- **Material Girls Quilt Shop**
- **Kitsap Audiology**
- **Pattern & Depression Glassware**
- **Spooner House Antiques**
- **OHara Collectibles**
- **Mikes Coins**

Industries in the Nonstore Retailers subsector include mail-order houses, vending machine operators, home delivery sales, door-to-door sales, party plan sales, electronic shopping, and sales through portable stalls (e.g., street vendors, except food). Establishments engaged in the direct sale (i.e., nonstore) of products, such as home heating oil dealers, newspaper delivery are included in this subsector. Some of these businesses may be an appropriate target market for retail or office space in Old Town Silverdale. A sample of non-store retailers currently located in Old Town includes:

- **Ash Grove Audiobook Exchange**
- **Sensaria Body Care**
- **Hello Dollie**
- **Morning Star Vending**
- **Alltime Vending.**

Port of
Silverdale

Chapter Three – Industrial & Commercial Development

Introduction

The following chapter evaluates the demand for a various land uses, including:

- Industrial
- Retail/commercial
- Recreation (Marina & Fuel Dock)

Supply of Developable Land

Industrial development is one of the primary roles that a port district can play in the **state of Washington**. Port districts commonly develop industrial parks in order to attract tenants who will **create good-paying jobs** to a community. In Kitsap County the Port of Bremerton has been the most **active in this kind of development**.

As part of the process for developing this Comprehensive Plan the potential for the Port of Silverdale **entering the industrial market** was investigated. As demonstrated below, there does not appear **to be a strong need** for the Port to enter the industrial land business.

The Silverdale Urban Growth Area (UGA) has more than 200 acres of vacant **commercial and industrial land**, according to the *Kitsap County 2005 Updated Land Capacity Analysis*. Of **this total nearly 80%** (i.e., 159 acres) is zoned for industrial use. (see Table 5 on the following page)

Countywide there is a total of 479 acres of vacant commercial and industrial land, of **which the Silverdale UGA** accounts for nearly 42%. Nearly 45% of this vacant land is zoned for industrial, and **75% of this vacant industrial land**, or 159 acres, is in the Silverdale UGA.

In addition to the vacant industrial land Kitsap County also has an equal amount of **industrial space that is underutilized**. Most of this underutilized industrial space (i.e. 185 acres) is located in the **South Kitsap Industrial Park**, but only 22 acres in the Silverdale UGA.

In total Kitsap County has 433 acres of vacant and underutilized industrial land, of which **181 acres, or 42% of the total**, is in Silverdale.

Vacancy rates for industrial buildings (as opposed to developable land) in North Kitsap **have generally been** quite low since late 2001. Since that time the rate has generally averaged less **than 5%**, **although in recent** periods that increased to as much as 7.6%.

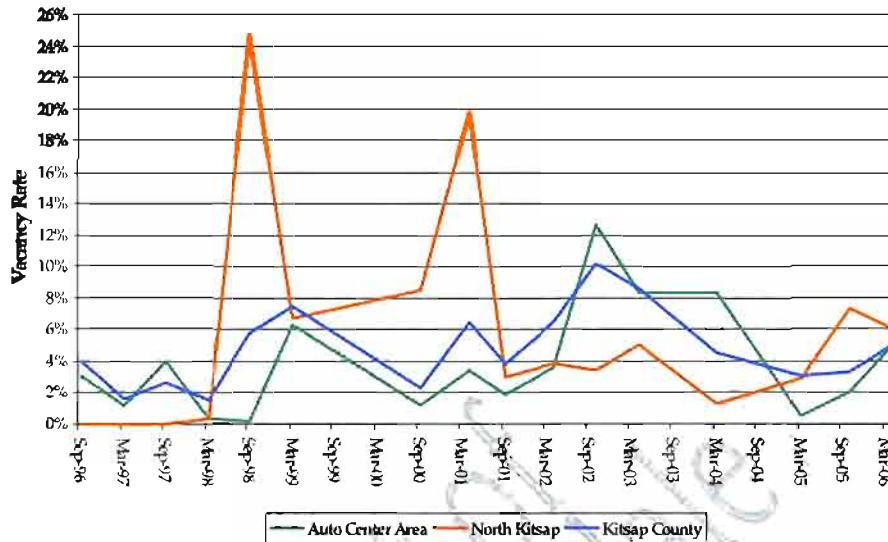
**Table 5 – Vacant and Underutilized Commercial and /Industrial Land
Unincorporated UGAs**

	Highway Tourist Comm.	Neighbor- hood Comm.	Urban Comm.	Regional Comm.	Business Park	Business Center	Indust.	Urban Village Center	Vacant Total
Vacant									
Bremerton East	2	-	-	-	-	-	-	-	2
Bremerton West	2	-	-	-	-	-	4	-	6
Central Kitsap	33	5	-	-	-	-	-	-	37
Gorst	4	-	-	-	-	-	-	-	4
Kingston	11	-	-	-	-	-	5	-	17
Port Orchard	57	4	-	-	-	-	2	-	63
Silverdale	-	2	-	23	17	-	159	-	201
SK Industrial Park	-	-	-	-	-	107	43	-	150
Total*	108	10	-	23	17	107	213	-	479
Underutilized									
Bremerton East	1	0	-	-	-	-	-	-	1
Bremerton West	2	-	-	-	-	-	1	-	4
Central Kitsap	10	1	-	-	-	-	5	-	16
Gorst	7	-	-	-	-	-	1	-	8
Kingston	6	-	-	-	-	-	-	2	9
Port Orchard	51	1	-	-	-	-	5	-	56
Silverdale	-	5	-	34	8	-	22	-	69
SK Industrial Park	-	-	-	-	-	582	185	-	767
Total*	78	6	-	34	8	582	220	2	930
Total									
Bremerton East	3	0	-	-	-	-	-	-	3
Bremerton West	4	-	-	-	-	-	5	-	9
Central Kitsap	43	5	-	-	-	-	5	-	53
Gorst	11	-	-	-	-	-	1	-	12
Kingston	18	-	-	-	-	-	5	2	25
Port Orchard	107	5	-	-	-	-	7	-	119
Silverdale	-	7	-	56	26	-	181	-	270
SK Industrial Park	-	-	-	-	-	689	228	-	917
Total*	186	17	-	56	26	689	433	2	1,409

Source: Kitsap County 2005 Updated Land Capacity Analysis

For most of the period since late 2001 the industrial building vacancy rate in North Kitsap County was substantially lower than either the county average or in the Auto Center Area. The recent run-up in vacancy rates in North Kitsap coincided with decreasing vacancy in these other areas, causing the North Kitsap rate to exceed the others. In general, though, vacancy rates for industrial space in Kitsap County are low.

Figure 14 – Kitsap County Industrial Vacancy Trends

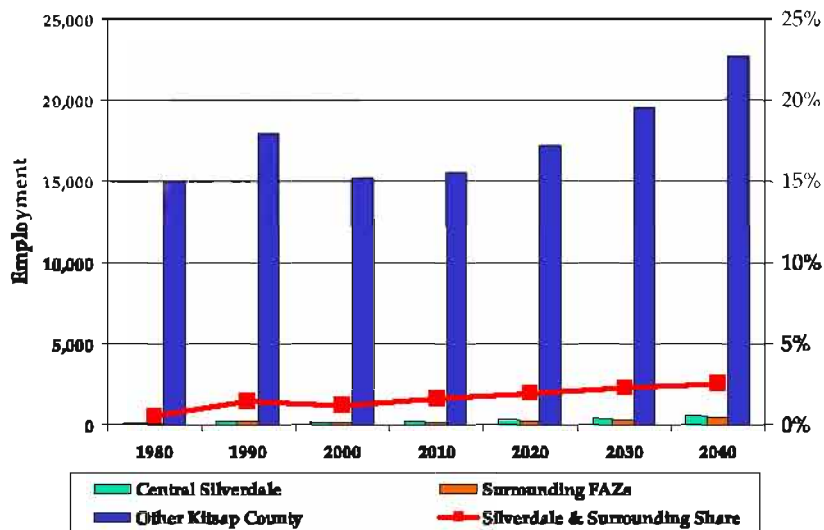


Source: Bradley Scott, Inc

Industrial Space Demand

Demand for industrial space is typically related to the manufacturing industry and the WTCU (wholesale trade, transportation, communications, utilities). As discussed earlier in this document, employment in these industry sectors is very low in the Silverdale area, and is not projected to increase significantly in the long run.

Figure 15 – Projected Job Growth in Silverdale - Industrial



Source: PSRC

Figure 14 shows that the Silverdale area had only 334 industrial jobs in 2000. The jobs total projected to grow to just 1,105 jobs by 2040, or an addition of approximately 20 jobs per year. Kitsap County is expected to see industrial employment grow by nearly 7,500 jobs between 2000 and 2004, and the Silverdale is not expected to account for more than 3% of the total by 2040.

Data from the Kitsap County Assessor show that Silverdale currently has approximately 500,000 square feet of industrial buildings, including light manufacturing, light utility/storage, warehouse, and small shop space. Based on an average of 2,000 feet per employee, projected job growth between 2007 and 2020 will create the demand for an additional 144,000 square feet of industrial space.

Assuming a floor area ratio (FAR) of 0.5, 144,000 square feet of industrial buildings would require less than 7 acres of industrial land. The Silverdale UGA currently has 22 acres of underutilized industrial space and 159 acres is in the Silverdale, so the supply of land should be more than adequate to meet this demand.

Industrial Space Conclusion

BST Associates contacted a number of commercial real estate agents in order to develop a better understanding of the industrial market in the Silverdale area. According to these agents vacancy rates for industrial space are currently low, but developers are reluctant to build speculative space. As a result, there is little industrial space available on short notice.

One factor limiting industrial growth around Silverdale is a sewer system that is inadequate for heavy industrial uses. This limits the potential uses of Silverdale industrial land to light manufacturing and distribution, while heavy manufacturing would be most likely to locate in the South Kitsap Industrial Area.

The large amount of vacant industrial land around Silverdale suggests that there has not been much market incentive for private developers to build industrial space in the area. It is possible that the Port of Silverdale could develop spec space in order to try to attract industrial jobs, something that the private sector is reluctant to do. However, the low number of jobs expected over the long run makes this a more limited prospect for the Port.

Commercial, Retail & Related Development

Development of mixed use buildings in waterfront areas has become a very popular and financially successful business in the past decade. There are numerous examples of these facilities in the Pacific Northwest, including the Riverplace project in Portland, East Bay Landing in Olympia, and Marina Village in Everett, among others. The waterfront in downtown Bremerton is currently undergoing a massive transformation, with redevelopment adding a new regional waterfront conference center, hotel, parking, restaurants, retail space condominiums, and an expanded marina facilities.

Mixed use developments include the following types of uses:

- Commercial office for professional services such as law offices, financial services, real estate offices, insurance companies, doctors, etc.
- Retail space generally oriented toward upscale retail goods such as art, jewelry, leather goods, wine shops etc.
- Personal services such as hair salons and barbers.
- Restaurant, delicatessen and/or related food products
- Condominiums or apartments

These developments are attractive additions to waterfront access. With the property that it has accumulated in the Old Downtown Silverdale area, the Port of Silverdale is in a good position to take the lead in promoting this kind of development.

Supply of Commercial Space

Office Space Supply

According to data from the Kitsap County Assessor, there is a current inventory of approximately 1.75 million square feet of retail space located within the Port of Silverdale district boundary. Nearly 800,000 square feet, or almost half, of this is classified as “office”. Medical and dental space accounts for another 286,000 square feet, or 16.4%. Churches account for 238,000 square feet (13.6%), recreational facilities account for 106,000 square feet (6.1%), and automotive uses account for 77,000 square feet (6.1%). Various other services account for the remaining 242,000 square feet, or 13.8% of the total.

Table 6 –Square Feet Services Space Located Within in Port of Silverdale Boundary

Retail	Sq Ft
Auto Service	59,316
Bank	52,200
Bowling Alley	50,089
Car Wash Auto	2,051
Church	161,146
Church w/ Sunday School	7,072
Cinema Theater	42,755
Clubhouse	68,290
Commercial Garage	11,453
Convalescent Hospital	27,944
Day Care Facility	37,555
Dental Clinic	12,411
Fitness Center	11,392
General Commercial	2,859
Health Club	1,608
Hotel/Motel Service	112,500
Hotel/Motel Unit	21,912
Kennel Animal Care	2,560
Medical Office	210,021
Mini-Lube Garage	1,364
Office	798,582
Senior Care Facility	36,000
Senior Clubhouse Center	1,640
Service Garage	2,752
Veterinary Hospital	12,405
Total	1,747,877

Source: Kitsap County Assessor, BST Associates

The vacancy rate for office space in Kitsap County has been declining for most of the past decade in Kitsap County, and in most recent years the rate in Silverdale was even lower.

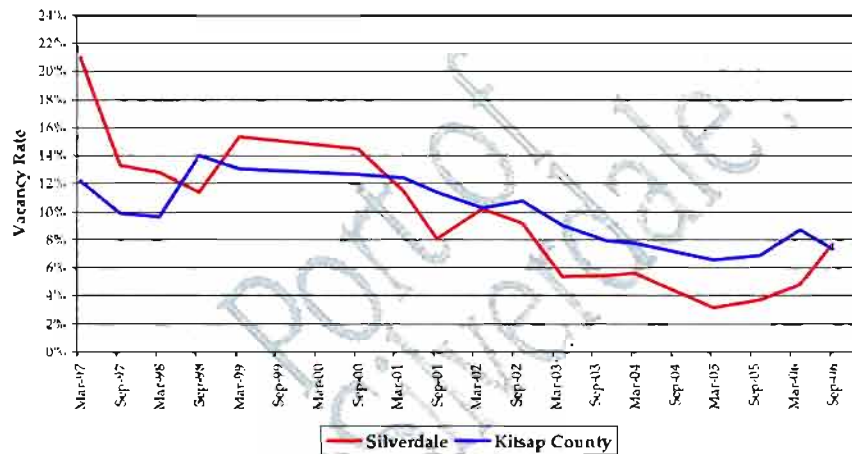
In March of 1997 the office vacancy rate in Kitsap County was 12%. This dropped slightly the next year, then reached a high of 14% in the fall of 1998. From that time on the rate declined steadily, to an average of less than 8% since September of 2003.

For most of the period between early 1997 and early 2001 the office vacancy rate in Silverdale was higher than the county average, typically running approximately 2 percentage points higher. Beginning in the spring of 2001, however, the Silverdale rate began to decline sharply, and since that time has averaged more than 2 points lower than the county rate. Since Spring of 2005 the Silverdale rate has been edging back upward, and by Fall of 2006 was essentially the same as the county average, or nearly 8%.

According to commercial real estate agents, the asking base rate for office space in Silverdale is close to \$17.00/sq ft/year, while the countywide asking rate is closer to \$15.00. Triple net costs add an additional \$3.50 - \$8.00 per year to this price.

The increase in vacancy over the past year or two has made leasing more difficult. Finding tenants is taking longer than before, and landlords are willing to offer more incentives to potential tenants in order to fill space.

Figure 16 – Kitsap County Office Vacancy Trends



Source: Bradley Scott, Inc

Retail Space Supply

According to data from the Kitsap County Assessor, there is a current inventory of approximately 2.8 million square feet of retail space located within the Port of Silverdale district boundary. The largest share of this is classified as “general retail”, followed by “department store”, “neighborhood shopping center”, and “regional shopping center”. These five designations account for nearly 80% of the retail space in the district.

Fluctuations in the rate of vacancy for retail space in Silverdale has followed the same general pattern as those for the county as a whole, with Silverdale seeing higher highs and lower lows than the county. In the late 1990’s the vacancy rate in Silverdale averaged less than 2% for more than two years, while the county average was closer to 6%. From early 2004 through early 2006 the Silverdale rate averaged less than 4% while the county rate averaged more than 6%.

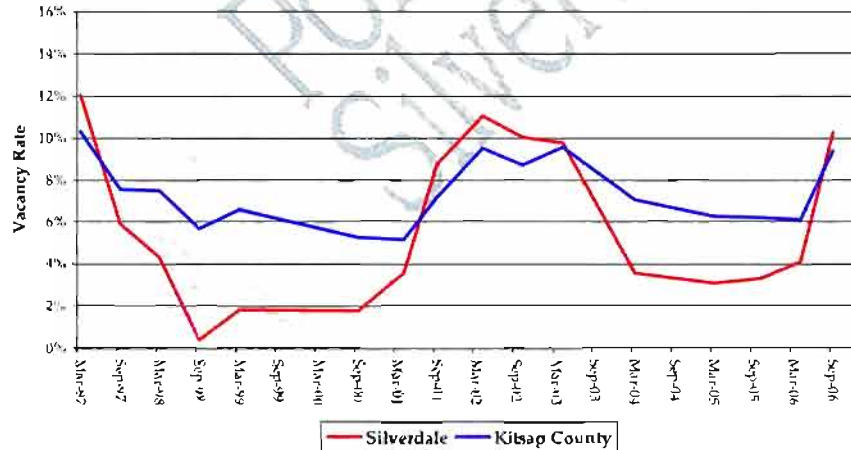
Table 7 –Square Feet of Retail Space Located Within in Port of Silverdale Boundary

Retail	Sq Ft
Auto Showroom	10,929
Convenience Market	26,417
Department Store	411,145
Discount	277,024
Fast Food Restaurant	24,089
General Retail	1,092,219
Neighborhood Shopping Center	360,811
Regional Shopping Center	333,499
Supermarket	114,131
Snack Bar	180
Tavern/Bar	9,987
Warehouse Discount Store	117,204
	2,777,635

Source: Kitsap County Assessor, BST Associates

At the other extreme, the Silverdale retail vacancy rate averaged more than 10% in 2002 while the county rate was closer to 9%. The most recent data shows that the retail vacancy rates for both Silverdale and Kitsap County jumped upward in late 2006, with the Silverdale rate moving above 10% and Kitsap County above 9%.

Figure 17 – Kitsap County Retail Vacancy Trends



Source: Bradley Scott, Inc

According to interviews with commercial realtors the average asking rate in Kitsap County can be as high as \$24.00/sq ft/year for retail space, but currently listed properties range from \$11.00 to \$20.00. The relatively low asking prices for currently listed properties is likely due to the relatively high vacancy rates.

Demand for Commercial Space

Office Space Demand

As described in Chapter 2, the services sector (finance, insurance, real estate & services, or “FIRES”) has been another major source of job growth in the Silverdale area, and employment in the

sector is expected to continue to grow strongly. PSRC projections show that an average of 140 service jobs may be added per year in the Silverdale area, driving demand for office space in the area.

Table 5 – Project Job Growth in Services Sector (FIRES)

Year	Central Silverdale	Surrounding FAZs	Silverdale & Surrounding	Other Kitsap County	Silverdale & Surrounding Share
1980	1,630	381	2,011	9,290	17.8%
1990	1,939	1,363	3,302	14,671	18.4%
2000	3,442	1,122	4,564	19,749	18.8%
2010	3,953	1,271	5,224	20,648	20.2%
2020	5,001	1,603	6,604	24,359	21.3%
2030	6,175	2,021	8,196	28,567	22.3%
2040	7,605	2,588	10,193	33,928	23.1%

Source: PSRC

Most of the growth in the services sector is forecast to occur in central Silverdale, which indicates strong potential demand for a Port-led redevelopment effort in Old Town Silverdale. This FAZ had service employment of approximately 3,400 in 2000, and accounted for 75% of these jobs. Between 2000 and 2040 job growth is projected to average 104 per year in central Silverdale and 37 in surrounding FAZs, and in 2040 central Silverdale is expected to still be the home to 74% of service jobs.

The share of Kitsap County service sector jobs that are located in the Silverdale area grew slowly between 1980 and 2000, but this growth in market share is projected to accelerate between 2000 and 2040. In 1980 the Silverdale area had less than 18% of county service jobs. This share climbed to nearly 19% in 2000, and is projected to reach 23% by 2040.

Increasing demand for office space created by the growth in the service sector has important implications for the Port of Silverdale, especially in regard to the Old Town property.

Using data from the Kitsap County Assessor, PSRC, and Dun & Bradstreet, the average commercial space per service employee ranges between 400 and 450 square feet in Silverdale. With services employment projected to grow by approximately 1,100 to 1,370 jobs between 2007 and 2020, an additional 440,000 to 575,000 square feet of office space will be needed by 2020 within the Port of Silverdale district.

Tempering this growing demand for office space is uncertainty in the Navy budget. Although government in general and the Navy in particular are projected to account for a decreasing share of total employment, all of Kitsap County, including Silverdale, is impacted by Navy spending decisions.

Retail Space Demand

With the opening of Kitsap Mall in 1985 central Silverdale became the one of the most important retail centers in Kitsap County. In 1980, prior to the mall opening, the Silverdale area accounted for one out of five retail jobs; by 2000 this share had jumped to nearly one of three retail jobs. Retail employment in the Silverdale area shot up from less than 1,700 jobs in 1980 to more than 5,300 in 2000.

The Puget Sound Regional Council projects that jobs in the retail sector will continue to grow in the Silverdale area, albeit at a more measured pace than during the previous two decades. Through the year 2010 the Silverdale area is projected to add an average of less than 40 jobs per year, but between 2010 and 2020 an additional 80 to 100 jobs per year are expected.

Most of the new retail jobs are likely to be in central Silverdale, but a large share is also expected to locate in the surrounding area.

Figure 18 – Projected Job Growth Retail Sector

Year	Central Silverdale	Surrounding FAZs	Silverdale & Surrounding	Other Kitsap County	Silverdale & Surrounding Share
1980	1,441	242	1,683	6,518	20.5%
1990	3,228	402	3,630	9,974	26.7%
2000	4,225	1,107	5,332	11,144	32.4%
2010	4,396	1,307	5,703	12,427	31.5%
2020	4,865	1,631	6,496	14,438	31.0%
2030	5,339	2,009	7,348	16,551	30.7%
2040	5,862	2,461	8,323	18,909	30.6%

Source: PSRC

Using data from the Kitsap County Assessor, PSRC, and Dun & Bradstreet, the average commercial space per retail employee ranges between 560 and 840 square feet in Silverdale. With retail employment projected to grow by approximately 475 to 725 jobs between 2007 and 2010, an additional 265,000 to 600,000 square feet of retail space will be needed by 2020 within the Port of Silverdale district.

Commercial Space Conclusion

Vacancy rates for commercial space are cyclical, and recent years have seen low vacancy rates. Although recent evidence indicates an increase in vacancies, in the long run the amount of commercial space needed will continue to grow as the population of Kitsap County climbs.

The demand for commercial space in Silverdale is strong; especially for office space to serve the fast-growing services industry. Employment growth projections for the Silverdale area indicate that as much as 575,000 square feet of office space may be needed between 2007 and 2020. The office market presents an attractive opportunity for the Port of Silverdale’s Old Town properties.

Retail space is also seeing increasing demand, although the rate of growth in the retail sector is not expected to be as strong as growth in services. Mixed use redevelopment of Port property in the Old Town area is likely to see demand from the retail sector as well as from the services sector.

The growth in the population of Kitsap County also means a need for additional housing. The Port of Silverdale property in Old Town Silverdale may present an attractive location for development of mixed use buildings that contain both housing and commercial space.

Chapter Four – Recreational Boating

Introduction

The Port of Silverdale extends from Hood Canal to Puget Sound. The Port currently provides two facilities for recreational boating, both of which are located in Old Town Silverdale:

- The Port of Silverdale pier provides transient moorage for up to three nights at a time, and offers fresh water hook ups, sewage pump out and power.
- The Port of Silverdale boat launch is one of the deepest in the area, allowing the launching of boats even at very low tides. Between April and October the ramp also has a pier alongside, which makes launching and entering your boat a breeze.

The Port of Silverdale does not currently have the facilities to offer year-round round permanent moorage. The following section examines the demand for such a facility.

Inventory of Moorage

Puget Sound Marinas

There are an estimated 39,000+ slips in marinas in Puget Sound. Kitsap County accounts approximately 7% of the total permanent slips. However, Kitsap County marinas have a higher proportion of smaller slips than found elsewhere in Puget Sound.

Table 8 – Summary of Permanent Moorage Slips in Puget Sound

County	Less than 30 feet	31 to 40	41 to 50	Over 50	Subtotal known lengths	Total	% Total
Clallam	645	188	242	38	1,113	1,113	2.8%
Jefferson	374	178	75	12	639	1,181	3.0%
Kitsap	751	532	273	78	1,634	2,740	7.0%
Mason	42	55	10	-	107	177	0.5%
Thurston	1,080	514	227	110	1,931	2,303	5.9%
Pierce	269	324	269	39	901	3,622	9.3%
King	1,665	2,293	883	604	5,445	8,057	20.6%
Snohomish	1,437	905	138	-	2,480	2,655	6.8%
Island	217	93	42	-	352	352	0.9%
Skagit	304	478	197	57	1,036	3,687	9.4%
Whatcom	1,344	1,258	492	262	3,356	3,356	8.6%
San Juan	320	343	127	160	950	1,450	3.7%
subtotal	8,448	7,161	2,975	1,360	19,944	39,130	100.0%
% Known	42.4%	35.9%	14.9%	6.8%			
Kitsap % Total	8.9%	7.4%	9.2%	5.7%	8.2%	7.0%	

Source: BST Associates, marinas

While data on length is incomplete¹ for some marinas, the information available shows that most of the existing slips in Puget Sound area are less than 40 feet long. Information on length is available for

¹ Some marinas have lineal moorage, which is difficult to translate into relative slip sizes, and others do not report their individual slip lengths.

nearly 20,000 slips. Of this total nearly 4,500 slips, or 78%, are 40 feet long or less. Approximately 15% are between 41 and 50 feet and 6% are over 50 feet long.

This summary underscores a problem for the marina industry, in that marinas that were built 25 or more years ago were sized to a fleet that no longer exists. As a result, some marinas are seeking to reconfigure to larger (and wider) slip sizes. Under the right circumstances, it also may create an opportunity for new marinas.

Table 9 – Kitsap County Marinas

Name of boating facility:	Public/Private	City	Permanent Slips	Transient Slips	Dock Space (ft)
Bainbridge Island Marina	Private	Bainbridge Island	40	-	2,000
Eagle Harbor Dock	Public	Bainbridge Island	-	-	100
Eagle Harbor Marina	Private	Bainbridge Island	107	-	100
Eagledale Moorings	Private	Bainbridge Island	34	-	-
Harbour Marina	Private	Bainbridge Island	50	-	-
Winslow Wharf Marina	Private	Bainbridge Island	239	-	-
Bremerton Marina	Public	Bremerton	220	100	1,200
Ilahaee State Park	Public	Bremerton	-	-	356
Port of Brownsville	Public	Bremerton	310	-	1,000
Port Washington Marina	Private	Bremerton	81	-	-
Port of Kingston	Public	Kingston	262	-	1,500
Blake Island State Park	Public	Manchester	-	-	1,744
Dockside Sales & Service	Private	Port Orchard	30	-	-
Port Orchard Marina	Public	Port Orchard	329	44	1,500
Port Orchard Marine Railway	Private	Port Orchard	51	-	-
Sinclair Inlet Marina	Private	Port Orchard	63	-	-
Bay Marine, Inc.	Private	Poulsbo	120	-	-
Liberty Bay Marina	Private	Poulsbo	177	-	-
Port of Poulsbo	Public	Poulsbo	278	133	400
Poulsbo Yacht Club	Private	Poulsbo	149	-	200
Seabeck Marina	Private	Seabeck	200	-	-
Port of Silverdale	Public	Silverdale	-	-	1,300
		Subtotal	2,740	277	11,400

Future Development of Marinas

There are several proposed marina projects in Kitsap County and elsewhere in the region.

The existing Bremerton Marina, which currently provides 42 transient slips, has a problem with ferry wake wash and has been under-utilized. The Port is currently constructing a new marina that will have approximately 350 slips, providing approximately 100 transient slips and 250 permanent slips. This development is considered an integral part of the effort to revitalize downtown Bremerton.

Another new private marina is being discussed at Port Washington Narrows, between Silverdale and Bremerton. Plans and details are uncertain at the present time.

A Seattle-based development group is also proposing the demolition of the current dilapidated Seabeck marina and construction of a 200-slip Olympic View Marina complete with a breakwater, fuel dock and pump-out station. Work is set for completion in 2008.

Marina projects in neighboring counties include:

- **Jefferson County**
 - The Port of Port Townsend is considering currently evaluating a reconfiguration at the Boat Haven that would likely lead to more large slips.
 - Port Ludlow completed the EIS for the 100-slip marina expansion was completed in 2002, but progress has bogged down due to litigation and it is unclear whether these plans will move forward.
 - Dredging at Cape George Colony is also being impacted by litigation and finances.
- **Skagit County**
 - The Swinomish Tribe has been 1,200-slip marina along the Swinomish Channel for more than 20 years, but the cost of development may not provide significant cash flow for the Tribe at today's moorage rates.
 - There are also a few small scale marinas being considered in Anacortes, but construction costs make the project financially questionable.
 - Marina reconfigurations are also being undertaken or considered at Cap Sante Marina in Anacortes and La Conner Marina.
- **Island County**
 - The recently completed a Master Plan for the Oak Harbor Marina calls for a reconfiguration of the marina, but the marina financial plan has not yet been completed.
 - The Port of South Whidbey Island is considering developing a permanent marina at Langley for up to 200 boats, but the Port has not completed the design or the financial feasibility assessment.
- **Snohomish County**
 - The Port of Everett is currently constructing the 155-slip 12th Street Marina.

Competitive Summary

The market region in which a new marina in Silverdale would compete includes the counties in northern Puget Sound (i.e., Kitsap, Jefferson, Whatcom, Skagit, San Juan and Island counties), which currently includes approximately 13,000 permanent moorage slips. Skagit County accounts for the largest share of moorage in northern Puget Sound, with approximately 29% of the total moorage. Whatcom County accounts for 26% of the available slips, while Kitsap accounts for 21%, San Juan accounts for 11%, Jefferson for 9% and Island County approximately 3%.

Demand for Moorage

The demand for marina space in Puget Sound is driven by the growth of boats that need wet moorage. Essentially any boat 30 feet and longer will require wet moorage, as will a portion of those less than 30 feet in length.

In Puget Sound, the fleet of recreational boats over 30 feet long has grown from 9,000 in 1990 to 13,200 in 2004. The fleet has grown at 2.8% per year, which is a much faster than the population growth rate of 1.5% per year.

Recreational Boat Fleet

In Washington boats must be registered if they are 16 feet or longer and not 100% man-powered. Boats less than 16 feet in length with 10 horsepower or less are not required to register unless they are used on federal waters, while craft that are 100% man-powered do not need to be registered.

The number of boats over 30 feet in length grew more rapidly, from 9,012 in 1990 to 13,217 in 2004 or at 2.8% per year. The population base in the counties bordering Puget Sound grew at 1.7% per year during this time period.

The trends indicate more rapid growth in boat ownership as the boat length increases:

- The number of boats between 16 and 20 feet in length grew at 1.1% per year,
- The number of boats between 21 and 30 feet in length grew at 1.3% per year,
- The number of boats between 31 and 40 feet in length grew at 2.2% per year,
- The number of boats between 41 and 50 feet in length grew at 3.9% per year,
- The number of boats between 51 and 60 feet in length grew at 5.1% per year, and,
- The number of boats over 60 feet in length grew at 7.9% per year.

Boat Sales

According to data from the Northwest Marine Trade Association, sales of new boats over 30 feet in length averaged 310 units per year between 2003 and 2006. Sales of used vessels typically average four times that of new vessels, but used vessels do not necessarily contribute to the net increase in demand for moorage, unless the new owner chooses to relocate the vessel. In addition, used boats may come from outside the region, creating additional demand for moorage.

The market for recreational boats is expected to remain relatively strong for the foreseeable future, which is discussed in the following sections.

Permanent Slips

Forecast of Boats & Slips

The future number of boats in Puget Sound was projected using regression analysis, which projects future boat ownership based upon growth in population, non-agricultural employment and real personal income. Based upon this approach, BST Associates projects that the recreational fleet of boats 21 feet and longer will grow from 112,135 boats in 2004 to 138,160 boats in 2020 or at 1.3% per year, which is approximately the same rate of growth as experienced between 1990 and 2004.

Growth is projected to be faster as the boat length increases, as is the case with existing trends. The wet moorage market focuses on boats over 30 feet in length. This group of boats is expected to increase from 13,217 boats in 2004 to 19,742 in 2020 or at 2.5% per year. The forecast growth rate is expected to slow slightly from recent trends but is still stronger than the expected growth in population, indicating increased market penetration.

The demand for wet moorage is expected to increase by 5,936 slips during this 16 year time period or approximately 371 slips per year. There is expected to be a shortage in the number of slips provided to serve this demand.

Marina Hinterland

The potential capture rate for a particular marina depends upon the market area that supports the marina. This in turn depends upon size of the population and housing base (primary and secondary), the availability of alternative moorage as well as other amenities that might attract the boater.

The regional use of marinas is a good starting point. Kitsap County draws primarily from the local market but is supplemented by boats from Central Puget Sound, other parts of Washington State and out of state residents. According to data from the Department of Licensing, most vessels that are kept in Kitsap County are owned by Kitsap County residents (82.7%). The remaining 17% are located in King Country (5.5%), Pierce County (3.2%), Jefferson County (1.2%), Snohomish County (0.9%), other Washington (2.9%) and Out of State residents (3.5%).

Marina projects in Bremerton, Seabeck and Kingston will draw from the same potential customer base as a new facility at Silverdale.

Demand for Permanent Slips

The demand for wet moorage slips (30 feet long and longer) in Puget Sound is expected to average 371 slips per year or 3,700 slips over a ten year period. Kitsap County currently accounts for approximately 7% of the moorage slips in Puget Sound. In the future, Kitsap County is expected to account for 13% (low) to 14.5% (high) of the new slips. The higher capture rate for new slips is due to the improbability of new marina slips being built in King and Snohomish counties.

The expected number of new permanent moorage slips in Kitsap County is expected to range between 481 and 537 over the next ten years, with a most likely projection of 509 new slips.

Marine Fuel

Local boaters have expressed interest in a fueling facility at the Port of Silverdale. Given market considerations, however, such a facility would not generate sufficient sales to cover the cost of construction.

Where boaters purchase fuel is tied to the size of the boat: boats that are too long to trailer must purchase fuel at marine fuel facilities, but boats that are trailered can purchase fuel anywhere. The closest marine fuel facilities are at Poulsbo, Brownsville, and Port Orchard. Since prices at marine fuel facilities tend to be higher than at gas stations, trailered boats are more likely to purchase fuel at a gas station.

Since there is a boat ramp in Silverdale but no marina with permanent moorage, much of the boating activity in the area involves trailerable boats. There are also just a handful of private docks along Dyes Inlet for larger vessels, along with a number of mooring buoys. This implies that the Port would generate sales from just a small portion of the boaters in the area.

Compounding the problem is the relatively short boating season, which impacts the financial feasibility of fuel facilities. As demonstrated in the preceding section of this report, most boat activity takes place between Memorial Day and Labor Day. This means that the Port of Silverdale, or any other marine fuel facility, must generate sufficient sales over a four month period to cover 12 months worth of operation & maintenance costs as well as financing costs.

A recent analysis for a similar fuel facility in the region estimated annual sales of 30,000 to 50,000 gallons of fuel. With the Port generating a margin of \$0.25 per gallon sold the net revenue to the Port would run between \$7,500 and \$12,500 per year. This represents the total available to pay the cost of construction, operations and maintenance, manpower, and other costs. This is clearly not a financially feasible concept.

Marina Demand Conclusions

Growth in the number of boats that require permanent (year-round) moorage will generate the need for around 300 to 400 more slips per year for the next ten years in Puget Sound. Kitsap County is expected to account for 12% to 15% of this demand, or 40 to 50 slips per year. This share of additional demand stands in contrast to the amount of moorage currently available in Kitsap County, which is approximately 7% of the Puget Sound total.

Two projects that are currently underway are likely to satisfy the demand for additional permanent moorage in Kitsap County. The first of these, the new downtown Bremerton marina, will have as many as 350 slips, while a reconstructed Seabeck marina is expected to provide 200 slips. In total these two facilities will provide an additional 550 slips, while anticipated demand for the next ten year is for 509 slips. As a result, it does not appear that there is a need for the Port of Silverdale to develop permanent moorage facilities.

Demand for transient moorage is seasonal, with nearly all of the demand occurring between Memorial Day and Labor Day. The demand for additional transient moorage space in Silverdale will depend on boaters being attracted to Old Town. Redevelopment of Old Town may increase the number of shore-side activities available to boaters, and increase the demand for temporary moorage. The Port should track usage of the existing transient dock to determine if and when the demand for temporary moorage exceeds the amount of available. When that occurs that Port should examine the possibility of expanding the existing facility.

Marina Finance Considerations

Construction costs for marinas are escalating rapidly, which makes it difficult to build marinas profitably or to find financing. Compounding the problem are low moorage rates in the Kitsap County market. Estimated construction costs at some recent marina projects in the Puget Sound have run \$10.00 per linear foot or more, but moorage rates at marinas near Silverdale are much lower:

- Port of Poulsbo - \$4.23
- Port of Brownsville - \$4.75 - \$5.75
- Port Washington Marina - \$5.75
- Port of Kingston - \$4.57

The New Bremerton marina is a \$23 million project, and a similar-sized project for Silverdale would likely face similar costs. This level of commitment would likely use all of the Ports financing capacity and preclude the Port of Silverdale from engaging in other types of projects.



**PORT OF SILVERDALE
2020 COMPREHENSIVE PLAN**

**APPENDIX H: Capital Improvement Program
and Self-Certification Forms**

Capital Improvement Program Form

SHORT AND LONG TERM OBJECTIVES				
Project	Timeline	Potential Funding Sources	Cost Estimate Range	Primary Decision Criteria
SHORT TERM OBJECTIVES Years 1-4				
Expand overwater facilities with non-motorized boating facility ramp and canoe access, extending mooring floats and complete dredging	1-4 years	Port Funds, Grants including Boating Facility Program, ALEA and WWRP-Water Access	\$700,000 - \$1,500,000	Volunteer and commercial community boating organizations have prompted the Port to rethink the marina in terms of human-powered boating and overwater events
Beach enhancement with soft beach protection from the boat launch east to the fixed pier	1-4 years	Port Funds, Grants including ALEA, WWRP-Water Access, ESRP, and Local Parks	\$250,000 - \$500,000	Soft armoring and a new pedestrian trail away from eroding lawn edge are encouraged. These beach restoration opportunities will be pursued in concurrence with project development priorities identified
Non-motorized boating center, with design including a new building and public viewing on top as a joint project with Kitsap County	1-4 years	Port Funds, Grants including WWRP-Local Parks, ALEA, WWRP-Water Access	\$350,000 - \$750,000	In partnership with Kitsap County Public Works this would develop a joint facility to serve Port and community needs including watercraft storage
Restoration and improvement of large motorized moorage floats – electrical rewiring, lighting and plug-in stations; possible re-plumbing and other features as designs progress	1-4 years	Port funds, Grants including Boating Facilities Program, ALEA, WWRP-Water Access, Local Parks, etc.	\$250,000 - \$500,000	Growing need for improved solutions for the storage and transport of sail and motor-powered watercraft currently stored on Port property
Joint effort with Central Kitsap School District (CKSD) to improve Strawberry Creek as a salmon stream, including acquisition of property to allow the stream to meander	1-4 years	Port funds, CKSD support, Grants including SRFB, PIE, ALEA, Water Access, etc.	\$250,000 - \$700,000	This salmon-bearing stream, bisects CKSD's acreage and flows through culverts into Dyes Inlet in a compromised fashion. The Port and CKSD would share in restoring Strawberry Creek
Planning and development of trail systems to enhance pedestrian connections, respect the natural flow of the area for future cultural, educational or hospitality use	1-4 years	Port funds, Grants including WWRP-Trails, Local Parks, Water Access, etc.	\$150,000 - \$500,000	Defining better pedestrian areas and linkages meets the Port's recreational mission. And, due to its proximity to Strawberry Creek and the existing pedestrian trail, it makes sense to expand this connection further by enhancing walkability

<p>Demolish the Old Town Pub building, possibly relocate the existing buildings to the east of the Pub building, and design/build a facility for waterfront oriented non-profit use and retail</p>	<p>1-4 years</p>	<p>Port Funds, non-profit support, philanthropic grants, historic preservation funds, economic development opportunities, etc.</p>	<p>\$500,000 - \$1,500,000</p>	<p>Determine if the existing Old Town Pub building can be renovated at a reasonable cost and what value it might provide to this part of Old Town given the Port's priorities, or if replacement or redevelopment is more feasible</p>
<p>LONG RANGE OBJECTIVES Years 4-6</p>				
<p>Identify attractive target properties not currently owned by the POS for acquisition, Acquire available properties to expand facilities. Work with the community to convert properties to beneficial use.</p>	<p>4-6 years</p>	<p>Port funds, Grants including WWRP- Trails ALEA, WWRP-Water Access, Local Parks, etc.</p>	<p>\$250,000 - \$2,000,000</p>	<p>One key piece of property on Lowell Street, a waterfront parcel on north side of Lowell Street, is targeted for acquisition. Other properties are identified in the plan.</p>
<p>Acquire property for sailboats storage and expanding non-motorized boating opportunities</p>	<p>4-6 years</p>	<p>Port funds, Grants including ALEA, WWRP- Local Parks, and others</p>	<p>\$250,000 - \$500,000</p>	<p>In support of a Sailboat Storage Project on Byron Street, the purchase of real estate at the southwest corner of Byron Street and McConnell Avenue is desirable</p>

Self-Certification Form

Planning Eligibility Self-Certification Form <i>Use this form to certify that the need for any grant projects have been developed through an appropriate planning process. Provide the completed form with the subject plans and adoption documentation to RCO via e-mail or other means of electronic access (i.e. Web link, Box.com, etc.).</i>		
Organization Name: PORT OF SILVERDALE		
Contact Name: Kathleen Byrne-Barrantes Tel: 360-697-5815 or email: kathleenbb@comcast.net		
Adoption Date of Submitted Documents: January 2020		
Seeking Eligibility for: <input checked="" type="checkbox"/> Recreation Grants <input checked="" type="checkbox"/> Conservation Grants <input checked="" type="checkbox"/> Both		
Initial Each to Certify Completion	Plan Element Certification	Document and Page Number Location of Information
TH	1. Goals, objectives: The attached plan supports our project with broad statements of intent (goals) <i>and</i> measures that describe when these intents will be attained (objectives). Goals may include a higher level of service.	Comp Plan pp.14, 26, 31, 33, 35, 37, 39, 41, 42, 43, Appendix H
TH	2. Inventory: The plan includes a description of the service area's facilities, lands, programs, and their condition. <i>(This may be done in a quantitative format or in a qualitative/narrative format.)</i>	Comp Plan pp. 15, 29, 35, 37, Appendix B, C, & G
TH	3. Public involvement: The planning process gave the public ample opportunity to be involved in plan development and adoption.	Comp Plan pp. 3, 9-13, 40, 42, and Appendices C, D, & E
TH	4a. Demand and need analysis: In the plans: <ul style="list-style-type: none"> An analysis defines priorities, as appropriate, for acquisition, development, preservation, enhancement, management, etc., and explains why these actions are needed. The process used in developing the analysis assessed community desires for parks, recreation, open space, and/or habitat, as appropriate, in a manner appropriate for the service area (personal observation, informal talks, formal survey(s), workshops, etc.). 	Comp Plan pp. 16-22, 23-42, 44, and Appendices C, D, E, & G
TH	4b. Level of Service assessment (optional): An assessment of the criterion appropriate to your community. Possibly establish a higher level of service as a plan goal (above).	In progress and coordinated with Kitsap County's plan
TH	5. Capital Improvement Program: The plan includes a capital improvement/facility program that lists land acquisition, development, and renovation projects by year of anticipated implementation; include funding source. The program includes any capital project submitted to the Recreation and Conservation Funding Board for funding.	Comp Plan pp. 42, 43, and Appendix H
TH	6. Adoption: The plan and process has received formal governing body approval <i>(that is, city/county department head, district ranger, regional manager/supervisor, etc., as appropriate)</i> . Attach signed resolution, letter, or other adoption instrument.	TBD

Certification Signature

I certify that this information is true and complete to the best of my knowledge.

Print/Type Name: Theresa Haaland

Signature (Hand Written or Digital): *Theresa Haaland*

Title: Auditor/Administrator, Port of Silverdale

Date: January 20, 2020